

# KODIAK CITY COUNCIL

## WORK SESSION AGENDA

**Tuesday, March 21, 2017**

**Kodiak Public Library Multi-Purpose Room**

**7:30 p.m.**

*Work sessions are informal meetings of the City Council where Councilmembers review the upcoming regular meeting agenda packet and seek or receive information from staff. Although additional items not listed on the work session agenda are sometimes discussed when introduced by the Mayor, Council, or staff, no formal action is taken at work sessions and items that require formal Council action are placed on a regular Council meeting agenda. Public comments at work sessions are NOT considered part of the official record. Public comments intended for the "official record" should be made at a regular City Council meeting.*

### Discussion Items

1. Public Comments (limited to 3 minutes)
2. Discussion About Discover Kodiak's Request to Expand Building.....1
3. Discussion About Amendment to Sales Tax Code.....3
4. Discussion About Formation of an Economic Development Committee.....77
5. Update on Fisherman's Hall Restroom Project
6. Discussion About Fisheries Analyst Contract .....81
7. Update on Kodiak Pedestrian Pathway and Near Island Development Plan.....103
8. Public Works Department Update
9. Elected Officials Training/Travel Requests
10. March 23, 2017, Agenda Packet Review

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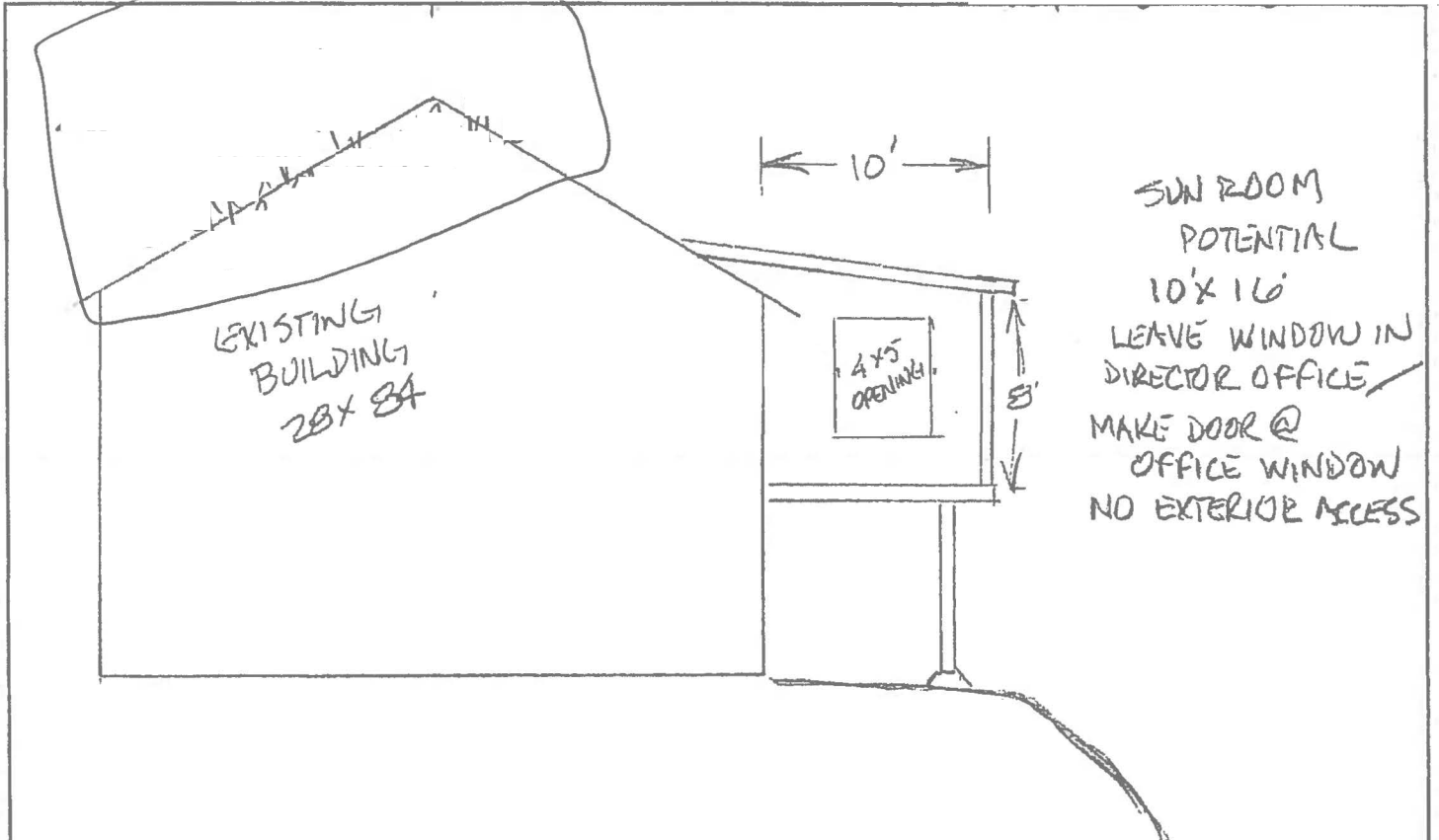
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SHEET NO. \_\_\_\_\_

CALCULATED BY \_\_\_\_\_ DATE \_\_\_\_\_

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SCALE \_\_\_\_\_

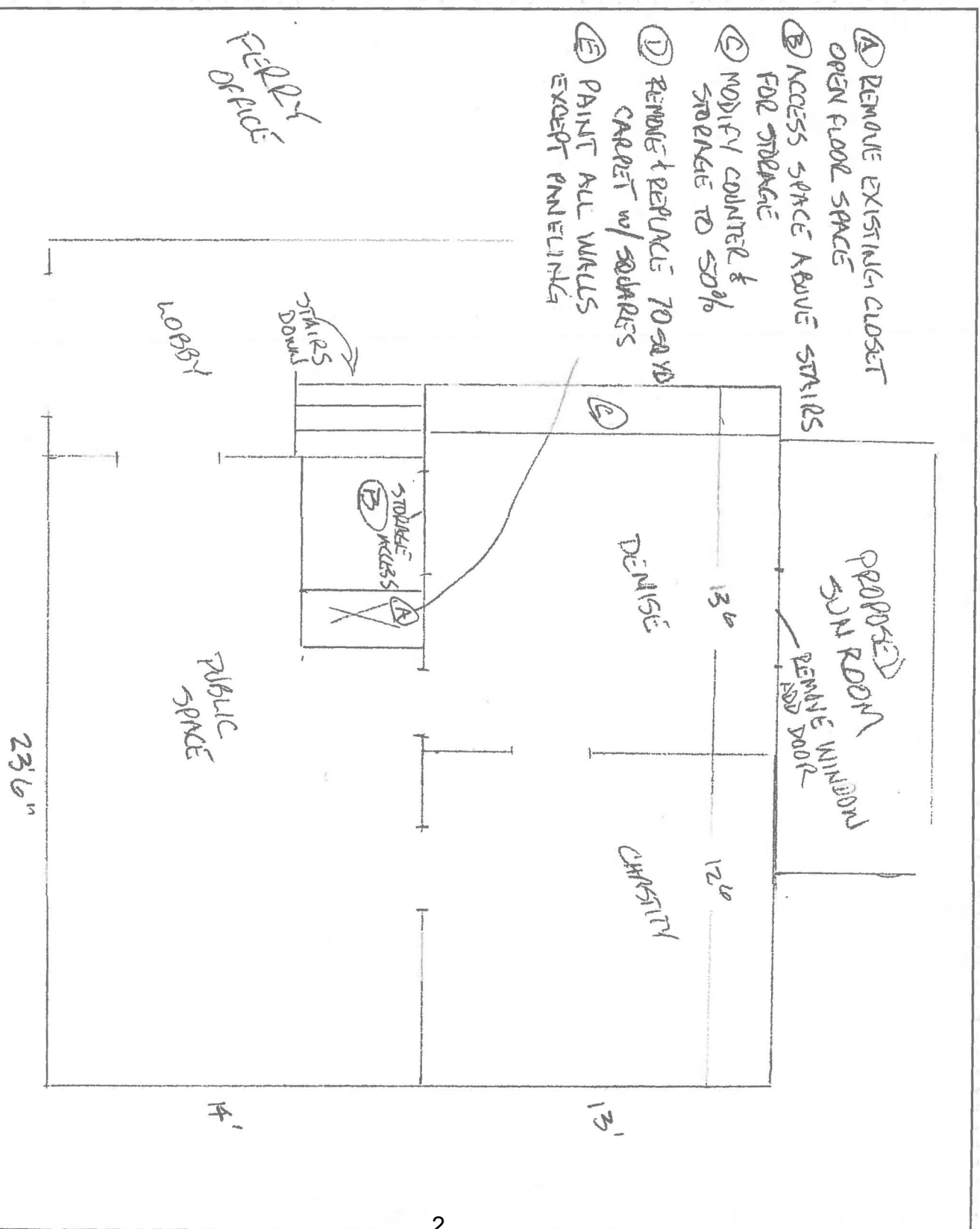


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- Ⓐ REMOVE EXISTING CLOSET  
OPEN FLOOR SPACE
- Ⓑ ACCESS SPACE ABOVE STAIRS  
FOR STORAGE
- Ⓒ MODIFY COUNTER &  
STORAGE TO 50%
- Ⓓ REMOVE & REPLACE TO 90%  
CARPET w/ SQUARES
- Ⓔ PAINT ALL WALLS  
EXCEPT PANELING

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OFFICE



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## MEMORANDUM TO COUNCIL

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**To:** Mayor Branson and City Councilmembers

**From:** Aimée Kniaziowski, City Manager

**Thru:** Kelly Mayes, Finance Director

**Date:** March 8, 2017

**Agenda Item:** **March 21, 2017 Work Session – Sales Tax Discussion**

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**SUMMARY:**

Sales tax discussions are listed as an agenda item on the City Work Session for March 21, 2017. To continue our discussion regarding this topic, city staff is requesting that you review the enclosed presentations prior to the work session.

**BACKGROUND:**

The State of Alaska provides funding to the City of Kodiak through various sources – capital grants, shared revenues, PERS on-behalf payments, and other miscellaneous sources. Due to economic conditions within the State, the State of Alaska budget has been experiencing a shortfall over the past three fiscal years. This shortfall at the state level has impacted funding provided to the City of Kodiak. The City revenues are generated through sales tax, property tax, city charges for services, and intergovernmental revenues. Due to continual decreases in city intergovernmental revenues, city staff has presented the enclosed data as items for discussion.

**ENCLOSURES:**

Enclosed are the work session presentations related to sales tax. These presentations were given on the following dates:

- Overview of Sales Tax – 01/11/2016
- Sales Tax Exemptions – 07/12/2016
- Casual & Isolated Sales / City Business Licenses – 08/09/2016
- Alternative Revenue Sources – 09/06/2016
- Planning for the Future – 10/11/2016

Please bring this package to the March 21, 2017 work session.

# City of Kodiak

Sales Tax – 01.11.2016



# Calendar of Discussions

- Sales Tax Exemptions – 07.12.2016
- Business Licenses and Permits – 08.09.2016
- Alternative Revenue Sources – 09.06.2016
- Planning for the Future – 10.11.2016 (Tentative)





## General Fund Revenues

- Primary Source of Revenue: Sales Tax
- Secondary Sources of Revenue: interfund charges to the enterprise funds; governmental grants; state revenue sharing; charges for services, etc.
- In FY 2015 - \$11.7 million was generated in sales tax revenue; estimated FY 2016 sales tax revenue - \$11.5M
- Sales Tax available for City use: \$10.5M as \$1M in transfers are for R&M / Capital projects



# Upcoming City Challenges

- Personnel –
  - Increasing employee costs
  - Pending Unionization
  - Actively Recruiting open positions
- Equipment –
  - Fire Truck to be replaced = \$1.5 million
  - Public Works heavy equipment - \$2.5 million
  - Fire Department equipment = est \$1 million
  - IT Software upgrades = \$200,000

# Upcoming City Challenges continued

- Infrastructure
  - PW Building – estimated \$5-\$10 million
  - Fire Station – estimated \$5-\$10 million
  - Mill Bay Road each year R&M = \$1.5 million
  - Mill Bay Road Rehabilitation = est \$6-\$8 million
  - Annual Sidewalk, Curb, & Gutter R&M = \$100,000
  - Storm Drainage Rehabilitation = est. \$8-\$10 million
  - Waste Water Treatment facility major upgrades
  - St Herman Harbor Replacement – est \$29 million



## Estimated Life of Unassigned Fund Balances

- FY 2015 General Fund – Fund Balance: 6,735,867
- FY 2016 estimated deficit = \$2,000,000
- If deficit trends continue = Fund balance will remain for 3 years (FY 2019)
- Estimated General Fund Needs based on City Challenges = \$3 - \$4 million per year

## Estimated Life of Unassigned Fund Balances

- FY 2015 Harbor Fund – Fund Balance: 3,523,982
- FY 2016 estimated deficit = \$119,254
- Fund Balance Assigned to R&M / Capital Projects = \$2,893,496
- Net Fund Balance after assignments = 511,232
- FY 2017 Budgeted deficit = \$1,369,696
- Fund balance will be eliminated in FY 2017 and would require General Fund transfers to balance budget for Harbor Operations

# Challenges to the Citizens of Kodiak

- Monthly Salary @ \$25 / hour = \$25/hr \* 2,080 per year = \$52,000 gross
  - Estimated 30% in federal taxes, PERS, etc. = 15,600
  - \$52,000 (gross) less \$15,600 (deductions) = 36,400
  - \$36,400 net divided by 26 pay periods per year = \$1,400 per pay period, est. \$2,800 per month
  - Rent / Mortgage = \$1,000 / month
  - Groceries = \$650 / month
  - Car Payment = \$400 / month
  - Insurance – renters/house & car = \$150 / month
  - All Utilities (W,S,P,H,C,T) = \$1,000 / month
- Net after monthly bills = \$2,800 income less \$3,200 expenses = (\$400)



# City of Kodiak

## Sales Tax Update



## City Council FY 2016 Budget Goals

- Annual budget goals include “reviewing ways to increase revenues in the General Fund to help offset increases in operating expenses, meet infrastructure needs, and to maintain the fund balance of the General Fund per the plan outlined during 01/14/2012” .



# Sales Tax Overview

- City currently levies 7% sales tax up to \$750 per transaction
- Sales tax was increased from 6% to 7% in 2013
- Sales tax levies and exemptions have not been modified since that time
- Annual allocations from Sales tax include the following:

	<u>Percentage</u>	<u>Maximum</u>
▫ Street Improvement Fund	0.45%	\$450,000
▫ Parks Improvement Fund	0.05%	\$ 50,000
▫ Port & Harbor Improvements	0.50%	\$500,000



# Sales Tax Exemptions

- There are 26 general exemptions from sales tax
  - Exemptions include, but are not limited to the following:
    - Casual and isolated sales, rentals, services
    - These typically include bazaars, craft shows, crab festival, and cruise ship events.
    - Medical services – licensed physicians, dentists, hospitals, prescription drugs, etc.
    - Religious and charitable organizations as defined by the Internal Revenue Code
    - School cafeterias and lunchrooms
    - Nonprofit and student organizations
    - Government agencies as defined and recognized by the United States and State of Alaska
    - Dues / fees to clubs, labor unions, and fraternal organizations
    - Subscriptions to newspapers and periodicals
    - Sales of insurance and bonds of guaranty and fidelity
    - Funeral charges



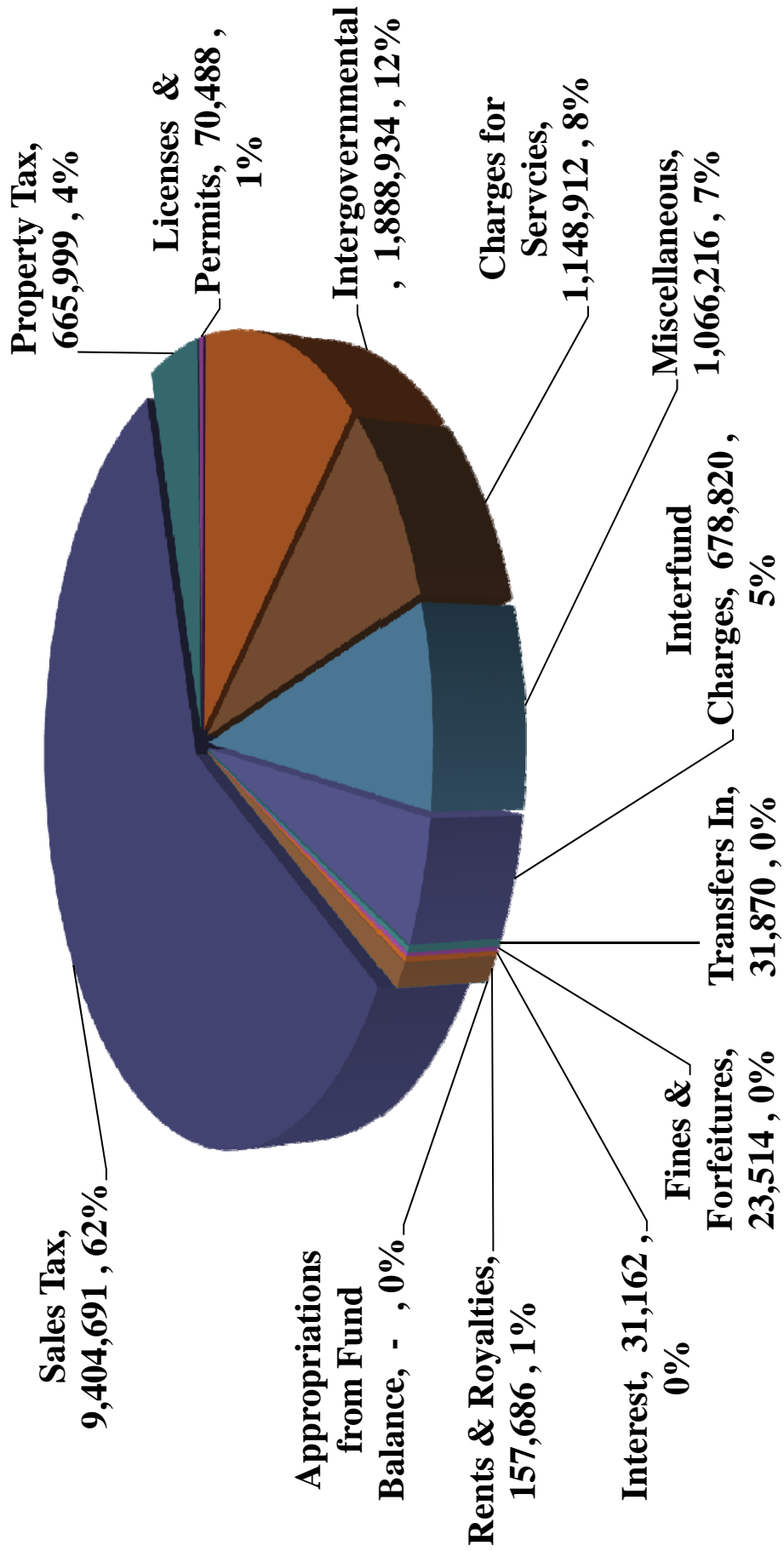
# Sales Tax Exemptions continued

- Transportation charges of commercial airlines, air charters, and passenger ship companies provided that this exemption shall not extend to boat charter operations not affecting interstate commerce
- Services rendered by banking or savings and loan institutions or credit unions
- Services rendered by an employer to an employee in the normal course of business
- Sales, rentals and services which the City is prohibited from taxing by the Constitution or the laws of the United States and the State of Alaska
- Retail sales in dining rooms or cafeterias of food furnished by nonprofit organizations under programs wholly or partially supported by government funds
- Nursery and babysitting services
- Long distance transmission of telephone and telegraph messages
- Sales of heating fuel exclusively for residential uses – this does not include fuel used in or on watercraft

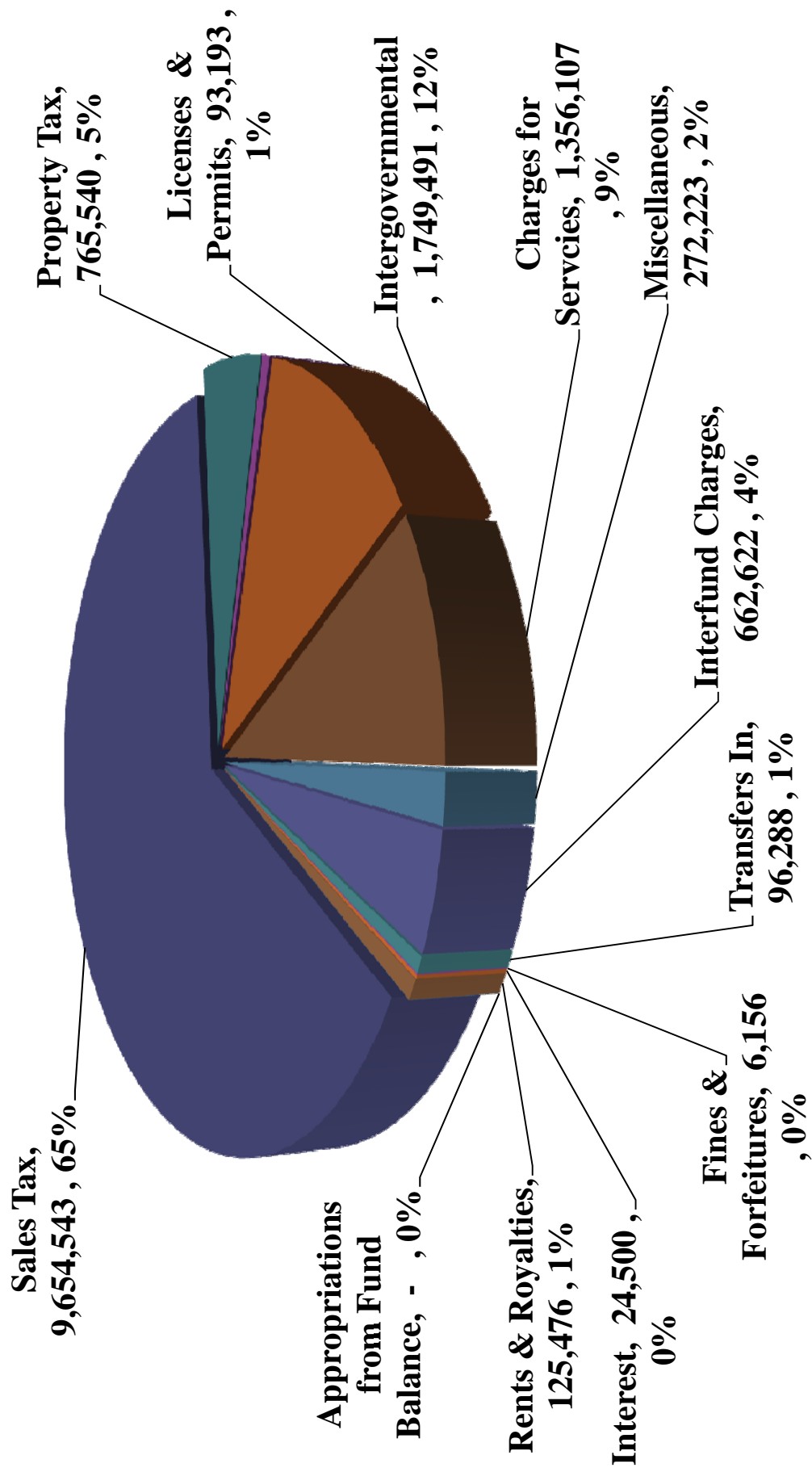
# Sales Tax Exemptions continued

- Sales of electric service to exclusively residential units
- Sales of propane gas exclusively for residential uses (e.g. cooking, water heating, etc.)
- Sales of fuel used in stationary power plants that generate electrical energy exclusively for private residential consumption
- Sales, rentals, and services to nonprofit organizations and associations operated primarily for the purpose of planning and conducting organized group activities for participants 18 years of age or less
- Charges for garbage/refuse collection for garbage/refuse generated exclusively by residential uses
- Sales of water and sewer utility services for residential use
- Sales by religious or charitable organizations as defined by the Internal Revenue Code, for pull tabs, raffles, lottery tickets, bingo cards, and other tokens of participation in games of chance and contests of skill.

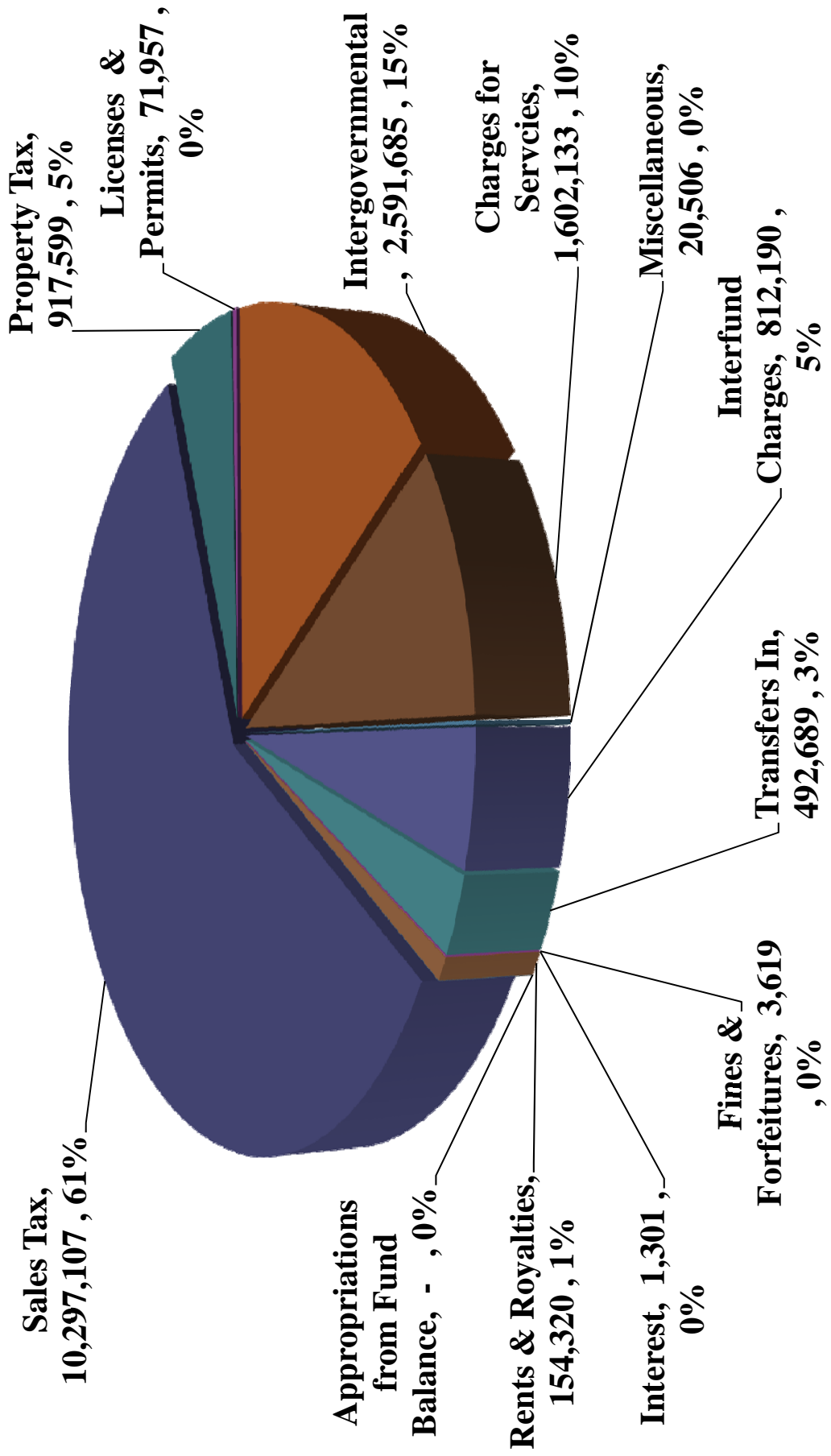
# Fiscal Year 2010 Revenue by Source



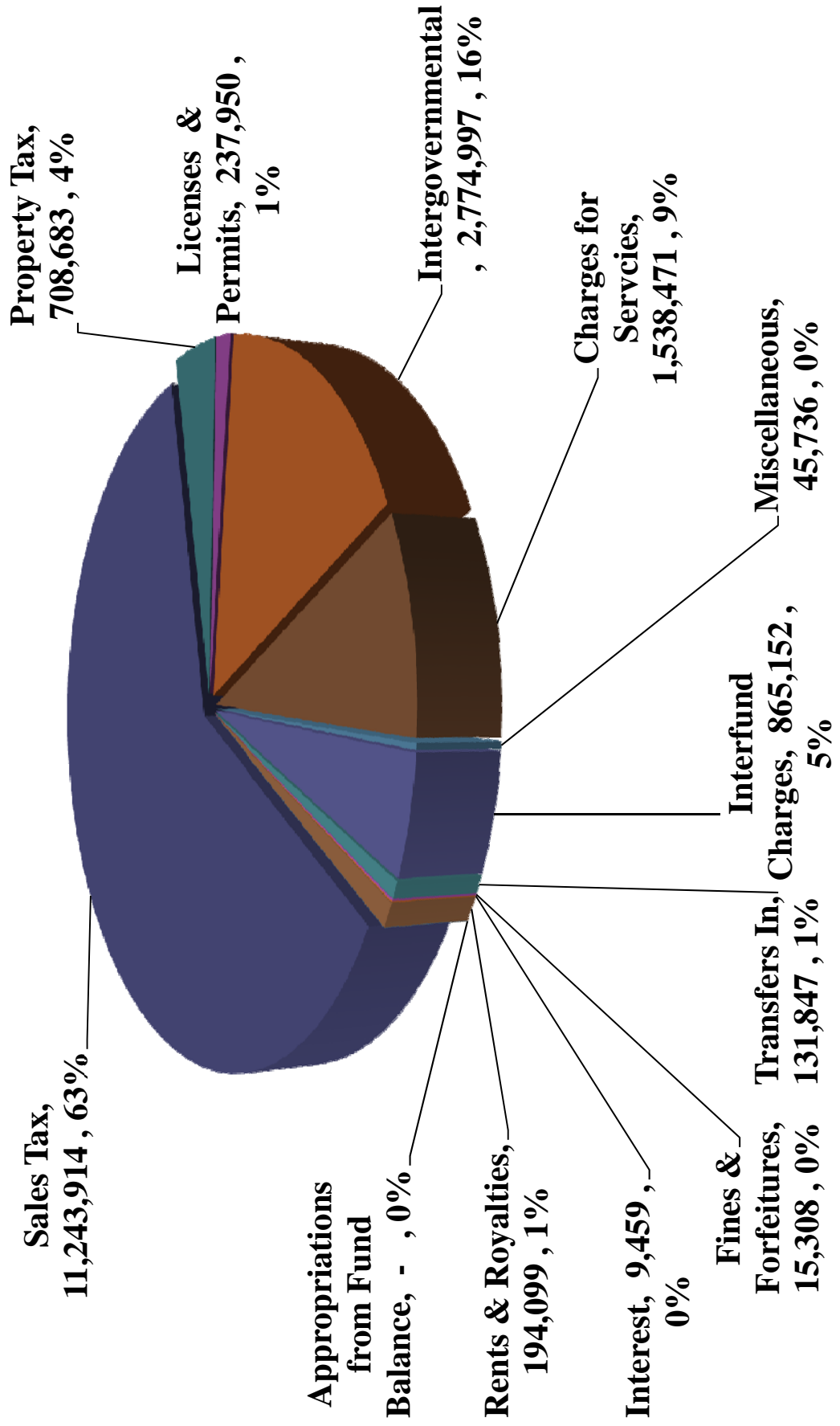
# Fiscal Year 2011 Revenue by Source



# Fiscal Year 2012 Revenue by Source

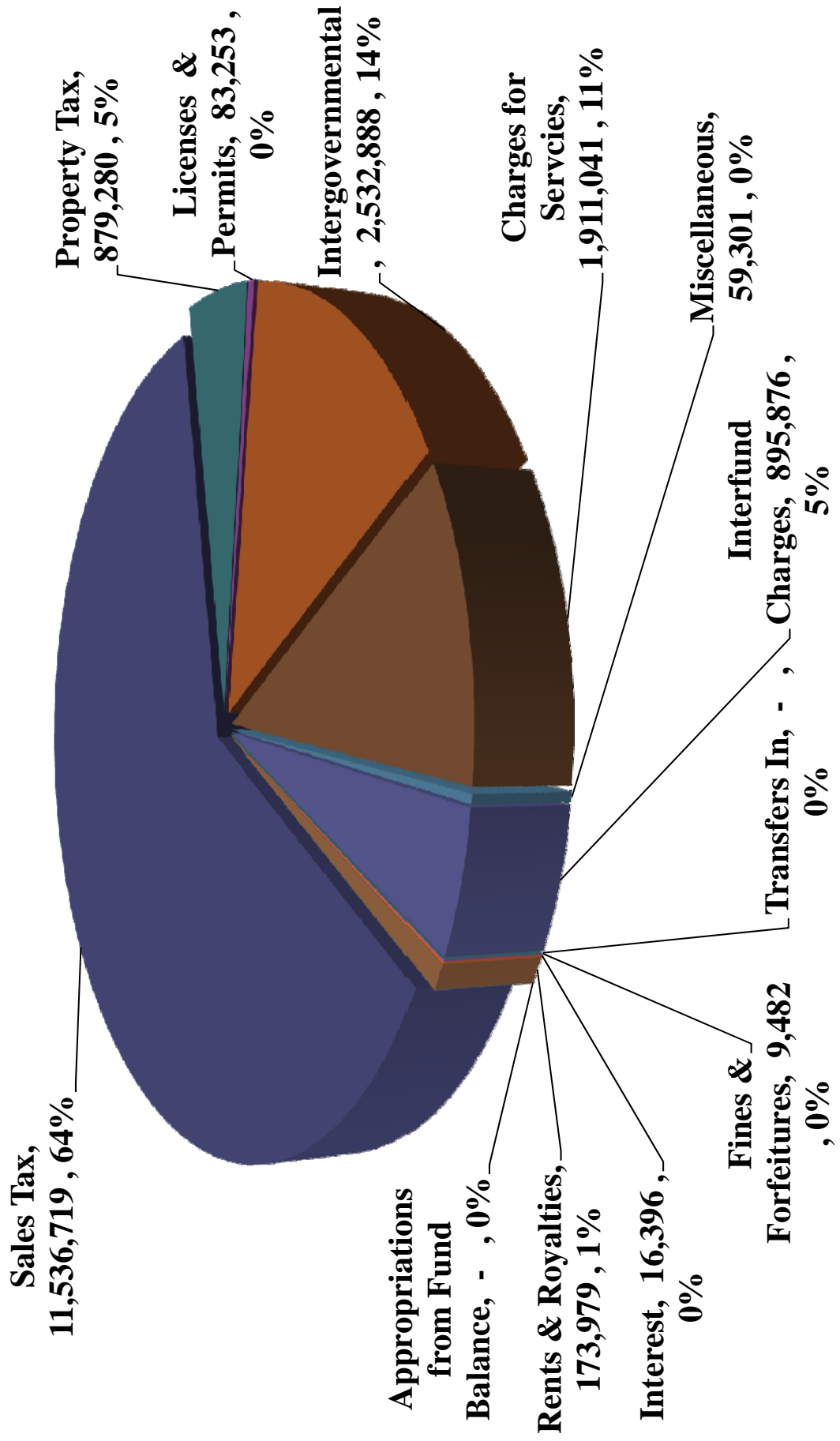


# Fiscal Year 2013 Revenue by Source

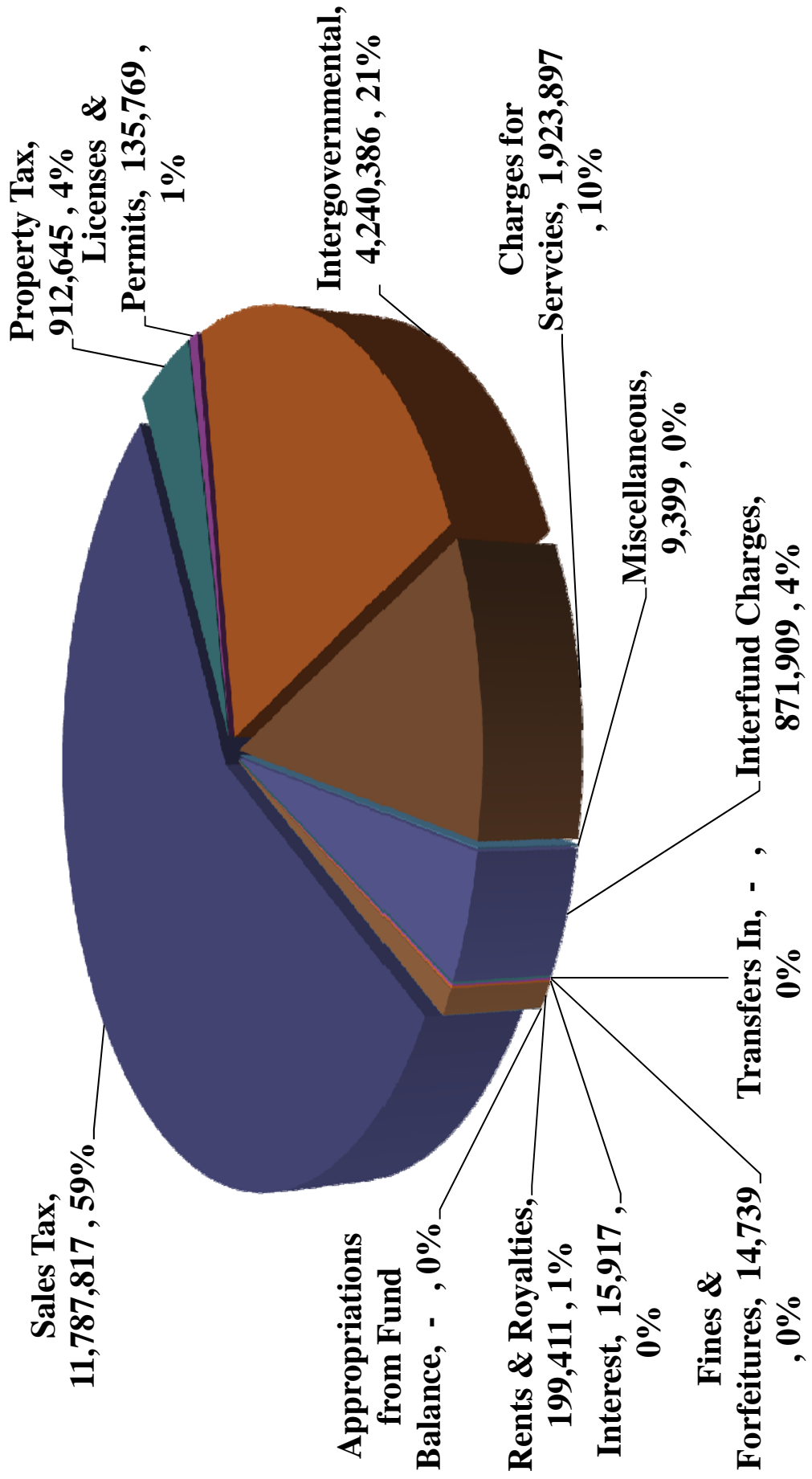




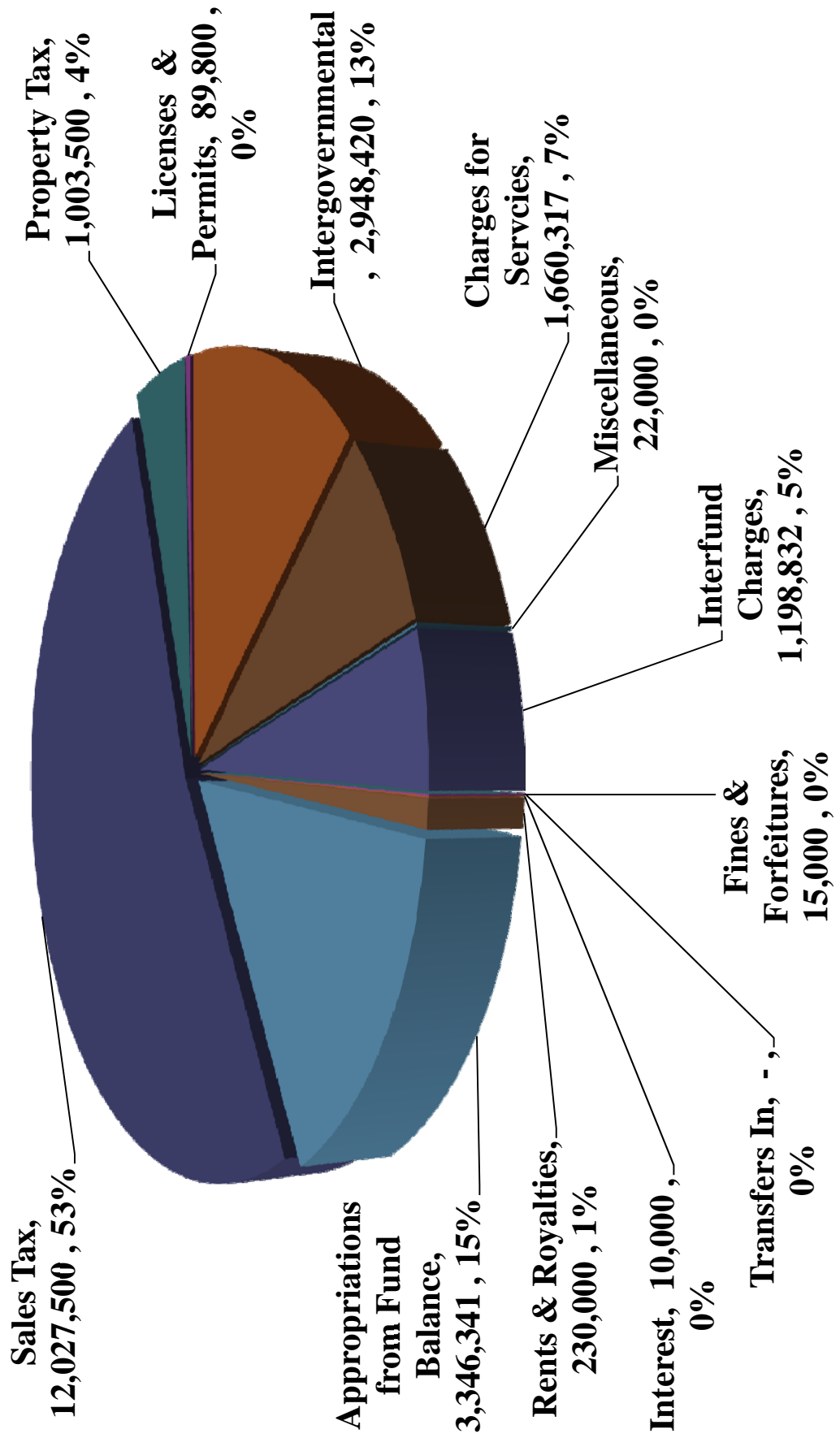
# Fiscal Year 2014 Revenue by Source



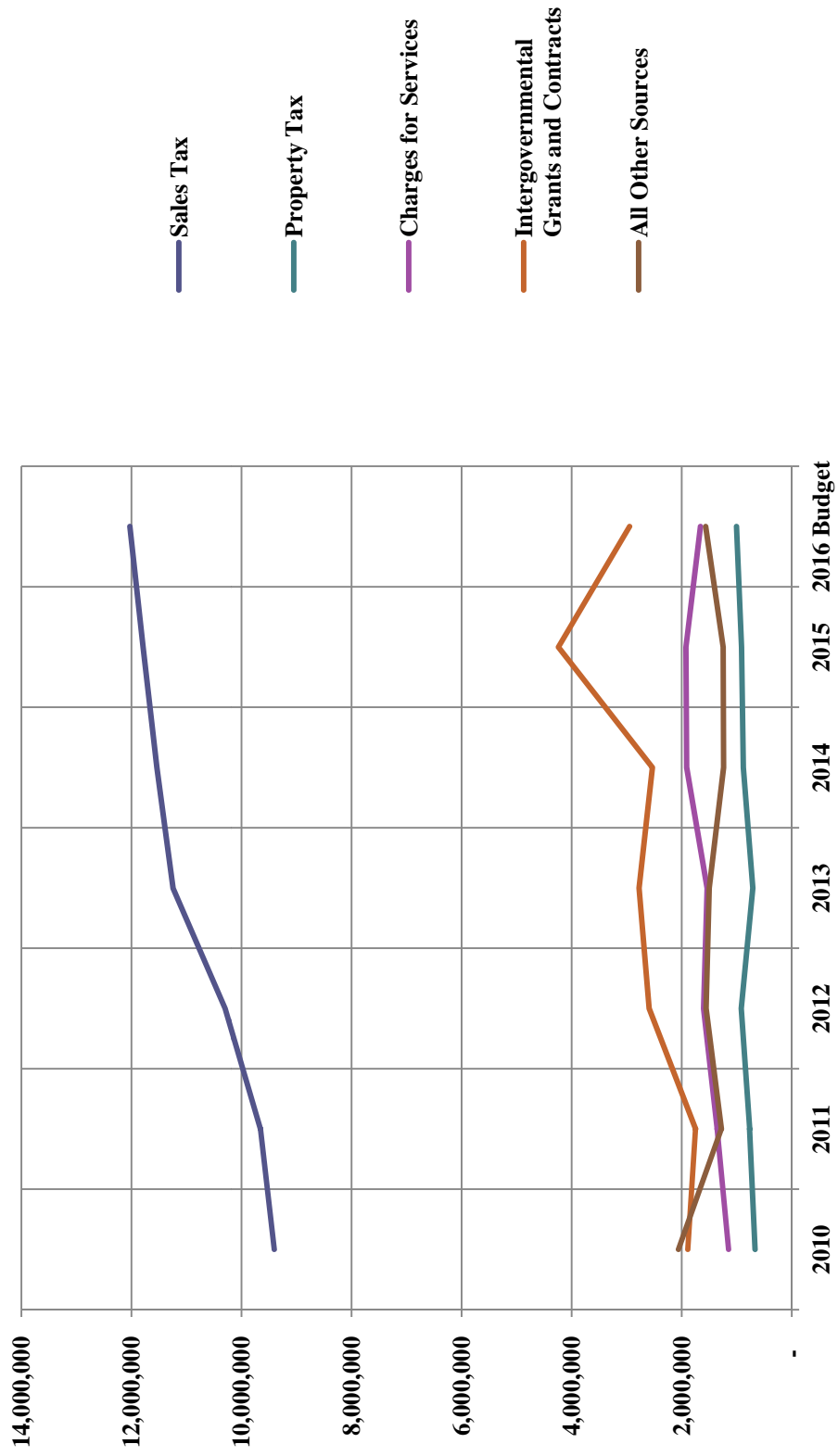
# Fiscal Year 2015 Revenue by Source



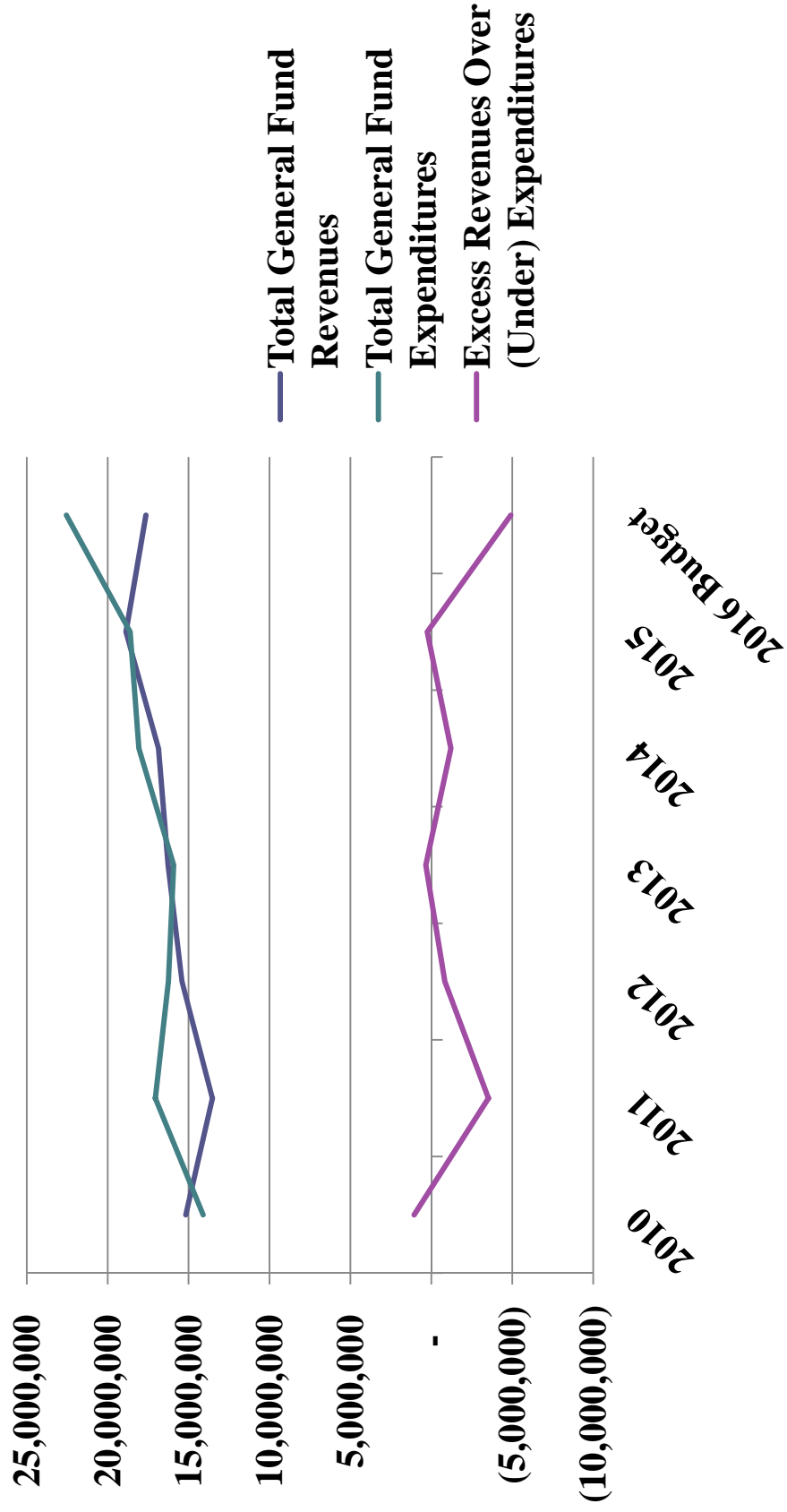
# Fiscal Year 2016 Revenue by Source



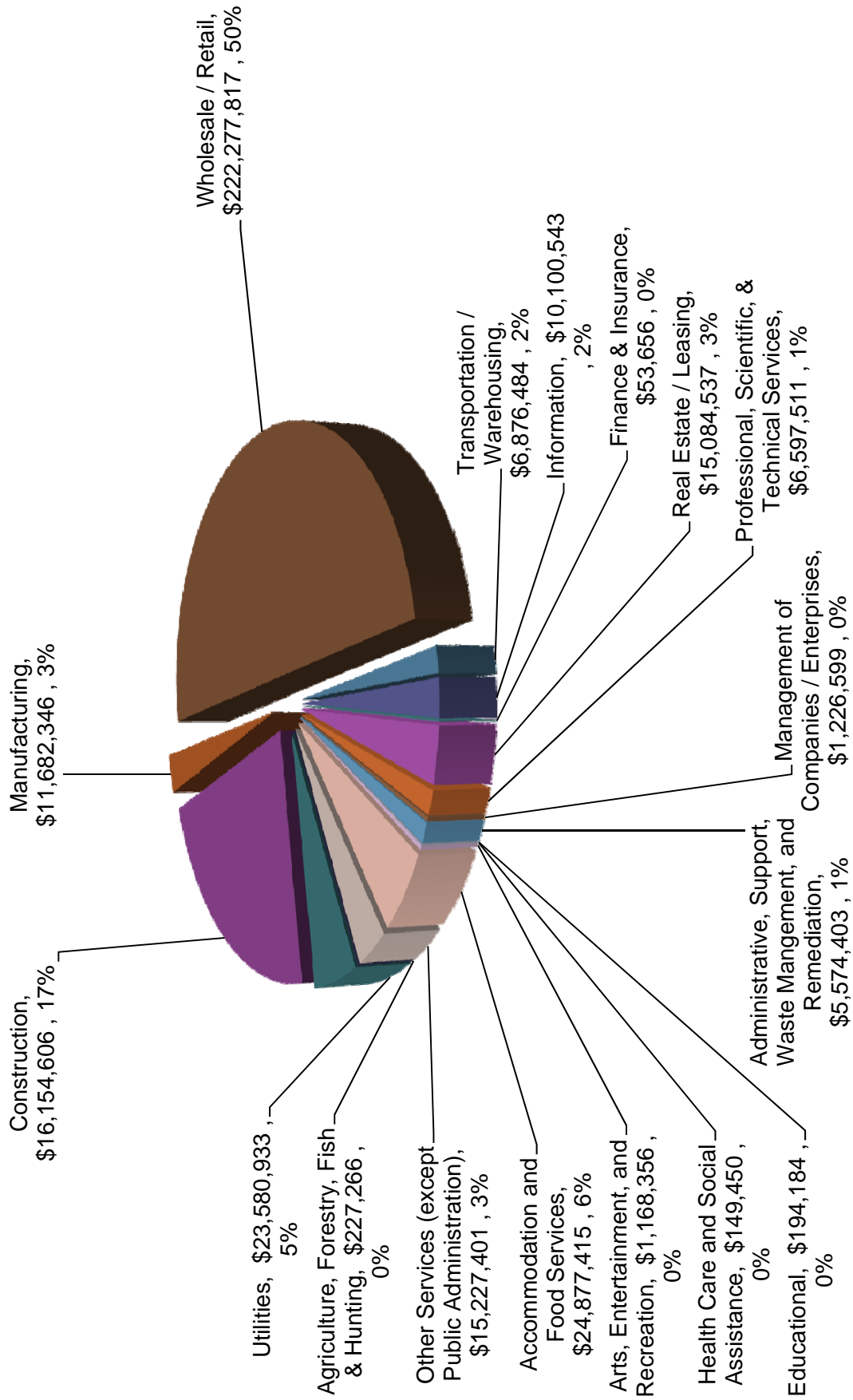
# Revenue by Source over Time



# Revenue and Expenditures Over Time



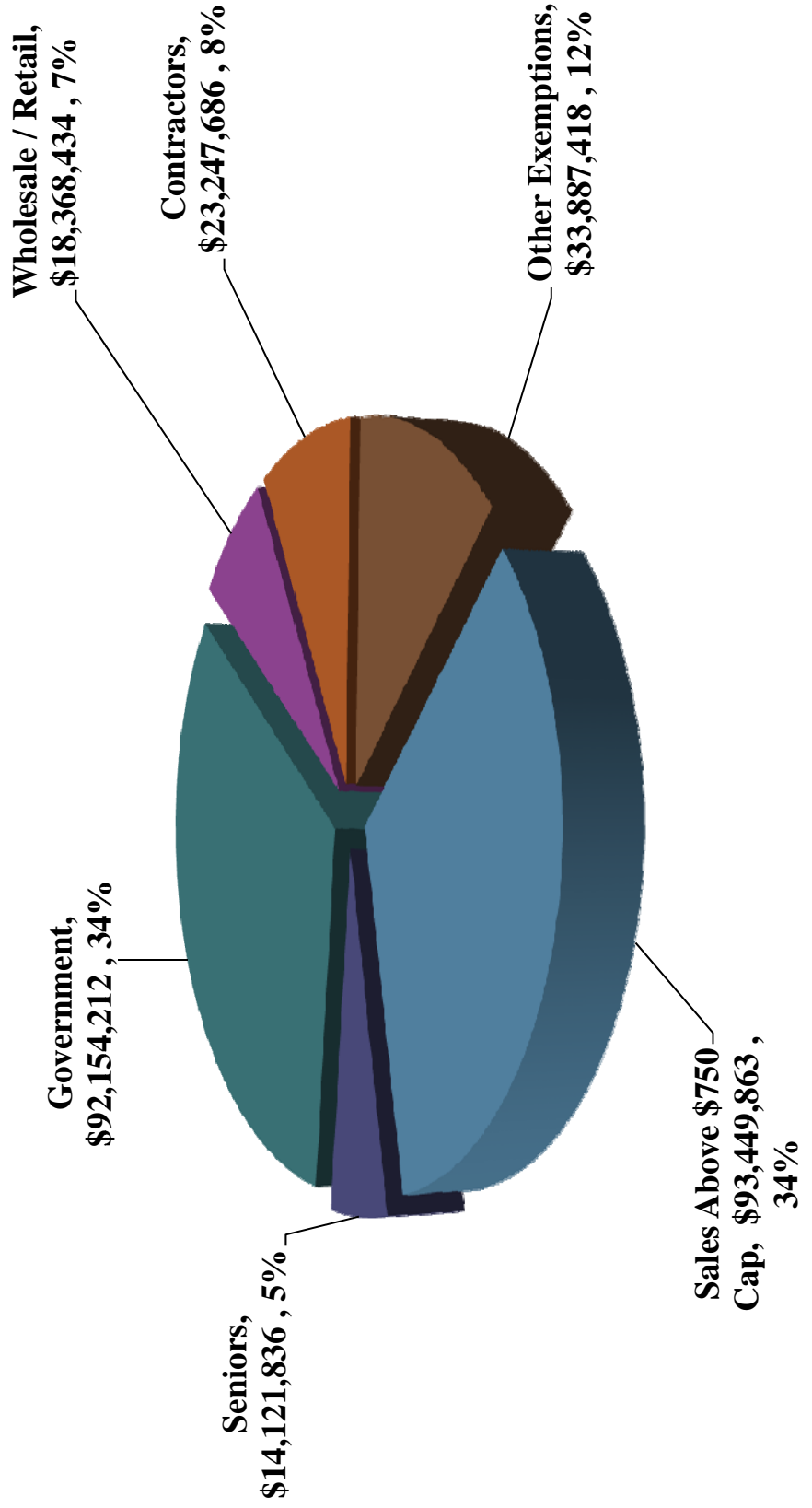
# FY 2015 Gross Sales by Business Type



# Fiscal Year 2015 Sales Tax

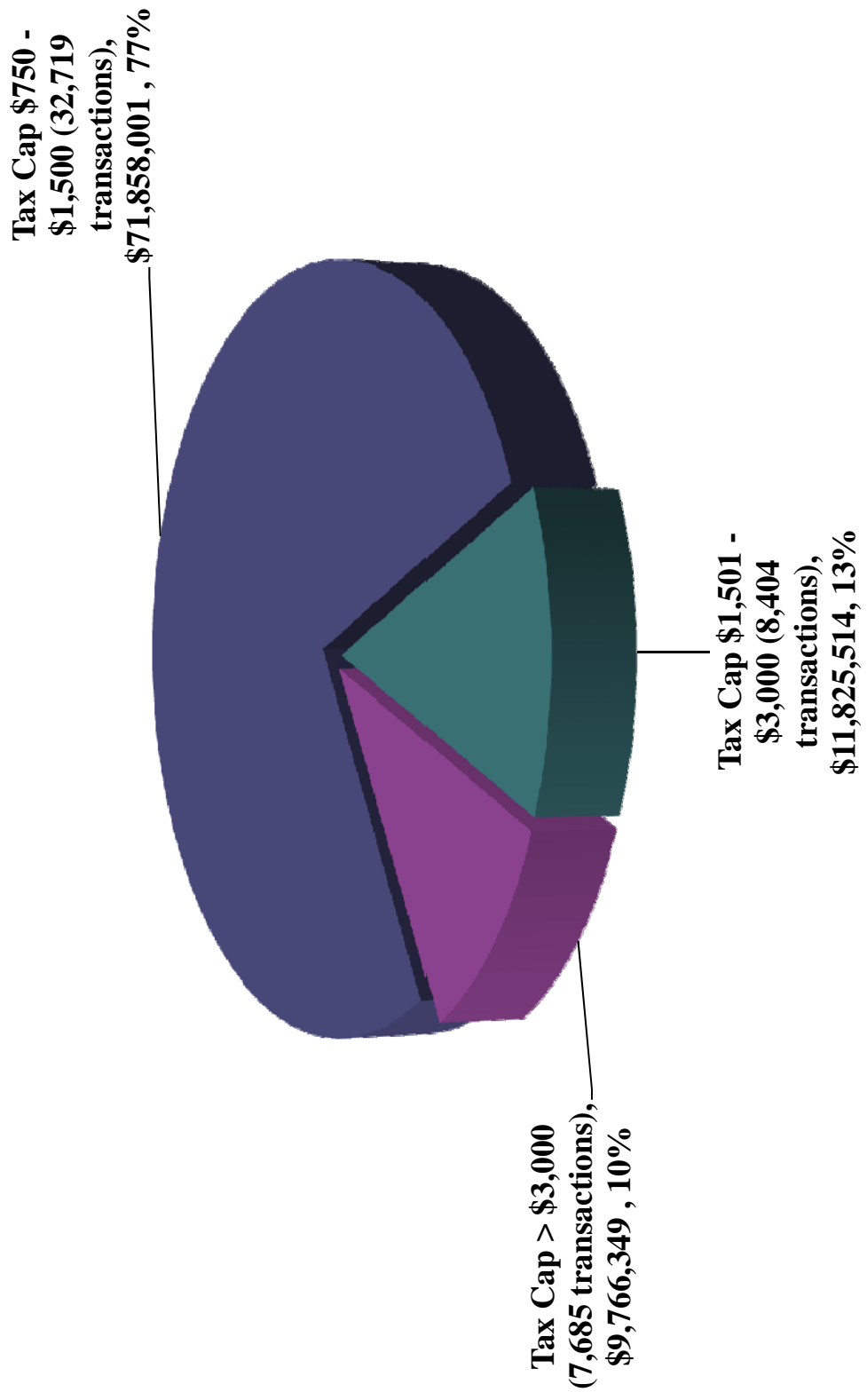
Gross Sales – Retail	\$341,814,684.62
Gross Sales – Services	\$84,953,088.44
Gross Sales – Rentals	<u>\$17,109,490.26</u>
<b>Total Gross Sales</b>	<b>\$443,877,263.32</b>
Senior Exemptions	\$14,121,836.48
Government Exemptions	\$92,154,212.11
Wholesale / Retail Exemptions	\$18,368,433.97
Contractors Exemptions	\$23,247,686.18
Other Exemptions	\$33,887,417.53
Exemptions for Sales Above \$750 Cap	<u>\$93,449,863.30</u>
<b>Total Exemptions</b>	<b>\$275,229,449.57</b>
<b>Net Taxable Revenues</b>	<b>\$168,647,813.75</b>
<b>FY 2015 Sales Tax Due</b>	<b>\$11,804,615.24</b>

# Sales Tax Exemptions





# Exemptions for Sales > \$750 Cap



# How does Kodiak compare?

- Cities used for comparison based on population ranges from 4,500 to 7,500 throughout Alaska: Bethel, Homer, Kenai, Palmer, and Unalaska
  - Bethel recently instituted marijuana tax of 15% and alcohol tax of 12% in addition to increasing excise taxes on tobacco
  - Unalaska, rather than senior exemptions, sends a refund of \$200 annual refund to eligible seniors
  - Sales Tax Rates are lower: 4.5%-Homer; 3.0%-Kenai; 3.0%-Palmer; 6.0%-Bethel; 3%-Unalaska
  - Unalaska generated approximately \$12 million in sales tax revenues on a 3% sales tax rate for 2014 and 2015. This does not include bed tax or raw fish tax.
  - Unalaska, Palmer, and Bethel require city business licenses and annual renewals
  - Many do not have tax caps

FY 2015 Full Year Actual Calcs Used for Base	
Retail Sales	341,814,684.62
Services	84,953,088.44
Rentals	17,109,490.26
Other	
Total Gross Taxable Revenue	443,877,263.32
Seniors	14,121,836.48
Government	92,154,212.11
Wholesale / Retail	18,368,433.97
Contractors	23,247,686.18
Other	33,887,417.53
Total Deductions	181,779,586.27
\$750-\$1,500 Deductions above Tax Cap	71,858,001.00
\$1,501-\$3,000 Deductions above Tax Cap	11,825,514.00
>\$3,000 Deductions above Tax Cap	9,766,349.00
Total Above \$750	93,449,864.00
Total Net Taxable	168,647,813.05
Total # of Transactions at 52.50	3,212,339.30
FY 2015 Sales Tax Due	11,805,346.91
FY 2015 Sales Tax Revenue Collected	11,759,863.00

Assumption 1 - Decrease Rate & Eliminate Cap	
Total Net Taxable	168,647,813.05
Tax Cap	93,449,864.00
New Total Net Taxable	262,097,677.05
X 5%	13,104,883.85
Projected Increase in Sales Tax Revenue	1,345,020.85
X 6%	15,725,860.62
Projected Increase in Sales Tax Revenue	3,965,997.62

7% Rate and Eliminate Cap	
Total Net Taxable	168,647,813.05
Tax Cap	93,449,864.00
New Total Net Taxable	262,097,677.05
X 7%	18,346,837.39
Projected Increase in Sales Tax Revenue	6,586,974.39

Assumption 2 - Decrease Rate & Increase Cap to \$1,500	
Total Net Taxable	168,647,813.05
Tax Cap	71,858,001.00
New Total Net Taxable	240,505,814.05
X 5%	12,025,290.70
Projected Increase in Sales Tax Revenue	265,427.70
X 6%	14,430,348.84
Projected Increase in Sales Tax Revenue	2,670,485.84

7% Rate and Increase Cap to \$1,500	
Total Net Taxable	168,647,813.05
Tax Cap	71,858,001.00
New Total Net Taxable	240,505,814.05
X 7%	16,835,406.98
Projected Increase in Sales Tax Revenue	5,075,543.98

Assumption 3 - Decrease Rate & Increase Cap to \$3,000	
Total Net Taxable	168,647,813.05
Tax Cap	83,683,515.00
New Total Net Taxable	252,331,328.05
X 5%	12,616,566.40
Projected Increase in Sales Tax Revenue	856,703.40
X 6%	15,139,879.68
Projected Increase in Sales Tax Revenue	3,380,016.68

7% Rate and Increase Cap to \$3,000	
Total Net Taxable	168,647,813.05
Tax Cap	83,683,515.00
New Total Net Taxable	252,331,328.05
X 5%	17,663,192.96
Projected Increase in Sales Tax Revenue	5,903,329.96

FY 2015 Full Year Actual Calcs Used for Base	
Retail Sales	341,814,684.62
Services	84,953,088.44
Rentals	17,109,490.26
Other	-
<b>Total Gross Taxable Revenue</b>	<b>443,877,263.32</b>
Seniors	14,121,836.48
Government	92,154,212.11
Wholesale / Retail	18,368,433.97
Contractors	23,247,686.18
Other	33,887,417.53
Sales Tax Included	-
<b>Total Deductions</b>	<b>181,779,586.27</b>
\$750-\$1,500 Deductions above Tax Cap	71,858,001.00
\$1,501-\$3,000 Deductions above Tax Cap	11,825,514.00
>\$3,000 Deductions above Tax Cap	9,766,349.00
<b>Total Above \$750</b>	<b>93,449,864.00</b>
<b>Total Net Taxable</b>	<b>168,647,813.05</b>
Total # of Transactions at 52.50	3,212,339.30
FY 2015 Sales Tax Due	11,805,346.91
FY 2015 Sales Tax Revenue Collected	11,759,863.00

Assumption 1 - Eliminate Senior Exemption	
Total Net Taxable	168,647,813.05
Senior Exemptions	14,121,836.48
<b>New Total Net Taxable</b>	<b>182,769,649.53</b>
X 7%	12,793,875.47
<b>Projected Increase in Sales Tax Revenue</b>	<b>1,034,012.47</b>
<b>Senior Rebates</b>	
Estimated 1200 Seniors	1,200
X \$200 Rebate	200.00
	\$ 240,000.00
<b>Total Net Increase from Senior Exemptions</b>	<b>\$ 794,012.47</b>

Assumption 2 - Add Exemption for Senior Groceries	
Total Net Taxable	168,647,813.05
Total Senior Exemptions	14,121,836.48
<b>Less Senior Exemptions for Groceries-Estimated</b>	<b>(3,606,028.92)</b>
<b>New Total Net Taxable</b>	<b>179,163,620.61</b>
X 7%	12,541,453.44
<b>Projected Increase in Sales Tax Revenue</b>	<b>781,590.44</b>
<b>Projected Net Increase with Senior Rebate Included</b>	<b>\$ 541,590.44</b>

Assumption 3 - Lower Tax on Groceries Only	
Annual Grocery Store Net Taxable Revenue	30,355,906.34
X 7%	2,124,913.44
<b>Amount included in Sales Tax Revenue</b>	<b>2,124,913.44</b>
Annual Grocery Store Net Taxable Revenue	30,355,906.34
X 5%	1,517,795.32
<b>Decrease in Sales Tax Revenue from 7% to 5%</b>	<b>(607,118.13)</b>

**Comparison of Exemptions**

Type of Exemption	Kodiak	Unalaska
Air Travel	3.08.040(k)	X
Casual Sales	3.08.040(a)	X
Child Care Services	3.08.040(p)	X
Church Sales	3.08.040(c)	X
Construction of New Residential Housing	Sale for Resale - 3.08.050(d)	X
Court Fixed Fees - any sale where the price is fixed by law, by a court of competent jurisdiction, or by other authority beyond the control of the seller	None per Kodiak Municipal Code	X
Dues and Fees - Paid to clubs, labor unions, professional organizations, and fraternal organizations.	3.08.040(g)	X
Exports		X
Federal and State Prohibitions	3.08.040(n)	X
Food Stamps		X
Freight, Delivery, and Wharfage	None per Kodiak Municipal Code	X
Governmental Agencies	3.08.040(f)	X
Insurance	3.08.040(i)	X
Loans - including relating interest	3.08.040(l)	X
Medical Services and Sales	3.08.040(b)	X
Newspapers	3.08.040(h)	X
Nonprofit Organizations	3.08.040(c); 3.08.040(e); 3.08.040(v); 3.08.040(y)	X
Public Food - the sale of food and beverages to the public in public school cafeterias or lunchrooms which are operating primarily for teachers and students and not for profit	3.08.040(d); 3.08.040(o) for nonprofits with government funded cafeterias	X

Raw Seafood Product - limitations apply	None per Kodiak Municipal Code	X
Resale	3.08.050	X
Seafood Processors and Brokers - limitations apply	None per Kodiak Municipal Code	X
Student Transportation	None per Kodiak Municipal Code	X
Telephone and Telegraph	3.08.040(q)	X
Utility Sales - sale of service by a public utility is exempt	<p>3.08.040(R)-heating fuel exclusively for residential</p> <p>3.08.040(s)-electrical service exclusively to residential;</p> <p>3.08.040(t)-propane gas exclusively for residential;</p> <p>3.08.040(u)fuel used in stationary power plants that generate electrical energy exclusively for private residential consumption;</p> <p>3.08.040(w)-charges for garbage/refuse collection for garbage/refuse generated by residential use;</p> <p>3.08.040(x)-sales of water and sewer utility services for</p>	X
Funeral Charges	3.08.040(j)	None
Employee / Employer - services rendered by an employee to an employer in the normal course of employment.	3.08.040(m)	None
Senior Exemptions	3.08.055	\$200 rebate
Tax Free Day	1st Saturday in March - 3.08.045	None

# City of Kodiak

## Sales Tax Discussions – Phase II



## Sales Tax Discussions – Phase II

- Business Licenses
  - Permitting – incorporated into the Business License process and clearly defining “Casual and Isolated” sales for the purpose of the general sales tax exemption
  - Outline of the Business License process
  - Timeline of events to occur
  - Direction from Council on proceeding



# Casual and Isolated Sales

- Exempt in the City of Kodiak Code
- No clear definition exists with the City Code
- City Code Exemption states
  - Casual and isolated sales and rentals of personal property, and services not rendered in the regular course of business of the seller
- Casual and isolated – infrequent? How many?
- B&Bs can be personal property – are these excludable?
- What is considered ordinary course of business?
- Miscommunications can occur related to this Sales Tax exemption
  - i.e. My profession = CPA, my hobby = photographer
  - The IRS would consider this taxable income
- If side businesses are considered IRS income taxable, then why not sales taxable?

## Casual and Isolated Sales Defined

- Direction requested from City Attorney's office
  - Casual and Isolated Definition could include the following:
    - The following sales are deemed to be casual or isolated sales by persons who
      - do not hold themselves out to the public as engaged in business, and
      - have not leased or rented the property in their business operations:
      - A garage sale, rummage sale, or similar sale of used household goods when sold by the owner in his capacity as owner and not in the capacity of a retail merchant.
    - Estate sales if held at the location of the home and not including other personal property from other sources.
    - Individuals selling property owned and previously used as non business personal property such as cars, boats and aircraft.

# Business Licenses Process

- All businesses will be required to hold and display a City of Kodiak Business License on Jan 1, 2017
- The only exemptions would be federal and local government agencies
- Nonprofits would be required to hold a business license – annual applications required at no cost and proof of 501c status
- Draft of new Chapter for municipal code included with presentation – currently being reviewed by City Attorney
- Specific lines of business based on the IRS NAICS System would be required to pay a tax deposit with the annual application fee of \$50
- Option to waive \$50 application fee for all existing sales tax filers for first year of implementation. However, complete applications would still be required
- City Finance Department will mail applications by December 1 to be due by January 31 of each year.
- Half year considerations given for new businesses in Kodiak that occur mid-year.

## Business License Process continued

- All person(s) offering items to sell – including bazaars, cruise ship tables, etc. would be required to obtain and display a business license.
- Special events licenses are not currently included as any business operating in a special event (i.e. Crab Festival) would already be required to obtain and display a business license.
- Any and all permits would still be required, along with a business license. Business licenses would not eliminate the need for any permit currently in effect under the City of Kodiak municipal code. (i.e. Taxi Cabs)
- Business License Application
- Business License example



## Timeline of Events

- Draft ordinances regarding Code changes to have prepared by September 11 regularly scheduled meeting. These changes would tentatively become effective mid-November
  - Code changes would include: (1) adding clarifying language to the Casual and Isolated sales general exemption; (2) Chapter in the KCC regarding business licenses
- Once ordinances become effective, Finance Department would send out business license applications in early December 2016 to be due January 31, 2017. First year application fees for businesses currently registered for sales tax would be waived for calendar year 2017 for FEES ONLY.

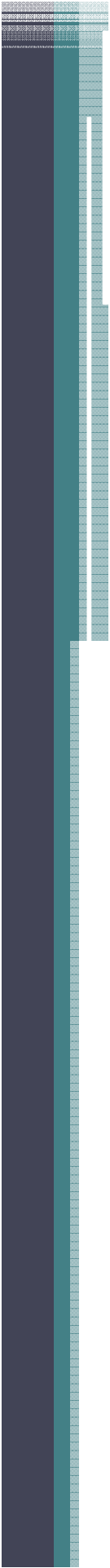


## Council Direction Requested

- Proceed with adding the clarifying language to Casual and Isolated Sales
  - Any changes, additions, deletions to be considered
- Proceed with drafting ordinances related to the new chapter for Business Licenses
  - Any changes, additions, deletions to be considered
- Proceed with waiving first year application fees

# City of Kodiak

## Sales Tax Discussions – Phase III Alternative Revenue Sources



# Potential Sources & Calculation Methodology

- Potential Sources
  - Sales tax on Alcohol
  - Sales tax on Tobacco
  - Marijuana Tax
  - Internet Tax – Sales Taxes and/or Use Taxes
- Due to unknown factors, each of these revenue sources from select municipalities was divided by the population for that particular area and then projected for the City



# Sales Tax on Alcohol

City	Population	Tax Rate	FY 2015 Revenues	\$ per Person
Bethel	6,241	0%	\$-0-	\$-0-
Dillingham	2,431	10%	\$297,325	\$122.31
Fairbanks	31,721	5%	\$2,239,679	\$70.61
North Pole	2,198	5%	\$211,997	\$96.45
Kodiak – est.	6,328	5%	\$446,792	Fairbanks Rate PP
Kodiak – est.	6,328	7% Avg Rate	\$610,336	Avg Rate \$96.45

In FY 2016, the City collected approximately \$525,000 related to alcohol retail sales. With an additional 5% alcohol sales tax rate, estimated annual revenue for the City would be and additional \$350,000 related only to the Alcohol Sales Tax.

# Sales Tax on Tobacco

City	Population	Tax Rate	FY 2015 Revenues	\$ per Person
Bethel	6,241	45%	\$480,876	\$77.05
Dillingham	2,431	-0-%	\$-0	\$-0-
Fairbanks	31,721	8%	\$887,239	\$27.97
North Pole	2,198	8%	\$132,228	\$60.16
Kodiak – est.	6,328	20% Avg Rate	\$348,420	Avg Rate \$55.06
Kodiak – est.	6,328	8%	\$176,995	Fairbanks Rate

# Sales Tax on Marijuana

Location	Year	Population	Recreational Marijuana Sales Tax Revenues	\$ per Person
State of WA	FY 2015	7,061,400	\$11,571,430	\$1.64
	FY 2016	7,170,351	\$30,017,823	\$4.19
Kodiak – est.	6,328		\$18,431	Avg Rate \$2.91

# Internet Sales - Sales Tax and Use Tax

- Two types of taxes related to Internet Sales
  - **Sales Tax** – sales tax charged by vendors on telephone or internet sales
  - **Use Tax** – Consumer use tax would be payable to the City by individuals and businesses when sales tax is due but has not been collected. If the item is purchased outside of the taxing jurisdiction (i.e. Kodiak City Limits) and the item is sold, leased, or delivered inside the City limits for use, storage, distribution, or consumption. Items purchased over the Internet and no sales tax is charged by the retailer would generate a use tax by the individual / business to be payable to the City.



# Challenges from Alternative Revenue Sources

- Difficult to project an accurate estimate for revenues to ensure that any of these – individually or in the aggregate – would generate sufficient revenue to meet the City’s needs.
- Marijuana estimates are very premature and revenue generated from this source could be 3-5 years in the future.
- Use tax generated from Internet Sales would be difficult to track for compliance.
- Creates the possibility that retailers would move outside the City limits.

# City of Kodiak

## Sales Tax Discussions – Phase IV Planning for the Future

# Climbing the Peaks





## 05.12.2016 – FY 2017 Budget Presentation

- City Infrastructure Needs
  - How to fund aging infrastructure
  - How to fund aging equipment
  - How to maintain the City workforce
- Options for generating revenue –
  - Sales Tax Review



## Upcoming City Needs

- In the next 6 years, \$73.9M in Water, Sewer, and Streets projects that will be required – 1/3 for each fund ~ \$24.6M per fund
  - Streets = General Fund
  - Water = Water Fund generated through Water Rates
  - Sewer = Sewer Fund generated through Sewer Rates
- Fire Department = General Fund ~ \$2.5M
  - Fire Truck (4 Years) = \$1.5M
  - Rescue Apparatus (3 Years) = \$200,000
  - Deferred Maintenance = \$50,000 per year until the new Fire Station has been completed
  - Fire Safety Equipment = \$250,000

## Upcoming City Needs continued

- Finance / IT = General Fund ~ \$750,000
  - Full replacement of Microsoft Office ~\$100,000
  - Full replacement of the Phone System ~\$100,00
  - Secure chip readers for City IDs for computer / IT access ~\$150,000
  - Hardware Equipment ~\$300,00
  - Additional components to Financial Software ~\$100,000
- City Administration = General Fund ~\$250,000
  - Economic Development Plan Implementation ~\$50,000
  - Pending = HR related costs ~\$200,000

## Upcoming City Needs continued

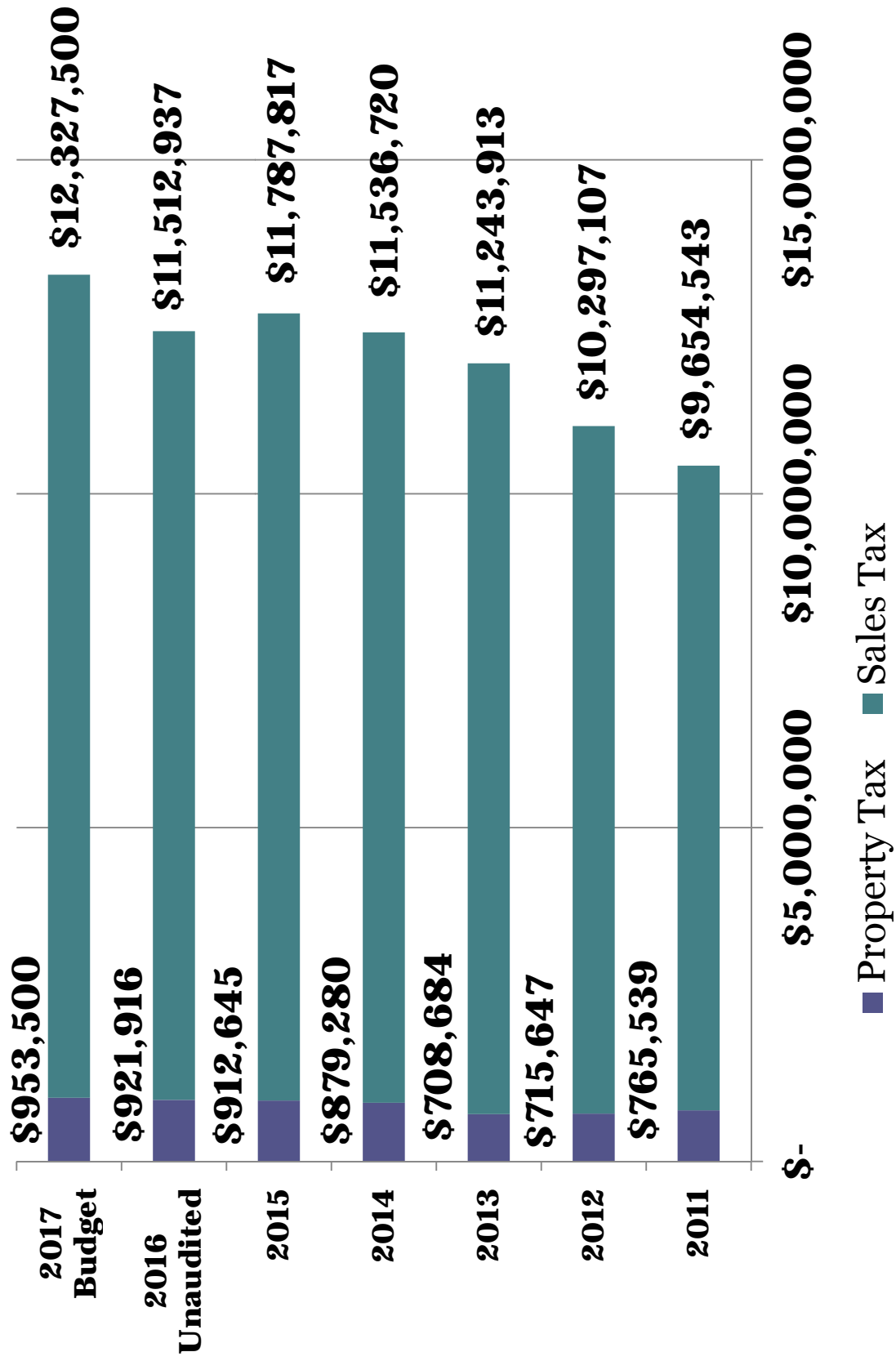
- Parks & Recreation = General Fund ~ \$450,000
  - Teen Center Siding~\$25,000
  - Fencing~\$15,00
  - Parking for Baranof ~\$300,000
  - Ice Rink Scoreboards ~\$10,00
  - Bathroom / Changing Facility ~\$100,000
- City Clerks = General Fund ~\$100,000
  - Records Management software upgrades~\$50,000
  - Additional storage costs ~\$50,000

## Upcoming City Needs continued

- Harbor = Harbor Fund~ \$29,000,000
  - St. Herman Harbor Replacement
- Cargo= Cargo Fund~\$5,000,000
  - Demolition of old Pier III and installation of new dolphins
- Shipyard = Shipyard Fund ~ \$8,000,000
  - Shipyard Building ~\$5,000,000
  - Shipyard pads ~ \$2,000,000

# Sales Tax & Property Tax

Year over Year Comparison



## General Fund Fund Balance

	Unassigned	Assigned	Nonspendable	Total
<b>2010 Actual</b>	<b>5,604,877</b>	<b>1,440,324</b>	<b>1,807,369</b>	<b>8,852,570</b>
<b>2011 Actual</b>	<b>4,865,844</b>	<b>1,638,770</b>	<b>2,117</b>	<b>6,506,731</b>
<b>2012 Actual</b>	<b>5,238,816</b>	<b>1,490,854</b>	<b>-0-</b>	<b>6,729,670</b>
<b>2013 Actual</b>	<b>7,402,867</b>	<b>1,156,232</b>	<b>13,741</b>	<b>8,572,840</b>
<b>2014 Actual</b>	<b>7,461,144</b>	<b>1,132,088</b>	<b>8,885</b>	<b>8,602,117</b>
<b>2015 Actual</b>	<b>6,735,867</b>	<b>3,346,341</b>	<b>16,709</b>	<b>10,098,917</b>
<b>2016</b>				
<b>Unaudited</b>	<b>8,675,304</b>	<b>-0-</b>	<b>24,513</b>	<b>8,699,817</b>
<b>2017 Budget</b>	<b>8,748,006</b>	<b>-0-</b>	<b>15,000</b>	<b>8,763,006</b>

From FY 2012 to FY 2013, there was a sales tax increase and a tax cap increase, therefore greater revenues were achieved thus resulting in an increase in unassigned fund balance.  
 From FY 2014 to FY 2015, there was an increase in grant funds received from governmental sources.  
 From FY 2015 to FY 2016, there was an increase in grant funds received from governmental sources.

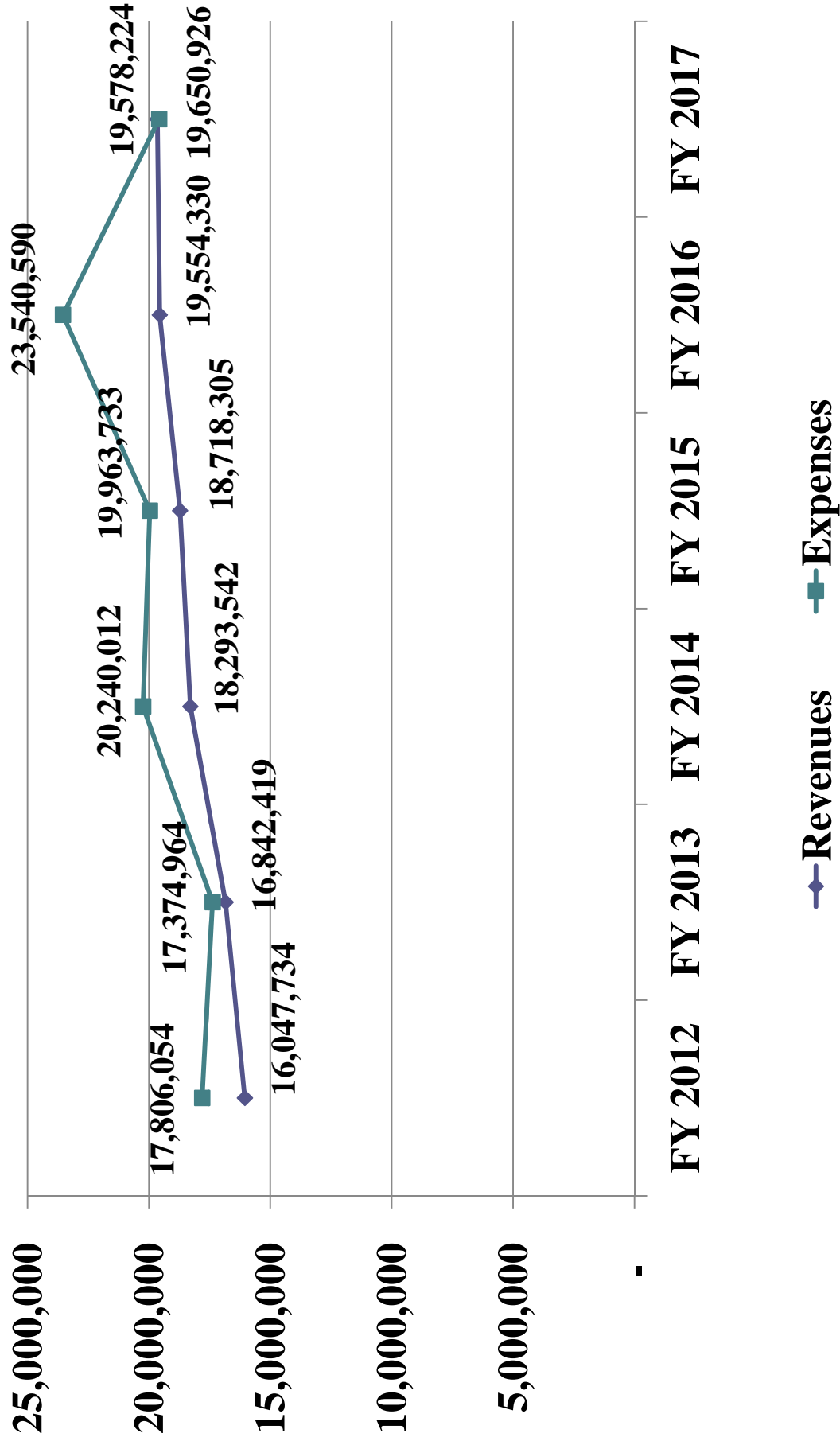
## General Fund - Transfers

	2012 Actual	2013 Actual	2014 Actual	2015 Actual	2016 Unaudited	2017 Budget
Disaster Admin	-0-	-0-	-0-	-0-	7,886	-0-
Enhancement	78,877	111,470	921,584	72,781	748,400	-0-
General Projects	565,500	409,500	927,500	274,000	20,000	-0-
Street Projects*	450,000	450,000	350,000	957,000	2,420,000	450,000
Building Projects	-0-	-0-	-0-	40,000	1,000,000	-0-
Cargo Projects	-0-	-0-	100,000	-0-	-0-	-0-
Harbor Projects*	500,000	500,000	500,000	500,000	500,000	500,000
Parks Projects*	50,000	50,000	50,000	50,000	50,000	20,000
Veh Replacement	21,812	66,882	85,312	98,574	174,663	203,594
Insurance fund	-0-	37,000	-0-	-0-	-0-	-0-
<b>Total</b>	<b>1,666,189</b>	<b>1,624,852</b>	<b>2,934,396</b>	<b>1,992,355</b>	<b>4,920,949</b>	<b>1,173,594</b>

\* Per KCC, \$500,000 is transferred to the Harbor Fund Projects; \$450,000 to the Streets Projects; and \$50,000 to the Parks & Rec projects.

# General Fund Revenue versus Expenses

Year over Year





# Climbing the Peaks



## 07.12.2016 – Exemptions

- Review of all Kodiak City Code general sales tax exemptions
- Assumption 1 – Eliminate Senior Exemption
- Assumption 2 – Add \$200 rebate to Seniors to offset elimination of senior exemption
- Assumption 3 – Add Senior exemption for groceries only to offset elimination of senior exemption
- Assumption 4 – Add senior exemption for groceries and \$200 rebate to offset elimination of senior exemptions

## 07.12.2016 – Exemptions continued

- Assumption 5 – Decrease sales tax rate to 5% on groceries
- Assumption 6 – Decrease sales tax rate to 5% and eliminate the Tax Cap
- Assumption 7 – Decrease sales tax rate to 6% and eliminate the Tax Cap
- Assumption 8 – Decrease sales tax rate to 5% and increase Tax Cap to \$1,500
- Assumption 9 – Decrease sales tax rate to 6% and increase Tax Cap to \$1,500

## 07.12.2016 – Exemptions continued

- Assumption 10 – Decrease sales tax rate to 5% and increase Tax Cap to \$3,000
- Assumption 11 – Decrease sales tax rate to 6% and increase Tax Cap to \$3,000
- Assumption 12 – Sales tax rate remains at 7% and increase Tax Cap to \$1,500
- Assumption 13 – Sales tax rate remains at 7% and increase Tax Cap to \$3,000
- Assumption 14 – Sales tax rate remains at 7% and eliminate Tax Cap

# Climbing the Peaks





## 08.09.2016 – Casual & Isolated Sales / Implementation of City Business Licenses

- Clearly defining casual and isolated sales
  - Draft Ordinance to update City Code
- City Business Licenses
  - Draft Ordinance to update City Code
- Potential Impact to Revenue: Unknown

# Climbing the Peaks





## 09.06.2016 – Alternative Revenue Sources

- Assumption 15 – Additional tax on Alcohol
- Assumption 16 – Additional tax on Tobacco
- Assumption 17 – Additional tax on Marijuana
- Assumption 18 – Internet Sales / Use Tax



# Climbing the Peaks

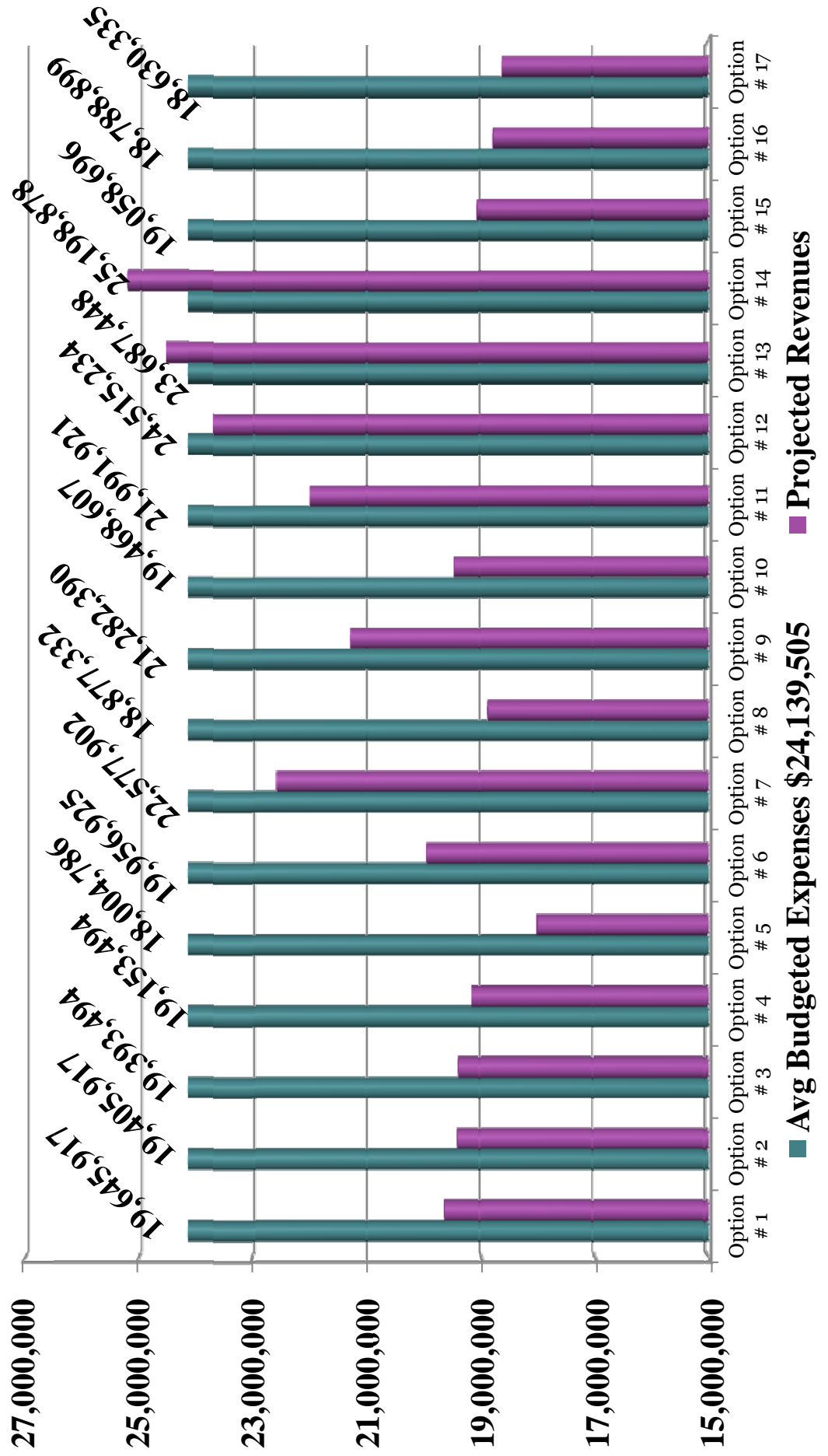




## Future Budget Impacts

- Average Budgeted Revenues – 5 Year Period  
~\$18,611,904
- Average Budgeted Expenses – 5 Year Period  
~\$20,139,505
- Anticipated Increases in Expenses for FY 2018 Budget  
~\$4,000,000
- Potential Increases in Expenses for current year FY 2017  
and future periods ~ \$5,300,000
- **Future Budget Impacts: Anticipated Increase of  
\$4,000,000 in expenses**

# Potential Impacts for Revenues



# Potential Impact to Fund Balance

	Potential Increase / (Decrease)	Projected Revenues (Potential Increase Plus Average Revenues of \$18,611,904)	Average Budgeted Expenses of \$20,139,505 Plus \$4,000,000	Increase / (Decrease) to Fund Balance
# 1 – Elim Senior Exempt	1,034,013	19,645,917	24,139,505	(4,493,588)
# 2 - \$200 Rebate to Srs & Elim Senior Exempt	794,013	19,405,917	24,139,505	(4,733,588)
# 3 – Elim Senior Exempt Except for Groceries	781,590	19,393,494	24,139,505	(4,746,011)
# 4 – Elim Sr Exempt Except for Groceries & \$200 Rebate to Srs	541,590	19,153,494	24,139,505	(4,986,011)
# 5 – Decrease ST Rate to 5% for Groceries Only	(607,118)	18,004,786	24,139,505	(6,134,719)
# 6 – Decrease ST Rate to 5% & Elim Cap	1,345,021	19,956,925	24,139,505	(4,182,580)
# 7 – Decrease ST Rate to 6% & Elim Cap	3,965,998	22,577,902	24,139,505	(1,561,603)
# 8 – Decrease ST Rate to 5% & Incr Cap to \$1,500	265,428	18,877,332	24,139,505	(5,262,173)
# 9 – Decrease ST Rate to 6% & Incr Cap to \$1,500	2,670,486	21,282,390	24,139,505	(2,857,115)

# Potential Impact to Fund Balance

	Potential Increase / (Decrease)	Projected Revenues (Potential Increase Plus Average Revenues of \$18,611,904)	Average Budgeted Expenses of \$20,139,505 Plus \$4,000,000	Increase / (Decrease) to Fund Balance
# 10 – Decrease ST Rate to 5% & Incr Cap to \$3,000	856,703	19,468,607	24,139,505	(4,670,898)
# 11 – Decrease ST Rate to 6% & Incr Cap to \$3,000	3,380,017	21,991,921	24,139,505	(2,147,584)
# 12 – ST Rate Remains at 7% & Incr Cap to \$1,500	5,075,544	23,687,448	24,139,505	(452,057)
# 13 – ST Rate Remains at 7% & Incr Cap to \$3,000	5,903,330	24,515,234	24,139,505	375,729
# 14 – ST Rate Remains at 7% & Elim Cap	6,586,974	25,198,878	24,139,505	1,059,373
# 15 – 5% ST on Alcohol Sales	446,792	19,058,696	24,139,505	(5,080,809)
# 16 – 8% ST on Tobacco Sales	176,995	18,788,899	24,139,505	(5,350,606)
#17 – Estimated Marijuana ST	18,431	18,630,335	24,139,505	(5,509,170)
# 18 – Internet Sales / Use	Unknown	Unknown	Unknown	Unknown

## Option # 19 – Staff Suggestions

- Pass a 5 year ordinance based on the following:
  - Year 1 – increase the sales tax cap to \$3,000
  - Year 2 – eliminate the sales tax cap
  - Year 3 through Year 4– Hold rate at 7% without a cap to evaluate fund balance stabilization
  - Year 4 through Year 5 – evaluate the potential to eliminate sales tax on residential rentals over 30 days (i.e. add an exemption that if a person has a residential rental over 30 consecutive days, then it is exempt from sales tax). Potential decrease in revenues ~ \$500,000 - \$750,000
  - Year 6 through Year 10 – evaluate the stabilization of the fund balance, evaluate aging infrastructure. Begin potential evaluations of lowering the sales tax rate.

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**A<sup>3</sup> Consulting, Creativity, and Communications**

5665 Sapphire Loop • Anchorage, Alaska 99504

(907) 230-2284 • wayers@gci.net

March 1, 2017

Aimée Kniazowski, City Manager  
City of Kodiak  
710 Mill Bay Road  
Kodiak, Alaska 99615

Re: City of Kodiak – Letter of Engagement March 2017

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The City of Kodiak seeks to reestablish its economic development program. In advance of the City Council’s March 21, 2017 work session, A<sup>3</sup> Consulting is engaged to prepare advance materials, develop an initial project implementation timeline, and further detail components described in ***City of Kodiak: Recommendations for Reestablishing an Economic Development Program.***

The scope of this engagement will not exceed 60 hours through March 31, 2017 at a rate of \$75 per hour. This time will be allocated at follows:

Follow -up calls with Council members	5 – 10 hours
Preparation of project timeline	5 hours
Write-up about Business Climate Survey	3 – 5 hours
March 21 work session prep	2 – 5 hours
Follow-up through March 31	25 – 35 hours

At the conclusion of this engagement, the City of Kodiak will have an implementation plan for the preliminary stage of its economic development program.

I enjoyed discussing the community with each council member and am grateful for the opportunity to visit Kodiak again after far too long an absence. Thank you again for the opportunity to work with the City of Kodiak.

**ACCEPTANCE:**

**CITY OF KODIAK**

**A<sup>3</sup> CONSULTING**

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Aimée Kniazowski, City Manager

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Wanetta Ayers

Successful economic development initiatives are driven by a solid foundation of economic analysis as well as continuous public engagement. Both aspects ensure that the effort is embraced broadly and survives political cycles.

The City of Kodiak seeks to harmonize its approaches to economic development. A strategic economic development planning process will help the COK develop and initiate a plan to achieve economic growth. A stepwise approach to organizing, assessing, and prioritizing actions with broad public engagement might include all or some of the following:

Phase	Outcomes	Recommended Engagement
Pre-planning and Visioning	<ul style="list-style-type: none"> <li>• Governance Body named and populated</li> <li>• Vision Statement finalized</li> <li>• Project Plan and Timeline published</li> </ul>	<ul style="list-style-type: none"> <li>• Branded Identity for the Effort</li> <li>• Media Releases</li> <li>• Citizen Survey – simple 3 – 5 Q</li> <li>• Public Meeting</li> <li>• Project Web Page and Social Assets</li> </ul>
Environmental Scan and Assessment	<ul style="list-style-type: none"> <li>• Economic Profile</li> <li>• Business Climate Survey – licensed businesses in COK</li> <li>• Key Informant Interviews – 15 to 20 one-on-one interviews with leaders of key community anchor institutions</li> <li>• Local Sentiments – values, aspirations, and concerns</li> <li>• Competitive Analysis – comparison to three peer communities</li> <li>• SWOT Analysis</li> </ul>	<ul style="list-style-type: none"> <li>• Business interviews and surveys</li> <li>• Citizen survey</li> <li>• Social channels – what’s happening, how citizens can be involved</li> <li>• Public Meeting</li> </ul>
Formulate Strategic Goals and Objectives	<ul style="list-style-type: none"> <li>• 5-year goals and objectives based on ESA</li> </ul>	<ul style="list-style-type: none"> <li>• Public Meeting</li> </ul>
Identify, Evaluate, and Prioritize Projects	<ul style="list-style-type: none"> <li>• Generate list of projects to achieve GSOs</li> <li>• Prioritize projects based on feasibility and ROI</li> </ul>	<ul style="list-style-type: none"> <li>• Governance Body</li> <li>• Public Meeting</li> </ul>
Develop Action Plans	<ul style="list-style-type: none"> <li>• Project Timelines</li> <li>• Project Leads</li> <li>• Project Partners</li> <li>• Evaluation Framework</li> </ul>	<ul style="list-style-type: none"> <li>• Governance Body</li> <li>• Public Meeting</li> <li>• Public Comment Period</li> </ul>
Implementation	<ul style="list-style-type: none"> <li>• Revised Job Description(s)</li> <li>• Professional Services Contract</li> <li>• Ongoing Governance</li> </ul>	<ul style="list-style-type: none"> <li>• Project Updates</li> <li>• Public Reports</li> <li>• Social Postings</li> </ul>

	<ul style="list-style-type: none"> <li>• Ongoing Administration</li> </ul>	
Evaluation Framework and Monitoring	<ul style="list-style-type: none"> <li>• Online Framework</li> <li>• Quarterly/Annual Metrics</li> <li>• Semi-Annual Report to Council</li> </ul>	<ul style="list-style-type: none"> <li>• Public Meeting/Report</li> </ul>

If the COK desires more of a strategic doing approach, some focused effort with the COK Council or a committee designated by the council, possibly with one or more representatives of the Fisheries Work Group, Downtown Revitalization Committee, Destination Kodiak. Chamber of Commerce, and others. Some possible, broad actions include:

**Business Expansion, Retention, and Resilience**

- Discover and address business concerns and pain points;
- Encourage businesses to prepare for natural disasters and economic downturns
- Review ordinances, fee structures, land management, and other assets controlled by the city for policy revisions that will encourage economic growth.

**Marketing and Public Diplomacy**

- Review and harmonize key positioning and marketing efforts with primarily external audiences
- Conduct regular outreach with key markets using a unified branding approach

**Workforce Development**

- Encourage prioritized efforts amongst the school district, Kodiak College, UAF School of Ocean Sciences, and others to ensure secondary and postsecondary education and training to address in-demand jobs that achieve a living wage.

**Infrastructure and Redevelopment**

- Identify planned and prioritized infrastructure investments that are most likely to support economic growth

The implementation of the above would be primarily through partnerships on contracted services. Public engagement would be limited, but include at some level, to ensure a successful effort.

There are many variables that are not addressed here given the limitations to time and lack of formal engagement.

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## AMENDMENT NO. 2

### Professional Services Agreement with McCarty and Associates for Fisheries Analyst Consulting and Related Services

This Amendment No 2 to the professional services agreement with McCarty and Associates for Fisheries Analyst Consulting and Related Services (Amendment No. 2) is made and entered into effective as of March 1, 2017, by and between the KODIAK ISLAND BOROUGH, organized under the laws of the State of Alaska, hereinafter referred to as the Borough; the CITY OF KODIAK, organized under the laws of the State of Alaska, hereafter referred to as the City; and MCCARTY AND ASSOCIATES, a sole proprietor company authorized to do business in Alaska, with offices located at Juneau, Alaska, hereinafter referred to as the Contractor (collectively referred to as the Parties).

#### RECITALS

WHEREAS, the Parties entered into a Professional Services Agreement made as of February 7, 2014, for Contractor to monitor, analyze and report on fisheries issues and policy developments that may impact or affect the economy and community in Kodiak, Alaska; for a two- year term ending on February 6, 2016 (Agreement); and

WHEREAS, the Parties amended the Agreement with Amendment No. 1, effective March 1, 2016, which provided that the Agreement may be extended for one year upon approval by the Borough, City, and Contractor; and

WHEREAS, the Parties desire to amend and extend the Agreement.

NOW, THEREFORE, in consideration of the foregoing recitals, which are hereby incorporated herein by reference, and of the mutual covenants and agreements herein contained, and other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, the Parties agree as follows:

1. **Amendments to Agreement.** The following amendments are made to the Agreement by this Amendment No. 2:

Section 2.0 Term of Agreement is amended in its entirety to read as follows:

This Agreement shall take effect March 1, 2017. This Agreement shall remain in full force and effect for one year, expiring on February 28, 2018. This Agreement may be amended only in writing and upon compliance with all applicable statutes, ordinances, and regulations.

Section 3.0 Fees is amended in its entirety to read as follows:

Contractor will be paid \$4,584 per month. Contractor shall be reimbursed for travel-related costs including airfare, local transportation, lodging and per diem for meals based on the current US Government Services Administration (GSA)

agency schedule when preapproved by the Fisheries Work Group co-chairs. The contractor shall submit monthly invoices, no later than the 10<sup>th</sup> of the following month, detailing work and expenses incurred. The Borough and City will each pay one half of each accurate monthly invoice.

Section 4.0 Scope of Services is amended to read as follows:

The Borough, City, and Contractor have agreed upon a scope of work described in the Contractor's proposal, Exhibit B, to provide Fisheries Analyst Services based on approved standards and instructions. Contractor will attend monthly meetings of the Kodiak Fisheries Work Group via teleconference, unless requested by the co-chairs to attend in person. Contractor will attend monthly Work Sessions of the Assembly and Council via teleconference. Attendance at other meetings may be requested by the Borough/City Fisheries Work Group. Additional Contractor fee for additional meetings (if any) must be agreed to, in writing, and approved by the Borough/City Fisheries Work Group.

In addition, Section 4 C of Exhibit B is amended to read:

Provide written monthly reports to both the Borough Assembly and the City Council and make monthly presentations via teleconference at Borough and City work sessions. Reporting should include discussions or updates of ongoing concerns, introductions of proposed changes, and a summary of work performed since the previous report.

This Scope of Services can only be changed in writing pursuant to Section 25.0 of the original Agreement.

Section 5.0 Report Schedule is amended in its entirety to read as follows:

Written monthly reports shall be provided to the Borough and City to be presented at monthly Borough Assembly and City Council work sessions and scheduled Fisheries Work Group meetings. Contractor shall also provide monthly fisheries updates at Borough and City work sessions orally via teleconference. Contractor shall report in writing and, orally via teleconference, to the Borough/City Fisheries Work Group on each fisheries meeting attended. Contractor shall attend telephonically and report to Borough/City Joint Work Sessions when requested by the Borough Assembly and City Council. Joint Work Sessions are anticipated to occur on a quarterly basis.

Section 6.0 Personnel/Organization is amended in its entirety to read as follows:

Key Personnel. Fisheries Analyst Services provided by the Contractor will be performed by **Heather McCarty**.

Changes in Key Personnel. The Contractor shall give the Borough and City, through notice to the Contracting Officers, reasonable advance notice of any necessary substitution or change of key personnel and shall submit justification

therefore in sufficient detail to permit the Borough and City to evaluate the impact of such substitution on this Agreement. No substitutions or other changes shall be made without the written consent of the Borough and City.

The contact person(s) on issues related to agenda setting and fisheries related issues of the Joint Fisheries Work Group, or direction on topics identified in the contract scope of work are the current co-chairs of the Fisheries Work Group.

The contact person(s) on the administration of this contract are the City Manager and Borough Manager.

Contractor will provide the Managers a listing of clients that are currently contracting with the Contractor. Any new clients will be divulged to the Managers.

Section 9.0 Compliance With Laws is amended in its entirety to read as follows:

The Contractor shall be familiar with and at all times comply with and observe all applicable federal, state and local laws, ordinances, rules, regulations, and executive orders, all applicable safety orders, all orders or decrees of administrative agencies, courts, or other legally constituted authorities having jurisdiction or authority over the Contractor, the Borough, the City or the service which may be in effect now or during performance of the services. Contractor shall maintain a current State of Alaska business license and shall provide a copy to both the Borough and City.

Section 11.0 Insurance is amended in its entirety to read as follows:

The Contractor understands that no Borough or City insurance coverage, including Workers' Compensation, is extended to the Contractor while completing the services described in this Agreement. The Contractor shall carry adequate (commercially reasonable coverage levels) insurance covering Workers' Compensation (if Contractor has employees), general public liability, automobile, and property damage including a contractual liability endorsement covering the liability created or assumed under this Agreement. The Contractor shall not commence work under this Agreement until the Contractor provides the Borough and City with certificates of insurance evidencing that all required insurance has been obtained. Contractor shall provide an updated insurance certificate at least ten days before expiration. These insurance policies and any extension or renewals thereof must contain the following provisions or endorsements:

Borough and City are additional insured hereunder as respects liability arising out of or from the work performed by Contractor.

Borough and City will be given thirty (30) days prior notice of cancellation or material alteration of any of the insurance policies specified in the certificate.

Insurer waives all rights of subrogation against Borough and City and their employees or elected officials.



The insurance coverage is primary to any comparable liability insurance carried by the Borough and City.

Upon request, Contractor shall permit the Borough and City to examine any of the insurance policies specified herein. Any deductibles or exclusions in coverage will be assumed by the Contractor, for account of, and at the sole risk of the Contractor.

The minimum amounts and types of insurance provided by the Contractor shall be subject to revision at the Contracting Officers' request in order to provide continuously throughout the term of the Agreement a level of protection consistent with good business practice and accepted standard of the industry.

Section 28.0 Notices is amended in its entirety to read as follows:

Any notices, bills, invoices, or reports required by the Agreement shall be sufficient if sent by the parties by electronic mail or by United States mail, postage paid, to the addresses noted below:

Kodiak Island Borough  
Attn: Borough Manager  
710 Mill Bay Road, Room 125  
Kodiak, AK 99615  
mpowers@kodiakak.us

McCarty and Associates  
Attn: Heather McCarty  
1537 Pine Street  
Juneau, AK 99801  
hdmccarty@gmail.com

City of Kodiak  
Attn: City Manager  
710 Mill Bay Road, Room 113  
Kodiak, AK 99615  
mtvenge@city.kodiak.ak.us

2. **Effect and Continuation of Agreement Terms.** Except as specifically modified herein, all of the terms, provisions, covenants and conditions of the Agreement continue in full force and effect without modification or change. The Parties hereby covenant, ratify and reaffirm each and every of their respective obligations under the Agreement as amended by this Amendment No. 2.
3. **Effective Date.** This Amendment No. 2 is effective as of March 1, 2017.
4. **Authority.** The Parties represent and warrant to each other that each has the full, complete and absolute authority to enter into this Amendment No. 2 that this Amendment No. 2 has been duly authorized by its local governing body or owners; that the person executing this Amendment No. 2 on its behalf has the full power and authority to do so; and this Amendment No. 2 is binding and enforceable against it in accordance with its terms.

5. **Counterparts.** For the convenience of the Parties hereto, this Amendment No. 2 may be executed, including by facsimile signature, in one or more counterparts, each identical to the other, so long as the counterparts in a set contain the signatures of all the Parties to this Amendment.

**IN WITNESS WHEREOF**, the parties have entered into this Amendment No. 2 effective as of the date and year hereinabove first written.

KODIAK ISLAND BOROUGH

\_\_\_\_\_  
By: Michael Powers  
Title: Borough Manager

\_\_\_\_\_  
Date

ATTEST:

\_\_\_\_\_  
Nova Javier, MMC  
Borough Clerk

CITY OF KODIAK

\_\_\_\_\_  
By: Mike Tvenge  
Title: Acting City Manager

\_\_\_\_\_  
Date

ATTEST:

\_\_\_\_\_  
Debra Marlar, MMC  
City Clerk

**McCarty and Associates**

\_\_\_\_\_  
By: Heather McCarty  
Title: Owner

State of Alaska  
Third Judicial District

The foregoing Instrument was acknowledged before me by \_\_\_\_\_  
on this \_\_\_\_\_ day of \_\_\_\_\_ 2017.

L.S.

\_\_\_\_\_  
Notary Public in and for Alaska  
My Commission Expires:

**Professional Services Agreement with  
McCarty and Associates  
for Fisheries Analyst Consulting and Related Services**

This **AGREEMENT**, made and entered into this **7<sup>th</sup>** day of **February, 2014** by and between the **KODIAK ISLAND BOROUGH**, organized under the laws of the State of Alaska, hereinafter referred to as the "**Borough**", the **CITY OF KODIAK**, organized under the laws of the State of Alaska, hereafter referred to as the "**City**" and **MCCARTY AND ASSOCIATES** a sole proprietor company authorized to do business in Alaska, with offices located at Juneau, Alaska, hereinafter referred to as the "**Contractor.**"

**WITNESSETH**

**WHEREAS**, the Borough and City wish to enter into a contract with an independent contractor to monitor, analyze and report on fisheries issues and policy developments that may impact or affect the economy and community in Kodiak, Alaska; and

**WHEREAS**, in response to a request for proposals, Contractor submitted a proposal asserting it is qualified to perform these services and able to do so in a timely manner;

**NOW, THEREFORE**, in consideration of the mutual promises and covenants contained herein, the parties agree as follows:

**1.0 DEFINITIONS**

1.1 "Agreement" shall mean this Professional Services Agreement, including:

Exhibit A – McCarty & Associates proposal dated January 21, 2014

Exhibit B – Borough and City's request for proposals

1.2 "Change Order" is an addition to, or reduction of, or other revision approved by the Borough and City in the scope, complexity, character, or duration of the services or other provisions of this Agreement.

1.3 "Borough" shall mean the Kodiak Island Borough, Alaska.

1.4 "Borough/City Fisheries Work Group" is a sub-committee of the Borough Assembly and City Council consisting of three Assembly members and three Council members (KIB Assembly Resolution FY2013-32).

1.5 "City" shall mean the City of Kodiak, Alaska.

- 1.6 "Contracting Officers" shall mean Borough Manager and the City Manager, and include any successor or authorized representatives.
- 1.7 "Contractor" shall mean McCarty and Associates.
- 1.8 "Fisheries Analyst Services" shall include monitoring, analyzing, and reporting to the Borough and City on fisheries issues and policy developments that may impact or affect the economy and communities with the City and the Borough, as further described in the Borough's and City's request for proposals (Exhibit B) and Contractor's proposal (Exhibit A).

**2.0 TERM OF AGREEMENT.** This Agreement shall take effect on February 7, 2014. This Agreement shall remain in full force and effect for two years expiring on February 6, 2016. This Agreement may be extended for two one year options to extend upon approval by the Borough, City and Contractor. This Agreement may be amended only in writing and upon compliance with all applicable statutes, ordinances, and regulations.

**3.0 FEES.** Contractor will be paid \$5,000 per month, at the beginning of each month, and reimbursed for reasonable travel-related costs including airfare, local transportation, lodging and per diem for meals based on the current US Government Services Administration (GSA) agency schedule. The Contractor will submit monthly invoices, detailing work and expenses incurred. The Borough and City will each pay one half of each accurate monthly invoice.

**4.0 SCOPE OF SERVICES.** The Borough, City, and Contractor have agreed upon a scope of work described in the Contractor's proposal, Exhibit A, to provide Fisheries Analyst Services based on approved standards and instructions [as specifically described in Exhibits A and B.] When available, Contractor will also attend local meetings of the Kodiak Fisheries Advisory Committee (KFAC) and the Kodiak Regional Aquaculture Association (KRAA). Attendance at other meetings may be requested by the Borough/City Fisheries Work Group. Additional Contractor fee for additional meetings (if any) must be agreed to, in writing, and approved by the Borough/City Fisheries Work group.

This Scope of Services can only be changed in writing pursuant to Section 25.0 of this Agreement.

**5.0 REPORT SCHEDULE.** Written quarterly reports shall be provided to the Borough and City within 30 days following the end of each calendar quarter. Contractor shall also report, written and/or oral, to the Borough/City Fisheries Sub-committee after each fisheries meeting attended and attend Borough/City Joint Work Sessions when requested by the Borough/City Fisheries Sub-committee. Joint work sessions are anticipated to occur on a quarterly basis.

**6.0 PERSONNEL/ORGANIZATION**

6.1 Key Personnel. Fisheries Analyst Services provided by the Contractor will be performed by:

**Heather McCarty**

6.2 Changes in Key Personnel. The Contractor shall give the Borough and City, through notice to the Contracting Officers, reasonable advance notice of any necessary substitution or change of key personnel and shall submit justification therefore in sufficient detail to permit the Borough and City to evaluate the impact of such substitution on this Agreement. No substitutions or other changes shall be made without the written consent of the Borough.

**7.0 STANDARD OF PERFORMANCE.** The Contractor agrees to use its best efforts to provide Fisheries Analyst Services. The Contractor accepts the relationship of trust and confidence established between it and the Borough and City by this Agreement. The Contractor covenants with the Borough and City to furnish its best skill and judgment. The Contractor shall provide all services in a competent manner.

**8.0 TIMELINESS OF PERFORMANCE.** Time is of the essence in this Agreement.

**9.0 COMPLIANCE WITH LAWS.** The Contractor shall be familiar with and at all times comply with and observe all applicable federal, state and local laws, ordinances, rules, regulations, and executive orders, all applicable safety orders, all orders or decrees of administrative agencies, courts, or other legally constituted authorities having jurisdiction or authority over the Contractor, the Borough, or the service which may be in effect now or during performance of the services.

**10.0 INDEMNITY.** The Contractor shall indemnify, defend, and hold harmless the Borough and City from and against any claim of, or liability for, negligent acts, errors, and omissions of the Contractor under this Agreement, including attorney fees and costs. The Contractor is not required to indemnify, defend, or hold harmless the Borough or City for a claim of, or liability for, its (the Borough or City, as applicable) independent negligent acts, errors, and omissions. If there is a claim of, or liability for, a joint negligent act, error, or omission of the Contractor

and the Borough and City, the indemnification, defense, and hold harmless obligation of the Contractor, and liability of the parties, shall be apportioned on a comparative fault basis. In this provision, "Contractor", "Borough" and "City" include the employees, agents, and contractors who are directly responsible, respectively, to each. In this provision, "independent negligent acts, errors, and omissions of the Borough and City means negligence other than in the Borough's and City's selection, administration, monitoring, or controlling of the Contractor, or in approving or accepting the Contractor's work.

**11.0 INSURANCE.** The Contractor understands that no Borough or City insurance coverage, including Workers' Compensation, is extended to the Contractor while completing the services described in this Agreement. The Contractor shall carry adequate (commercially reasonable coverage levels) insurance covering Workers' Compensation, general public liability, automobile, professional liability, and property damage including a contractual liability endorsement covering the liability created or assumed under this Agreement. The Contractor shall not commence work under this Agreement until the Contractor provides the Borough and City with certificates of insurance evidencing that all required insurance has been obtained. These insurance policies and any extension or renewals thereof must contain the following provisions or endorsements:

- a. Borough and City are additional insured thereunder as respects liability arising out of or from the work performed by Contractor.
- b. Borough and City will be given thirty (30) days prior notice of cancellation or material alteration of any of the insurance policies specified in the certificate.
- c. Insurer waives all rights of subrogation against Borough and City and their employees or elected officials.
- d. The insurance coverage is primary to any comparable liability insurance carried by the Borough and City.

Upon request, Contractor shall permit the Borough and City to examine any of the insurance policies specified herein. Any deductibles or exclusions in coverage will be assumed by the Contractor, for account of, and at the sole risk of the Contractor.

The minimum amounts and types of insurance provided by the Contractor shall be subject to revision at the Contracting Officers' request in order to provide continuously throughout the term of the Agreement a level of protection consistent with good business practice and accepted standard of the industry.

**12.0 GOVERNING LAW.** The laws of Alaska will determine the interpretation, performance and enforcement of this Agreement.

**13.0 OWNERSHIP OF WORK PRODUCTS.** Payment to the Contractor for services hereunder include full compensation for all work products and other materials produced by the Contractor pertaining to this Agreement.

The originals of all material prepared or developed by the Contractor or its employees, agents, or representatives hereunder, including documents, drawings, designs, calculations, maps, sketches, notes, reports, data, models, computer tapes, and samples shall become the property of the Borough and City when prepared, whether delivered or not, and shall, together with any materials furnished the Contractor and its employees, agents, or representatives by the Borough and City hereunder, be delivered to the Borough and City upon request and, upon termination or completion of this Agreement. Materials previously created and copyrighted by the Contractor included in this project will remain property of the Contractor. Copies will be made available to the Borough and City upon request. Materials purchased from and copyrighted by third parties are not included in this provision.

**14.0 PATENTS, TRADEMARKS, AND COPYRIGHTS.** The Contractor agrees to defend, indemnify, and save the Borough and City harmless from and against any and all claims, costs, royalties, damages and expenses of any kind of nature whatsoever (including attorneys' fees) which may arise out of or result from or be reasonably incurred in contesting any claim that the methods, processes, or acts employed by the Contractor or its employees in connection with the performance of services hereunder infringes or contributes to the infringement of any letter patent, trademark, or copyright. In case such methods, processes, or acts are in suit held to constitute infringement and use is enjoined, the Contractor, within reasonable time and at its own expense, will either secure a suspension of the injunction by procuring for the Borough and City a license or otherwise, or replace such method, process, etc., with one of equal efficiency.

**15.0 NONWAIVER.** No failure of the Borough, City or Contractor to insist upon the strict performance by the other of any of the terms of this Agreement or to exercise any right or remedy herein conferred, shall constitute a waiver or relinquishment to any extent of its rights to rely upon such terms or rights on any future occasion. Each and every term, right, or remedy of this Agreement shall continue in full force and effect.

**16.0 SAFETY/PERFORMANCE.** The Contractor shall comply with all federal and state statutes, ordinances, orders, rules, and regulations pertaining to the protection of workers and the public from injury or damage, and shall take all other reasonable precautions to protect workers and the public from injury or damage.

## **17.0 SUSPENSION OR TERMINATION.**

17.1 Fault Termination or Suspension. This Agreement may be terminated by any party upon ten (10) days written notice if another party fails substantially to perform in accordance with its terms. If the Borough or City terminates this Agreement, they will pay the Contractor a sum equal to the percentage of work completed and accepted that can be substantiated by the Contractor, offset by any amounts owed to the Borough or City. However, within the ten (10) day Notice of Intent to terminate the party in default shall be given an opportunity to present a plan to correct its failure.

17.2 Convenience Suspension or Termination. Any party may at any time terminate or suspend this Agreement upon 30 days' prior written notice to each of the other parties, for any reason including its own needs or convenience. In the event of a convenience termination or suspension for more than six (6) months, the Contractor will be compensated for authorized services and authorized expenditures performed to the date of termination or suspension. No fee or other compensation for the uncompleted portion of the services will be paid, except for already incurred indirect costs which the Contractor can establish and which would have been compensated but because of the termination or suspension would have to be absorbed by the Contractor without further compensation.

17.3 Activities Subsequent to Receipt of Notice of Termination or Suspension. Following receipt of a Notice of Termination or suspension and except as otherwise directed by the Contracting Officers, the Contractor shall:

- a. perform only work authorized under this Agreement through the termination or suspension date and to the extent specified in the Notice; and
- b. deliver in the manner, at the times, and to the extent directed by the Contracting Officers, work in progress, completed work, supplies, and other material produced as a part of, or acquired in respect of the performance of the work terminated or suspended by the Notice.

**18.0 EQUAL EMPLOYMENT OPPORTUNITY.** The Contractor shall not discriminate against any employee or applicant for employment because of race, religion, color, national origin, or because of age, physical handicap, sex, marital status, change in marital status, pregnancy, or parenthood when the reasonable demands of the position do not require distinction on the basis of age, physical handicap, sex, marital status, changes in marital status, pregnancy, or parenthood. The Contractor shall take affirmative action required by law to ensure that applicants are employed and that employees are treated during



employment without regard to their race, color, religion, national origin, ancestry, age, or marital status.

**19.0 NO ASSIGNMENT OR DELEGATION.** The Contractor may not assign, subcontract or delegate this Agreement, or any part of it, or any right to any of the money to be paid under it without written consent of the Contracting Officers.

**20.0 INDEPENDENT CONTRACTOR.** The Contractor shall be an independent contractor in the performance of the work under this Agreement, and shall not be an employee or agent of the Borough or of the City.

**21.0 PAYMENT OF TAXES.** As a condition of performance of this Agreement, the Contractor shall pay all federal, state and local taxes incurred by the Contractor and shall require their payment by any other persons in the performance of this Agreement.

**22.0 PRECEDENCE AND DIVISIBILITY.** The provisions of this Agreement shall fully govern the services performed by the Contractor. If any term, condition, or provision of this Agreement is declared void or unenforceable, or limited in its application or effect, such event shall not affect any other provisions hereof and all other provisions shall remain fully enforceable.

**23.0 ENTIRE AGREEMENT.** This Agreement contains the entire agreement between the parties as to the services to be rendered by the Contractor. All previous or concurrent agreements, representations, warranties, promises, and conditions relating to the subject matter of this Agreement are superseded by this Agreement.

**24.0 CLAIMS AND DISPUTES.** Venue for all claims and disputes under this Agreement, if not otherwise resolved by the parties, shall be in the appropriate Alaska State court in Anchorage or Kodiak, Alaska.

**25.0 CHANGES IN SCOPE OF WORK.**

25.1 General. Additional services not specifically provided for in this Agreement will not be compensated.

25.2 Changes in Scope of Work. The Contracting Officers may, at any time, by a written Change Order delivered to the Contractor, make changes to the scope of work, or authorize additional work outside the scope of work to the extent authorized by Borough and City appropriations.

25.3 Compensation to the Contractor. If any Change Order for which compensation is allowed under this Article causes an increase or decrease in the estimated cost of, or time required for, the performance of

any part of the work under this Agreement, or if such change otherwise affects other provisions of this Agreement, an equitable adjustment will be negotiated. Such an adjustment may be:

- a. in the estimated cost or completion schedule, or both;
- b. in the amount of fee to be paid; and
- c. in such other provisions of the Agreement as may be affected, and the Agreement shall be modified in writing accordingly.

25.4 Any claim by the Contractor for adjustment under this section must be asserted within fifteen (15) days from the day of receipt by the Contractor of the notification of change; provided, however, that the Contracting Officers, deciding that the facts justify such action, may receive and act upon any such claim asserted at any time prior to final payment under this Agreement. Failure to agree to any adjustment shall be a dispute within the meaning of Section 2540 of this Agreement.

## **26.0 LIMITATION OF FUNDS.**

26.1 At no time will any provision of this Agreement make the Borough or City liable for payment for performance of work under this Agreement in excess of the amount that has been appropriated by the Borough Assembly (for the Borough) or City Council (for the City) and obligated for expenditure for purposes of this Agreement.

26.2 Change orders issued pursuant to Section 25 of this Agreement shall not be considered an authorization to the Contractor to exceed the amount allotted in the absence of a statement in the change order, or other modification increasing the amount allotted.

26.3 Nothing in this Section shall affect the right of the Borough and City under Section 17 to terminate this Agreement.

**27.0 PRIOR WORK.** For the purposes of this Agreement, work done at the request of the Borough and City before execution of this Agreement, if any, shall be deemed to be work done after its execution and shall be subject to all the conditions contained herein.

**28.0 NOTICES.** Any notices, bills, invoices, or reports required by the Agreement shall be sufficient if sent by the parties by electronic mail or by United States mail, postage paid, to the addresses noted below:

Kodiak Island Borough  
Attn: Borough Manager  
710 Mill Bay Road, Room 125  
Kodiak, AK 99615  
[bcassidy@kodiakak.us](mailto:bcassidy@kodiakak.us)

McCarty and Associates  
Attn: Heather McCarty  
1537 Pine Street  
Juneau, AK 99801  
[hdmccarty@gmail.com](mailto:hdmccarty@gmail.com)

City of Kodiak  
Attn: City Manager  
710 Mill Bay Road  
Kodiak, AK 99615  
[akniaziowski@city.kodiak.ak.us](mailto:akniaziowski@city.kodiak.ak.us)

IN WITNESS WHEREOF, the parties have executed this Agreement.

**Kodiak Island Borough**

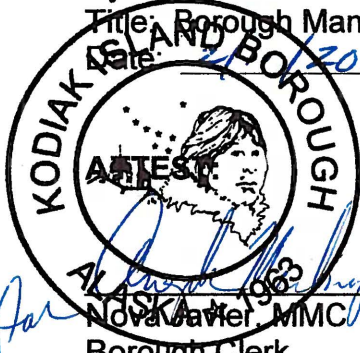
**McCarty and Associates**

*Charles E. Cassidy Jr*

By: Charles E. Cassidy Jr  
Title: Borough Manager  
Date: 1/30/14

*Heather D. McCarty*

By: Heather McCarty  
Title: Owner  
Date: 1/30/14



*Debra Marlax*  
Date: 1/30/14  
Title: MMC  
Borough Clerk

(Borough seal)

**City of Kodiak**

*Aimée Kniaziowski*

By: Aimée Kniaziowski  
Title: City Manager  
Date: 2/9/14

**ATTEST:**

*Debra Marlax*

Debra Marlax, MMC  
City Clerk



(City seal)

January 21, 2014

Kodiak Island Borough  
Office of the Manager  
710 Mill Bay Road  
Kodiak, AK 99615

Dear Mr. Cassidy:

I am submitting this proposal in response to the Request for Proposals for the services of a Fisheries Analyst for the Kodiak Island Borough and the City of Kodiak. Along with this transmittal letter I attach the required Implementation Plan, my company profile and work experience and credentials in the form of an expanded CV, my proposed fee, and four references.

It is with great pleasure and anticipation that I submit a proposal to work with the community leaders and people of Kodiak. I am a 35-year resident of coastal Alaska fishing communities, from a commercial salmon fishing family, and a true believer in the need for reasonable and informed local decision-making to sustain and develop this state's fisheries resources. Although I have never lived in Kodiak, I have been visiting your beautiful island regularly for more than 20 years, as a long-time member of the Policy Council for your "Fish Tech" Center, as a faithful ComFish attendee, as a member of the North Pacific Council "family," and as a consultant for a Kodiak processor who works with all species.

My experience and familiarity with the local commercial and sport fisheries has been augmented by ten years of work with a community development group in the Bering Sea, where I gained an appreciation for the importance of subsistence fisheries to the Alaska Native cultures. The long association with the University of Alaska School of Fisheries and Ocean Sciences in both an advisory capacity and as an administrator of an industry-funded marine research program has given me insight into the important role of science in sustaining fisheries. I also served for three years as the fishery representative on the North Pacific Research Board, advocating for research in support of pressing fishery management issues.

As to current experience in the fishery regulatory process, I have attended every meeting of the North Pacific Fisheries Management Council (NPFMC) for the last 15 years, serving on a Council committee, making testimony and moving forward a number of important Council actions on behalf of employers and clients. I have also been to every regular meeting of the International Pacific Halibut Commission (IPHC) for the last six years, and have served on its Processor Advisory Group during that period. I cut my regulatory teeth on salmon issues when I worked with the salmon industry in Prince William Sound, moving actions through the Board of Fisheries (BOF) and the Department of Fish and Game. I have not spent a lot of time in the last few years at the BOF, but I am very familiar with that process.

Because of my varied experiences, I understand fishery resource management issues in Alaska, from several different perspectives: harvesting, processing, aquaculture, marketing, Alaska Native culture, sustainability, research and education. I have participated in formal and informal bodies, boards and commissions at all levels, and am able to communicate well in those settings.

I am very conversant with the State Constitution and the State and Federal fishery regulations, and with the processes that create them. The Magnuson Stevens Act with its national standards has been in my briefcase for years, and is dog-eared from use. I have taken a national course in the NEPA regulations that guide Federal regulatory decision-making.

One of my personal and professional hallmarks is fairness and honesty; I hope that conversations with my references will bring that out. Because I have represented many different sectors and interests, and have lived in and focused on fishing communities, I really do perceive and approach issues from multiple angles – and always with the interests of the community uppermost. I believe I can offer unbiased fisheries analysis and advice to the community leaders of Kodiak.

It is clear to me that some might consider my work with a Kodiak processor a conflict of interest with representing the City and Borough of Kodiak. If I am asked to work with Kodiak, I am prepared to immediately and completely end that professional relationship. My other client relationships, in my opinion, could not be judged a conflict of interest.

Regarding technical considerations, I do have property and car insurance, including liability insurance, and can provide that information. I do not have professional liability insurance, as it has not been required in other contracts and it is a considerable expense. I am in the process of renewing my Alaska business license.

I believe I have provided all of the required information – but if you have any questions or need additional information please call or email me any time. I look forward to hearing from you, and to serving the people of Kodiak.

Best regards,

  
Heather McCarty

## **Request for Proposals: FISHERIES ANALYST**

### **1. AUTHORITY**

- A. The authority to issue this Request for Proposal (RFP) is granted under KIBC 3.30.110 Competitive sealed proposals – Negotiated procurement.

### **2. PURPOSE AND INTENT**

- A. It is the primary intent of this RFP is to select a qualified firm or individual to act as a Fisheries Analyst. The Fisheries Analyst will work jointly for the local governments of Kodiak Island: the Kodiak Island Borough (KIB) and the City of Kodiak (City). The selected proposer will enter into a term contract for a period of two years with options for two additional one year extensions. The term contract will include a six month initial review to ensure an understanding of the Scope. The contract will also include a termination clause by either party with a 30 day notice.
- B. The work shall be performed in compliance with all applicable federal, state and local requirements. The KIB and City require all work to be performed efficiently, cost effectively, and according to best business practices of the industry.

### **3. BACKGROUND/SITUATION**

The economy of the Kodiak region is based on a healthy, diverse and well-managed fisheries resource that includes more than 30 species of fish harvested and processed onshore. The Kodiak region comprises all fisheries user groups -- subsistence, sportfish, commercial sportfish, personal use, commercial fish, the processing sector, fisheries research and protection sectors, and the fishing industry support sector. Policy development and regulatory management in the Kodiak region is challenging because of the need to address such diverse and competing concerns as species fished, vessel size, gear type, limited entry permits, quota share, limited license permits, and crew share interests.

### **4. SCOPE (aim or purpose)**

The primary role of the Fisheries Analyst will be to monitor, analyze and report on fisheries issues and policy developments that may impact or affect the economy and community of Kodiak.

- A. Monitor actions and pending decisions of regulatory bodies such as the NOAA/National Marine Fisheries Service, the State of Alaska Department of Fish and Game, and International Pacific Halibut Commission to anticipate regulatory actions that could impact Kodiak's economy.
- B. Attend meetings of the North Pacific Fisheries Management Council, State of Alaska Board of Fisheries, and other fishery meetings as directed, to

provide a summary of issues, discussion and actions to the Assembly/City Council that could impact Kodiak's economy.

- C. Provide written quarterly report to both the Borough Assembly and the City Council and make presentations at joint Borough Assembly/City Council work sessions. Reporting should include discussions or updates of ongoing concerns, introductions of proposed changes, and a summary of work performed since the previous report.
- D. Prepare analyses of potential impacts of proposed regulatory changes on the Kodiak economy.
- E. Provide information to Assembly/Council to analyze and understand issues based on facts and from a neutral point of view, and to make decisions or develop positions on issues affecting or impacting the economy and people of Kodiak.
- F. The successful proposer should possess the following qualifications:
  - 1. An understanding of local and regional fisheries to include species, vessel and gear types, fisheries user groups -- subsistence, sportfish, commercial sportfish, personal use and commercial fish, and the processing sector.
  - 2. A thorough understanding of the State constitution regarding Alaska's fisheries and all state and federal fisheries laws and regulations.
  - 3. An understanding of fisheries resource management issues and how they affect Kodiak's seafood industry and economy.
  - 4. Documented recent experience in state and federal fisheries resource management processes.
  - 5. Current or recent involvement in the fisheries regulatory process in both the State and Federal environment, specifically the North Pacific Fisheries Management Council, IPHC and AK Department of Fish and Game, Board of Fish.
  - 6. Familiarity with the Magnuson Stevens Act and how the national standards pertain to community fisheries.
  - 7. Ability to communicate effectively with local government bodies and to provide written reports both to and on behalf of the governing bodies.
  - 8. The ability to remain neutral on divisive issues.
  - 9. The ability to work well with boards and commission at the federal, state and local levels.
  - 10. Ability to provide applicable insurance coverage, such as workers' compensation, commercial automobile liability and professional liability.
  - 11. Possession of a State of Alaska Business License.



In the event that the proposer does not possess the above qualifications, the proposer must include a plan for addressing the lack of experience or qualification.

Additionally, if a proposer possesses a perceived conflict of interest, the proposer must also include a discussion about how the conflict of interest will be avoided

- G. The successful proposer will enter into a professional services contract that must be approved by the Kodiak Island Borough Assembly and Kodiak City Council.

## 5. REQUIRED INFORMATION

- A. Proposals must include:
- Transmittal Letter
  - Implementation Plan
  - Company Profile
  - Experience
  - Credentials
  - Proposed Fee
  - References
  - Ability to provide applicable insurance coverage

## 6. PROPOSAL PROCESS

- A. Proposals will be accepted until 3:00 PM on Tuesday, January 21, 2014. It is the proposer's responsibility to deliver proposals to:

Kodiak Island Borough  
Office of the Manager  
710 Mill Bay Road  
Kodiak, AK 99615  
(907) 486-9301

Faxed and electronic proposals will not be accepted.

- B. Proposals must be clearly marked: **FISHERIES ANALYST SERVICES PROPOSAL**

Provide six unbound copies of the proposal.

C. Evaluation Methodology: Each proposal will be evaluated according to criteria and given the relative weight shown in the table below.

EVALUATION CRITERIA	RELATIVE WEIGHT
PROPOSED RATES	25%
QUALIFICATIONS/EXPERIENCE	25%
IMPLEMENTATION PLAN	25%
REFERENCES	15%
OVERALL QUALITY OF PROPOSAL	10%

A recommendation of the top respondent will be made based on the results of reference checks, qualifications of the firm and response to the Scope of service provided. This will be submitted to the Kodiak Island Borough Assembly and the City of Kodiak City Council for approval and award.

D. Miscellaneous

The KIB and City reserve the right to reject or accept any or all bids, to waive irregularities or informalities in the proposal, and to give particular attention to the qualifications of the Proposer.

KIB and City reserve the right to issue written addenda to revise or clarify the RFP, respond to questions, and/or extend or shorten the due date of proposals.

KIB and City retain the right to cancel the RFP process if it is in their best interest. Any cost incurred by proposers for the preparation and submittal of the proposal is the sole responsibility of the proposer.

A proposal may be corrected or withdrawn by a written request received prior to the deadline for receipt of proposals.

All proposals become part of the public record and no part of any proposal may be confidential.

All proposals and other material submitted become KIB and City property and may be returned only at their option.

KIB and City assume no responsibility or liability for the transmission, delay, or delivery of proposals by either public or private carriers.

Any and all media announcements pertaining to this RFP require KIB and City's prior written approval.

This RFP does not obligate KIB and City or the selected proposer until a contract is signed and approved by all parties.

Contact Borough Manager Bud Cassidy at (907) 486-9302 or at [bcassidy@kodiak.us](mailto:bcassidy@kodiak.us) with questions regarding this solicitation.

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# Kodiak Pedestrian Pathway

## PLANNING AND DESIGN

**Draft Pedestrian  
Pathways Plan for  
Public Review**  
February 2017



Prepared For: City of Kodiak  
Prepared By: DOWL

# Kodiak Pedestrian Pathway

PLANNING AND DESIGN



Prepared For:  
City of Kodiak  
Public Works Department  
2410 Mill Bay Road  
Kodiak, Alaska 99315



Prepared By:  
DOWL  
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## List of Acronyms

ADF&G	Alaska Department of Fish and Game
BIA	Bureau of Indian Affairs
COK	City of Kodiak
DOT&PF	Department of Transportation and Public Facilities (State of Alaska)
FLH	Office of Federal Lands Highway
FHWA	Federal Highway Administration
FTA	Federal Transit Administration
ITN	Island Trails Network
KIB	Kodiak Island Borough
NEPA	National Environmental Protection Agency
ROW	Right-of-way
TIGER	Transportation Investment Generating Economic Recovery
US DOT	United States Department of Transportation
USFWS	United States Fish and Wildlife Service
WFLHD	Western Federal Lands Highway Division

# EXECUTIVE SUMMARY

A waterfront town tucked along the coast of Kodiak Island, the City of Kodiak (COK) is a destination for cruise ships that tour Alaska's waters. The city receives an annual influx of tourists during the cruise-ship season, April through September. Over this six-month period in 2016, the city welcomed 19,224 cruise ship passengers, according to the 2016 cruise ship schedule. In order to accommodate these visitors the State of Alaska distributes the Commercial Passenger Vessel Excise Tax, a cruise-ship "head tax", to coastal cities hosting cruise ships. As a recipient, the COK has completed plans and projects aimed at improving the pedestrian infrastructure starting at Pier 2, where cruise ships dock.

This Pedestrian Pathways Plan builds on previous efforts by focusing on pedestrian improvements between downtown and periphery tourist attractions; specifically a Study Area from Center Avenue, to the Fred Zharoff Memorial Bridge (referenced as the bridge to Near Island), and to the Kodiak Public Library (referenced as the library).

The steps to develop this plan included: researching previous plans in and around the area, walking the site to inventory existing conditions, analyzing opportunities and constraints, collecting feedback through a public involvement process, developing a list of potential projects, and analyzing that list to determine an implementation strategy.

The public involvement process included two open houses, collecting feedback through an online comment form and project email, and discussions with stakeholders. Based on research and feedback, recommendations for proposed projects within the Study Area were developed within four categories: library connections, bridge connections, Mission Road connections, and other opportunities.

Ten specific projects were identified:

- L** Library Connections
  - Sidewalk on Egan Way (L1)
  - Trail from Erskine Avenue to the Kodiak Public Library (L2)



Figure 1: Isometric sketch of the Study Area and recommended projects.

The ten projects were then scored based on these criteria:

- potential use intensity,
- estimated costs,
- level of effort,
- public input, and
- potential maintenance.

The ranking criteria resulted in the following order:

1. Crosswalk Improvements on Rezanof Drive (L5)
2. Wayfinding (O1)
3. Sidewalk on Egan Way (L1)
4. Pavement Sidewalk on Rezanof Drive (B2)
5. Sidewalk on Mission Road (M1)
6. Trail from Erskine Avenue to the Kodiak Public Library (L2)
7. Trail from Rezanof Drive to Erskine Avenue (L3)

8. Connection from Lower Erskine Avenue to Rezanof Drive (B1)
9. Trail from Mission Road to the Bridge Connection (M2)
10. Widen the Sidewalk on Rezanof Drive (L4)

Beyond the 10 (ten) specific projects these general recommendations were also outlined: connections outside of the Study Area (specifically waterfront connections), coordination with Discover Kodiak, and pedestrian design recommendations.

This Pedestrian Pathways Plan provides recommendations which, if implemented, can improve safety and the pedestrian experience for tourists and locals walking from downtown, to the bridge to Near Island, to the library, and to the surrounding amenities.

# Overall Schematic



## Library Connections

- L1 - Sidewalk on Egan Way
- L2 - Trail from Erskine Ave. to the Kodiak Public Library
- L3 - Trail from Rezanof Dr. to Erskine Ave.
- L4 - Widen the Sidewalk on Rezanof Dr.
- L5 - Crosswalk Improvements on Rezanof Dr.



## Bridge Connections

- B1 - Connection from Lower Erskine Ave. to Rezanof Dr. (Options A & B)
- B2 - Pave the Sidewalk on Rezanof Dr.



## Mission Road Connections

- M1 - Sidewalk on Mission Rd.
- M2 - Trail from Mission Rd. to the Bridge Connection



## Other Recommendations

- O1 - Wayfinding

- Study Area
- Potential Connections
- Existing Sidewalks or Connections

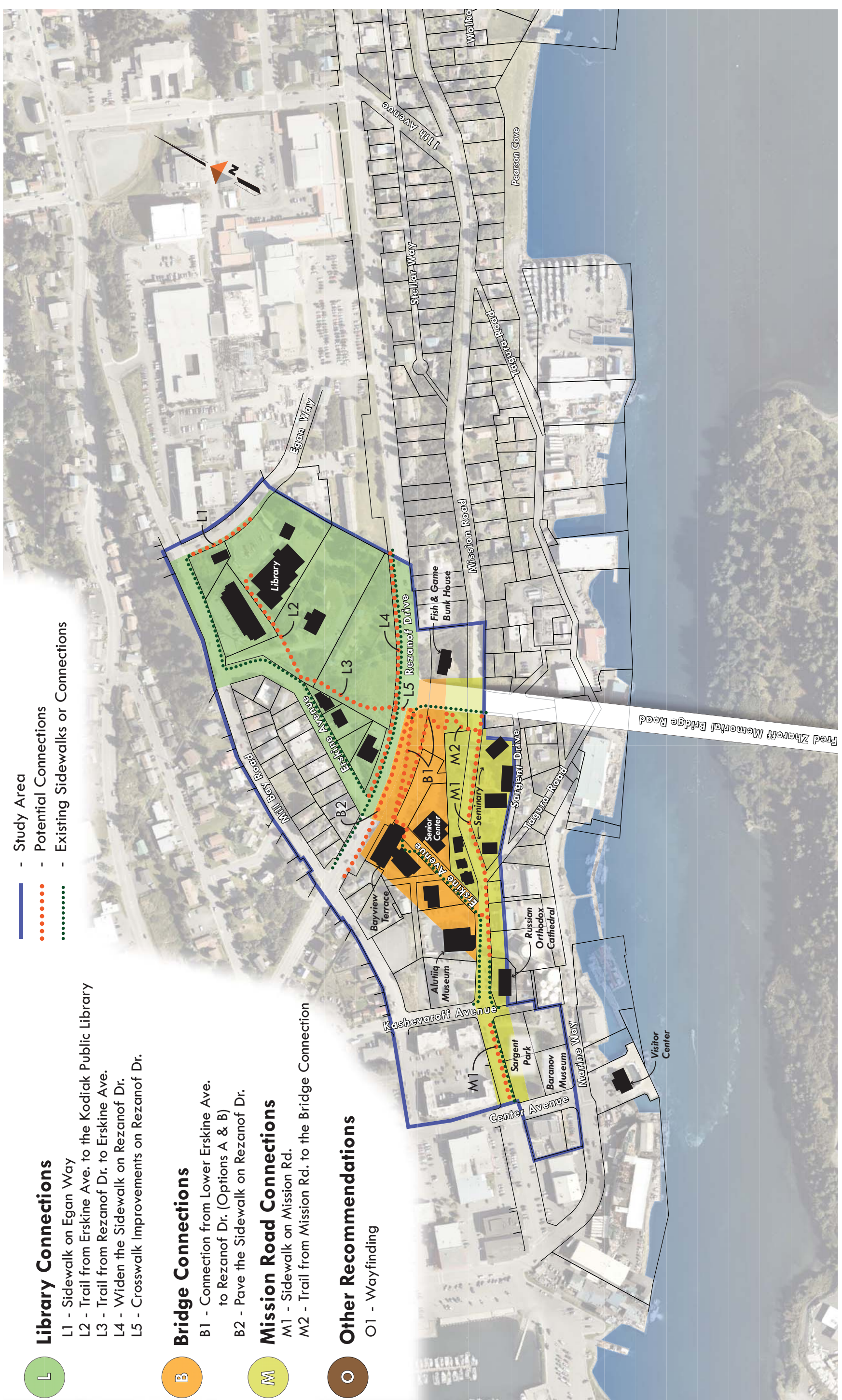


Figure 2: Overall schematic plan.

# SECTION I: PLAN BACKGROUND

## Introduction

The City of Kodiak (COK) is a coastal town located on the northeast shoreline of Kodiak Island. The city is the most populated area of the island, with a population of 6,253<sup>1</sup>. As such it is the island's main transportation hub, with a nearby airport and extensive dock infrastructure including Piers 1 and 2, which serve visitors. Pier 1 serves the Alaska Marine Highway vessels, which are used by residents and visitors, many of which are pedestrians, while Pier 2 is the docking point for visiting cruise ships.

In 2016, the COK was scheduled to receive 19,224 cruise ship passengers over a six-month period, from April through September. The 2016 schedule accounted for thirteen vessels, operated by three different cruise lines, with passenger totals ranging from 382 to 1,898 per ship. In order to assist in accommodating this influx of visitors, the State of Alaska collects and distributes the Commercial Passenger Vessel Excise Tax, a cruise-ship "head tax", to towns like the COK. This excise tax is intended to be used for improvements to cruise ship passenger related infrastructure. Past projects in coastal Alaskan towns include improvements to docks and harbors, sidewalks and sea walks (waterfront pedestrian facilities), and the development of waterfront master plans among others<sup>2</sup>.

As a recipient the COK has used the tax to fund plans and projects aimed at improving pedestrian connections for these visitors. Also funded by the cruise ship tax, this Pedestrian Pathways Plan continues this effort to improve facilities by analyzing and making recommendations regarding pedestrian connections to visitor destinations within walking distance of the COK's downtown.

<sup>1</sup> United States Census. *Quick Facts Population Estimate as of July 1, 2015*. Retrieved from: <http://www.census.gov/quickfacts/table/P5T045215/0240950>

<sup>2</sup> Alaska Department of Commerce, Community, and Economic Development. "Commercial Passenger Vessel Excise Tax: Community Needs, Priorities, Shared Revenue, and Expenditures Fiscal Years 2007 – 2014". January 2014.

## Plan Description

The purpose of the Pedestrian Pathways Plan is to evaluate options and to make recommendations for improved pedestrian links from Center Avenue to the Fred Zharoff Memorial Bridge (referenced as the bridge to Near Island), and to the Kodiak Public Library (library). The Study Area, as outlined in Figure 1, is bordered by Center Avenue to the southwest, Mill Bay Road to the west, Egan Way to the north, and Mission Road to the east and south. Adjacent to the downtown area and waterfront, this Study Area includes several of the COK's main attractions: the Alutiiq Museum, the library, and the pedestrian link to Near Island. Because of these attractions, tourists and locals alike circulate this area on foot. Many of the streets within the area have sidewalks. Some walks are narrow and some streets lack sidewalks.

## Planning Context

### PREVIOUS PLANS & PROJECTS

To better understand the context of the Study Area we reviewed previous plans and projects relevant to this plan.

These plans and projects fall into two categories:

- Plans that are broad in scope, covering regional and city-wide areas and topics.
- Previous plans and projects specific to pedestrian infrastructure near the current Study Area. These plans focused on specific pedestrian improvements from Pier 2, where cruise ship passengers disembark, through downtown.

The plans specific to pedestrian improvements resulted in projects including:

- The construction of a sidewalk from Pier 2 to Jack Hinkle Way.
- Additional bulk head parking off of Shelikof Street, along St. Paul Harbor, is currently in the design process.



Image 1: View of the distant mountains and waterfront from the bridge to Near Island.

Figure 2 outlines the areas that previous plans adjacent to the Study Area covered. Tables 1, 2 and 3 outline the plans and projects that were reviewed for relevant information.

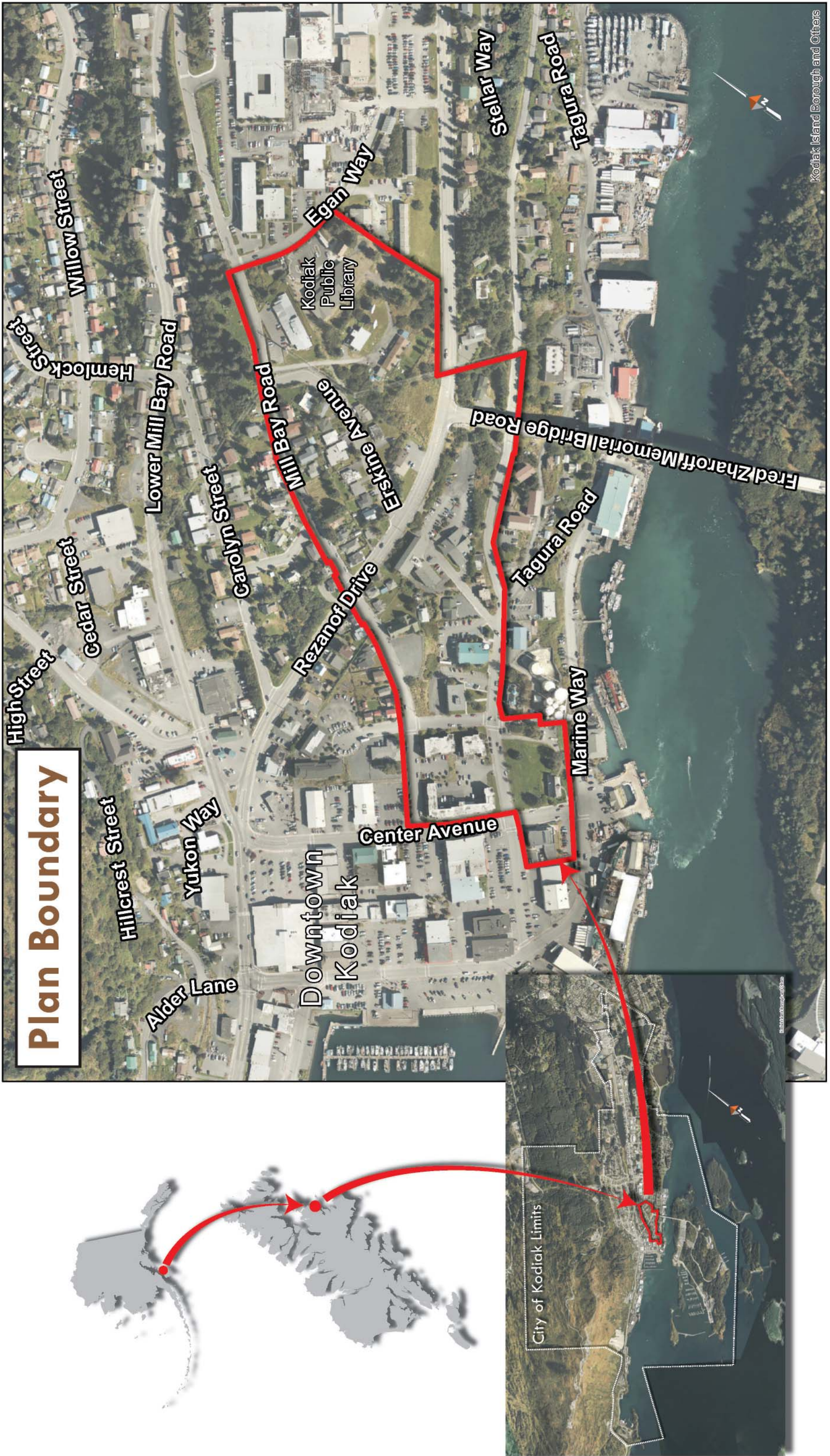
- Title 15 – Buildings and Construction
- Title 16 – Subdivision
- Title 17 – Zoning Code.

and design are in the Kodiak Island Borough Code (KIBC). Sections of code that are relevant to this plan include:

### REGULATION & CODE

The City of Kodiak (COK) is an incorporated home-rule city in the Kodiak Island Borough (KIB). The KIB is classified as a Second Class Borough with area-wide powers that include planning, platting and land use regulation. Therefore, ordinances pertinent to planning

For this plan to be adopted as an official Guiding Document it will need to be approved by the Planning and Zoning Commission of KIB.



Kodiak Island Borough and Others

Figure 3: Location map and plan boundary.



## Broad Plans Reviewed for Pertinent Information

<p>2002 Community Design Workshop Final Report</p>	<ul style="list-style-type: none"> <li>Design charrette focused on developments in downtown Kodiak and Near Island; included recommendations specific to pedestrian enhancements:             <ul style="list-style-type: none"> <li>Crosswalk and sidewalk improvements within the Study Area.</li> <li>Sidewalk along Mission Road from Erskine Avenue to under the bridge to Near Island.</li> <li>Trail from the new Mission sidewalk under the bridge up to bridge entrance (stairway improvement right along either side of bridge abutment).</li> <li>Sidewalk on south-side of Rezanof Drive from Mill Bay Road to bridge.</li> <li>Widen sidewalks on north-side of Rezanof Drive from downtown past this Study Area.</li> <li>Improve pedestrian safety by narrowing roadways, providing curb extensions, and more striped crosswalks.</li> </ul> </li> </ul>
<p>2008 Kodiak Island Borough Comprehensive Plan</p>	<ul style="list-style-type: none"> <li>Comprehensive plan describing the cultural, political, economic and environmental aspects of the Island, adopted by the Borough to guide planning decisions.</li> <li>Broad goals specific to pedestrian facilities such as: “incorporate creation of bicycle lanes or paths into road construction or improvement projects, where feasible, and incorporate creation of bicycle and pedestrian pathways in residential and other development projects”.</li> </ul>
<p>2010 COK Waterfront Master Plan</p>	<ul style="list-style-type: none"> <li>Overview of waterfront facilities’ existing conditions and desired improvements.</li> <li>Recommendations for open space development including amenity improvements to Pearson Cove.</li> </ul>
<p>2011 Kodiak Road System Trails Master Plan</p>	<ul style="list-style-type: none"> <li>Trails and pedestrian circulation inside and outside the urban context throughout Kodiak Island.</li> <li>Specific Project Recommendations (From Map 11 Proposed Improvements – Urban System, Proposed Trail Classification):             <ul style="list-style-type: none"> <li>Trail between Mission Road and Marine/Tagura under the bridge (Class 3b trail<sup>1</sup>).</li> <li>Pedestrian Trail along Mission Road to Mission Beach (Class 5 trail<sup>2</sup>).</li> </ul> </li> </ul> <p><sup>1</sup>Trail classes are defined by many factors, please reference the Kodiak Road System Trails Master Plan for a complete description. In general the classes referenced here are:</p> <ol style="list-style-type: none"> <li>Class 3b Trail: Developed/improved trail, typically constructed of native materials, trail experience is natural/semi-primitive.</li> <li>Class 5 Trail: Fully developed trail, rural to urban, constructed of asphalt or hardened surface.</li> </ol>
<p>2015 Downtown Kodiak Revitalization Action Initiatives</p>	<ul style="list-style-type: none"> <li>Key projects noted by the Downtown Kodiak Revitalization Special Committee, including the following within the Study Area:             <ul style="list-style-type: none"> <li>Install stairs up from Senior Citizens of Kodiak parking lot to bridge and crosswalk from bridge across Rezanof.</li> <li>Make a lot at Kashevaroff and Mill Bay into a small children’s park.</li> <li>Increase crosswalks as designed in 2002 Community design workshop report.</li> <li>Convert block of Marine Way between Sargent Park and Refuge into pedestrian historic core area.</li> </ul> </li> </ul>

Table 1: Broad plans reviewed for pertinent information

## Plans Specific to Pedestrian Infrastructure Adjacent to the Study Area

<p>2010 Pier 2 to Downtown Sidewalk Design Final Design Study Report</p>	<ul style="list-style-type: none"> <li>Analyzed the potential sidewalk improvements to allow cruise ship passengers to walk the waterfront to downtown.</li> <li>Prioritized a series of projects within the area.</li> <li>Catalyst for sidewalk construction and bulkhead parking improvements.</li> </ul>
<p>2010 COK Landscape Master Plan for the Downtown Water, Sewer and Storm Drain Master Plan</p>	<ul style="list-style-type: none"> <li>Master plan of pedestrian facilities in the downtown core of Kodiak.</li> <li>Identifies pedestrian and bicycle routes that link to this Study Area.</li> </ul>
<p>2012 Development Concept Plan Shellikof Street: Jack Hinkle Way to Marine Way</p>	<ul style="list-style-type: none"> <li>Developed design specific to pedestrian and parking improvements in area of focus.</li> <li>Concept design for bulkhead parking lot.</li> </ul>

Table 2: Plans specific to pedestrian infrastructure adjacent to the study area.

## Construction & Design That Resulted From Previous Pedestrian Plans

<p>2014 Sidewalk Construction</p>	<ul style="list-style-type: none"> <li>Construction of the sidewalk from Pier 2 to Jack Hinkle Way (see the image below).</li> </ul>
<p>2016 Bulkhead Parking Lot Design</p>	<ul style="list-style-type: none"> <li>Current design for the construction of bulkhead parking off Shellikof Drive near downtown.</li> </ul>

Table 3: Construction and design that resulted from previous pedestrian plans.



Image 2: Sidewalk constructed along Shellikof Street from Pier 2 to Jack Hinkle Way.

# SECTION II: PLAN DEVELOPMENT

## Plan Process

The development of this plan included research of previous plans, on-site visits, public meetings and analysis of the area to determine specific projects that will improve pedestrian circulation. The planning process started at the end of 2015 and will run through beginning of 2017. The steps to develop this plan have included:

- Research of the Study Area
  - Reviewing Previous Plans
  - Site Walks
  - Inventory of Existing Conditions
- Collecting feedback on existing conditions and preferred areas of focus. This included feedback from the first public open house on March 8, 2016 and online feedback.
- Developing draft recommendations.
- Presenting the draft recommendations and collecting feedback. Starting with the second open house on May 23, 2016, and continuing through the end of September 2016.
- Presenting the draft recommendations to the City Council on August 9, 2016.
- Developing the Draft Kodiak Pedestrian Pathways Plan.

This document is the draft plan for public review. Moving forward the next steps include:

- Collect feedback on the draft plan through January 2017 (current document).
- Incorporate feedback.
- Develop final plan.

## Existing Conditions

To understand the opportunities and constraints related to pedestrian infrastructure, information on existing conditions was collected from public data, from site visits, and from public feedback.

Figure 3 and the subsections below outline and depict the existing conditions information cataloged in six categories:

Street character was used as an evaluation category because it can help determine the best route for tourists. Street character is a subjective description of how a road might feel as a pedestrian and can be the result of many things: how wide a road is; whether or not there are pedestrian amenities; or the existence of street trees. For this study two street attributes were considered significant character contributors: adjacent landuse and traffic.

Based on these criteria streets in the Study Area fall into three groups:

1. High volume/fast traffic surrounded by mostly commercial landuses
  - Rezanof Drive
2. Low volume/slow traffic surrounded by mostly commercial and institutional landuses
  - Mission Road
  - Egan Way
  - Center Avenue
  - Erskine Avenue (south of Rezanof Drive)
3. Low volume/slow traffic surrounded by mostly residential landuses
  - Erskine Avenue (north of Rezanof Drive)
  - Mill Bay Road
  - Kasheveroff Avenue

All streets within the Study Area are owned and maintained by the COK except for Rezanof Drive and the bridge to Near Island. These are owned by the DOT&PF. (Information derived from the 2013 Kodiak Urban Area Road Service Map provided by KIB.)

The Study Area's streets range from narrow, quiet residential streets to busy corridors resulting in a variety of pedestrian experiences. All streets in the study area have two-lane, two-way traffic and most have designated on-street parking (Figure 3 shows where on-street parking was either designated with signage or observed during site visits).

## TERRAIN & VANTAGE POINTS

The Study Area is a south-facing slope between the COK's waterfront and the northeast base of Pillar Mountain. There is an overall elevation change of approximately 150 feet. The elevation starts at 20 feet (adjacent to the Visitor Center), and rises to above 170 feet (at the library). The bridge to Near Island, halfway between these high and low points, sits at approximately 115 feet. This elevation change provides scenic vantage points, but steep terrain can make pedestrian connections challenging.

Low volume/slow traffic provide a context for a safe and comfortable walk. While residential streets might not warrant additional tourist foot-traffic. The preferable routes for tourists will most likely be streets with low volume/slow traffic and with adjacent commercial and institutional landuses.



Top Left:  
Image 3: Mission Road looking southwest from the Alutiq Museum.



Bottom Left:  
Image 5: Erskine Avenue near the intersection with Mill Bay Road.



Top Right:  
Image 4: The intersection of Erskine Avenue and Mission Road southwest of the bridge to Near Island.



Bottom Right:  
Image 6: Gravel shoulder along Rezanof Drive that is used as a walkway by pedestrians.



# Vantage Points

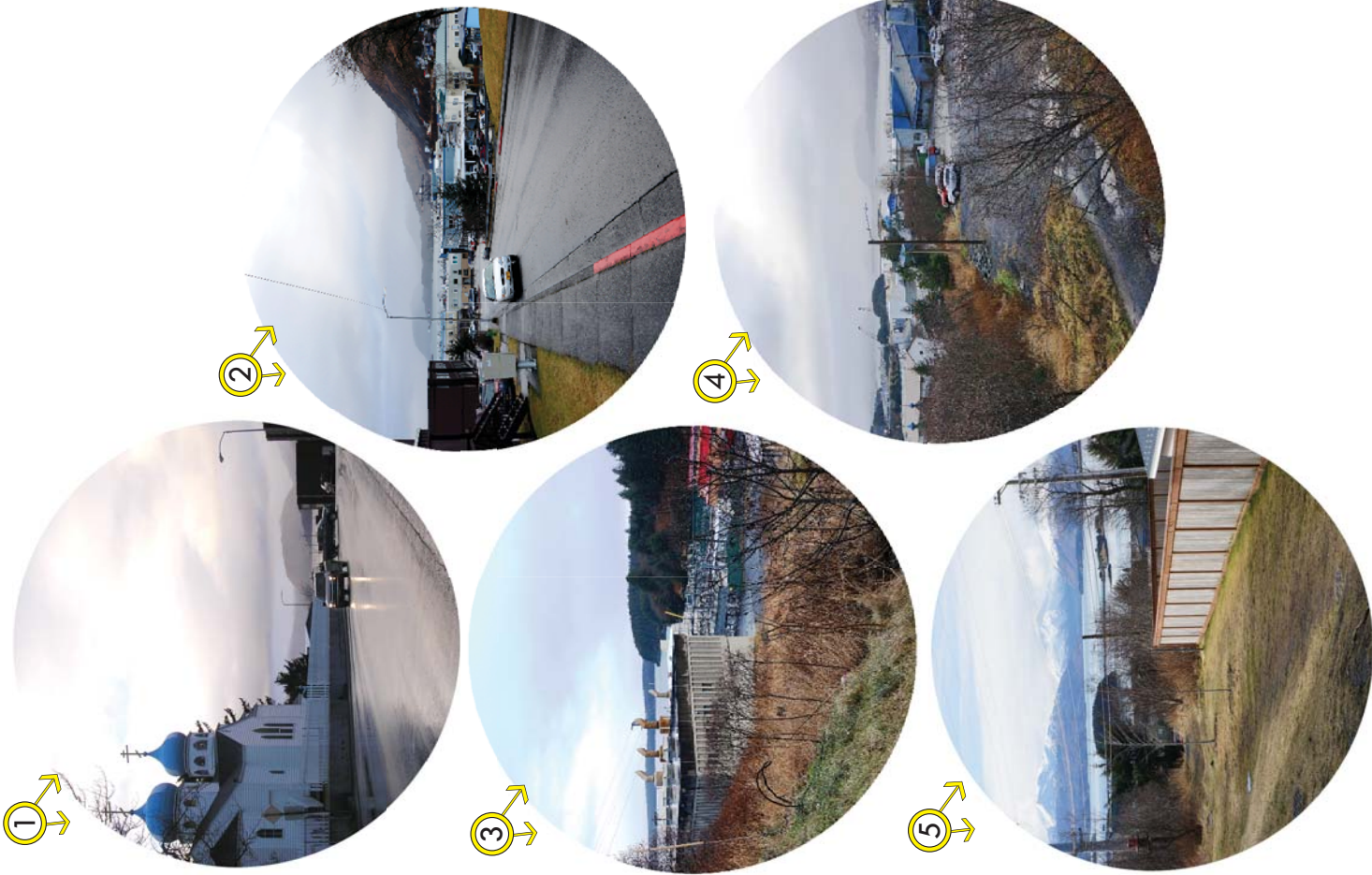


Image 7: Vantage Point #1- View from Mission Road of the Holy Resurrection Russian Orthodox Cathedral.

Image 8: Vantage Point #2 – View of the waterfront from Mill Bay Road and Kasheveroff Avenue looking towards the water.

Image 9: Vantage Point #3 – View from Mission Road looking northeast under the bridge.

Image 10: Vantage Point #4 – View of downtown Kodiak and the waterfront from the bridge to Near Island looking south.

Image 11: Vantage Point #5 – View of the water and surrounding mountains from the library social trail.

# Existing Conditions



Figure 5: Site inventory figure of existing conditions.

## PEDESTRIAN CIRCULATION

Tourists and residents circulate through the Study Area on foot to access attractions within it and beyond. Pedestrian routes are either along established paved sidewalks or on undeveloped paths through right-of-way, public land, or private properties. Figure 3 shows existing paved sidewalks, pedestrian related signage, shoulders along roads that are used by pedestrians, and social trails. Social trails are informal dirt trails that were not constructed or legally designated, but became distinct overtime by repeated use. The locations of social trails indicate connections that people are making where no established trail or sidewalk exists. The Study Area has several of these social trails as depicted by the dotted orange lines in Figure 3. The social trails indicate where a new sidewalk or trail improvements would formalize connectivity.

## ZONING, LAND USE AND OWNERSHIP

Zoning, land use, and ownership information comes from the Kodiak Peninsula Borough Geographic Information System data. The five zoning districts found in the Study Area include the Retail Business (RB) District, Public Use Lands (PL) District, Conservation (C) District, Two-Family Residential (R-2) District, and Multi-Family (R-3) District. The zoning districts are generally established to guide land use and development decisions.

**The RB District** is established for the purpose of providing for a wide range of retail and service businesses for the consumer population of large segments of the community. Because of the potential for heavy traffic and the appearance and performance of these uses, this district is located on the periphery of residential areas and at the intersections of arterial and major collector streets and roads.

Within the Study Area, the RB District is generally located south of Rezanof Drive and includes a mix of land uses, including multi-family residential, institution, church, business residential, and business.

**The PL District** is established as a land use district for publicly owned land containing recreational, educational and institutional uses. There are several areas zoned PL District within the Study Area: the Baranov Museum and surrounding parcels and the Kodiak Public Library, owned by the COK, are examples of institution land use.

The Kodiak Island Housing Authority owns a multi-family residential building adjacent to the library. The parcel on the corner of Egan Way and Mill Bay Road is owned by the KIB and used for institutional land use. Alascom Incorporated owns a parcel south of the library, which is designated institution use. The Federal Government owns a parcel north of Rezanof Drive. Additionally, the Federal Bureau of Land Management owns a vacant parcel on Mill Bay Road.

**The C District** is established for the purpose of maintaining open space areas while providing for single-family residential, and limited commercial land uses. Within the Study Area, the C District includes a parcel below the bridge to Near Island owned by the U.S. Fish and Wildlife Service. This parcel is currently permitted to the State of Alaska Department of Transportation and Public Facilities (DOT&PF) for the use and maintenance of the bridge. The bunk house on this parcel is used to house State of Alaska Fish and Game employees.

**The R-3 District** is established as a land use district for one-, two-, and multi-family dwellings and limited office uses where public water and sewer services are available. The purpose of the zoning district is to encourage residential land use, prohibit commercial and industrial uses, and discourage uses that are incompatible with residential uses.

Within the Study Area, the R-3 District is generally located southeast of the intersection of Mill Bay Road and Rezanof Drive. Several parcels are privately owned and developed with single-family residences. Two parcels are owned by the Community Baptist Church and developed with a church and parking lot.

**The R-2 District** is established as a land use district for single-family and two-family residential dwellings and limited office uses where public water and sewer services are available. The purpose of the zoning district is to encourage low density residential land use, prohibit commercial and industrial uses, and discourage uses that are incompatible with residential use.

Within the Study Area, the R-2 District is located northeast of the intersection of Mill Bay Road and Rezanof Drive and along Erskine Avenue. The parcels in the Study Area are privately owned single-family residences.



Image 12: Residential area along Mill Bay Road.



Image 13: Mission Road looking west from under the bridge to Near Island.



Image 14: Social trail on the embankment along Rezanof Drive near the bridge to Near Island.



Image 15: Social trail from the bend in Erskine Avenue to the Library.



Image 16: Intersection of Mission Road and Tagura Road.

## UTILITIES

Street lighting exists in several locations within the Study Area, such as along Mill Bay Road and Rezanof Drive. The section of Mission Road, between Center Avenue and Kashevaroff Avenue, has street lights. Center Avenue between Mission Road and Marine Way also has street lighting. Erskine and Mission Road, north of Kashevaroff Avenue, do not have street lighting. There is no pedestrian-scale lighting developed within the Study Area.

In general, the Study Area is served with basic utilities. Water and sewer service is provided by the COK, Public Works Department. The Kodiak Electric Association provides power to the COK. Overhead power-lines run through the Study Area from the transmission station located northwest of the Study Area. Telecommunication services are provided by General Communications, Incorporated.

## POINTS OF INTEREST

The Study Area includes some of the most notable attractions in the COK. Center Street is a main street through downtown. At its southeast end it passes the Kodiak National Wildlife Refuge Visitor Center, the Baranov Museum and dead-ends at the Kodiak Chamber of Commerce Visitor Center. Mission Road, which runs perpendicular to Center Avenue, provides access to Sargent Park, the Holy Resurrection Russian Orthodox Cathedral, that Alutiiq Museum, and St. Herman's Chapel. Buses drop cruise-ship visitors along this section of Mission Road due to the density and variety of attractions (as indicated by the 'Points of Interest' on Figure 3). The north end of the Study Area includes the library, the Russian Cemetery, and government offices which draw tourists and locals alike. This area also plays a role in access to nearby festivals and markets, including:

- Kodiak Brown Bear Festival, in March, hosted by the Kodiak Wildlife Refuge.
- Kodiak Whale Festival, from April through May, observed in a variety of venues including the Kodiak Senior Center, the Baranov Museum, the Kodiak Wildlife Refuge.
- Kodiak Crab Festival, May (Memorial Day Weekend), at venues located throughout downtown Kodiak.
- 1st Friday Art Crawls, observed monthly, in coffee shops and galleries.
- Pilgrimage to St. Herman, on to Spruce Island. Special services are held at the Holy Resurrection Orthodox Cathedral.
- Kodiak Bear Town Art & Crafts Market, operates periodically throughout the year, at the Su'naq Tribe bingo Hall.
- Ongoing Senior Center events, including the Arts & Crafts Fair in November.



Image 17: Signage on Mission Road to the Alutiiq Museum and to the Senior Center.

## EXISTING CONDITIONS SUMMARY

The information on existing conditions provided a baseline on what pedestrian infrastructure exists and what is lacking. For example, although several streets have sidewalks, there are areas where people walk that lack a formal connection (i.e. social trails that cut through vegetated areas outside of right-of-way or streets that lack sidewalks). The existing conditions information was shared at the first public open house. The public provided feedback and added their own observations on the area. This information also served as baseline for determining areas of need and in which areas tourists might prefer to walk. Each proposed project in the Recommendations section has a description of the existing conditions that factored into the project being included in the list.

## Public Involvement

The public involvement process included two open houses, collecting comments from an online project comment form, and from a project email address, as well as from discussions with stakeholders. These forums were used to:

- inform the public about the plan,
- listen to local perspectives on the area,
- define needed improvements, and
- collect feedback on the plan.

### OPEN HOUSE #1

The first open house took place on March 8, 2016, from 6-9pm at the downtown Kodiak Harbor Convention Center. The meeting was attended by 24 residents as well as project representatives from DOWL and the COK. The goal of this meeting was to introduce the project, and collect feedback on existing conditions, determine the preferred routes for upgrades, and understand what types of enhancements are needed.

Two feedback exercises were conducted during the meeting: collecting comments on maps, and a 'Concept Route' voting exercise. For the mapping exercise attendees wrote and drew what they saw as issues and opportunities on aerials of the study area. These comments are consolidated in Figure 4.



Image 18: Project presentation at the second open house.

For the 'Concept Route' voting exercise, a map was provided showing various routes, some existing and some proposed. Attendees were asked to vote for their two preferred routes for pedestrian infrastructure improvements. Attendees were encouraged to provide additional routes if their preferred route was not shown. Vote tallies revealed the top three preferred routes for improvements were:

- 'Route C': From Center Street, to Mission Road, to Erskine Avenue, to the Near Island bridge
- 'Route D': From Center Street, to Mission Road, to the Near Island bridge
- 'Route G': Crossing Rezanof Drive, then to the library via an alternative trail

Comments and information collected during this first phase of feedback coalesced into pedestrian project priorities, including:

- Specific Location Improvements:
  - Route C; improvements from Mission Road, up Erskine Ave, up to the bridge

- Route D; improvements along Mission Road, then up under the bridge
- Route G; alternative trail from Rezanof Drive to the library.
- Improve crossing safety at Rezanof Drive from the bridge.
- Route to Pearson Cove

- General Improvement Considerations:
  - Improve wayfinding
  - General concern for pedestrian safety
  - Cruise-ship passenger drop-off should be considered and programmed accordingly

Based on the comments from this open house and online we developed a list of potential projects which were presented at the second open house.





## OPEN HOUSE #2

The second open house was held on May 23rd, again from 6-9pm at the downtown Kodiak Harbor Convention Center. There were 12 attendees, some had been to the first open house, but the majority were participating for the first time. Drafts of potential pedestrian improvement projects were presented. The proposed pedestrian improvements included adding sidewalks and trails, and were developed based on open house and online feedback.

Using the same format as the first meeting, two interactive exercises were held as detailed in the following paragraphs. During the first exercise, attendees participated in a voting exercise. Each attendee was provided with three sticker dots and asked to place the dots next to the projects they felt should be highest priority. Prior to the exercise, it was explained to participants that this would be one of several ranking criteria that would be included in the scoring matrix. The top priority projects identified through the voting exercise were:

1. Crosswalk on Rezanof Drive (7 votes)
2. Connection from Lower Erskine Avenue to Rezanof Drive (4 votes)
3. Sidewalk on Egan Way (4 votes)
4. Wayfinding (4 votes)

For the second exercise attendees were asked to write down any issues or concerns regarding the draft Overall Schematic Map depicting all the potential projects, as shown on Figure 7. General feedback included:

- Safety concerns regarding crossing Rezanof Drive
- Limited right-of-way along Marine Way might make it hard to provide a pedestrian facility (route outside of Study Area)
- Safety concerns for pedestrians and bike riders along Mission Road
- Better wayfinding needed, especially to get tourists to the library

## SUMMARY OF PLAN DEVELOPMENT

The Pedestrian Pathways Plan was developed using background research, inventory of existing conditions and the public involvement processes.

Steps in developing the Plan included:

1. Site Inventory: provided a baseline understanding of the opportunity and constraints within the Study Area.
2. Site Walk: site observations and seeing socials trails indicated areas of need. Those observations were shared at the first open house.
3. First Open House: the public provided feedback on areas to for the planners focus on and what specific projects should be considered.
4. Feedback: feedback was used to establish a list of potential projects. The list was presented at the second public open house.
5. Second Open House: feedback from the second open house was used to develop the final list of recommendations.

Each step contributed to the final list of recommendations.



  <b>Vote for Your Top Projects</b> Place dot next to projects you feel should be considered highest priority	
<b>Library Connections</b>	
L1	Sidewalk on Egan Way ★★★★★
L2	Trail from Erskine Ave. to Library ★★
L3	Trail from Rezanof Dr. to Erskine Ave.
L4	Widen Sidewalk on Rezanof Dr.
L5	Rezanof Crosswalk Improvements ★★★★★
<b>Bridge Connections</b>	
B1	Connection from Lower Erskine Ave. to Rezanof Dr. ★★★★★
B2	Pave Sidewalk on Rezanof ★★
<b>Mission Connections</b>	
M1	Sidewalk on Mission Road ★★
M2	Connection from Mission to Bridge Connection
<b>Waterfront Connections</b>	
W1	Connection from Mission to Marine Way (Under Bridge) ★★★★★
W2	Sidewalk on Marine Way From Center Ave. to Pearson Cove
<b>Other Opportunities</b>	
O1	Branding
O2	Wayfinding ★★★★★
O3	Photo Opportunities
O4	Park at Mill Bay & Kashevaroff
O5	Historic Core Area
<b>Other Projects Not Shown</b> <i>Write note next to dot.</i>	

Figure 8: Concept projects and votes from the second open house.

# Public Comments From Open House #2

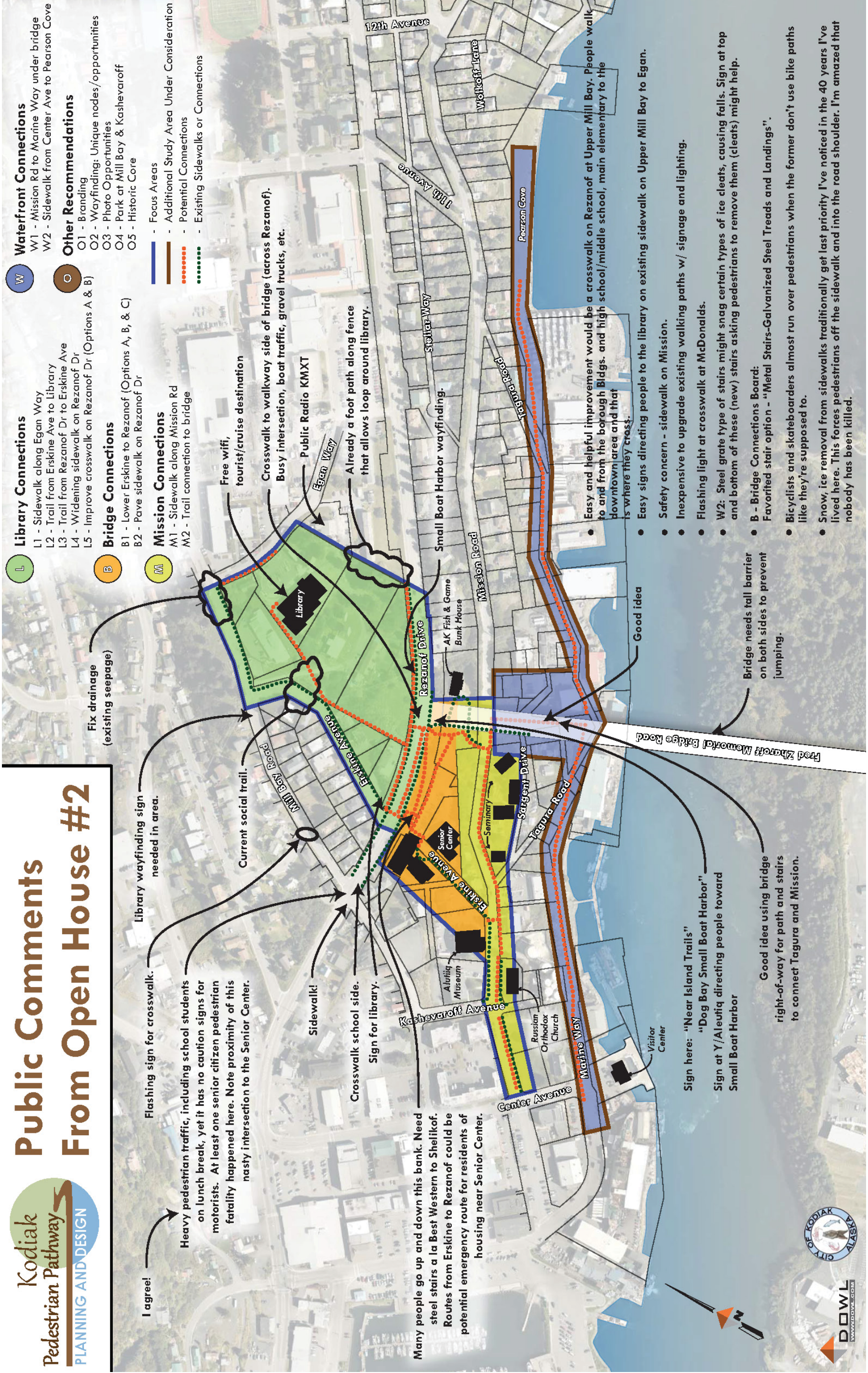


Figure 9: Consolidated graphic comments from the second open house.

# SECTION III: RECOMMENDATIONS

## Summary of Recommendations

The Overall Schematic Plan (Figure 9) shows the network of proposed pedestrian improvement projects in the Study Area. The Study Area is broken down into four smaller focus areas. Within each area is a list of potential projects. Potential projects include:

- L** Library Connections
  - L1 Sidewalk on Egan Way
  - L2 Trail from Erskine Avenue to the Kodiak Public Library
  - L3 Trail from Rezanof Drive to Erskine Avenue
  - L4 Widen Sidewalk on Rezanof Drive
  - L5 Crosswalk Improvements on Rezanof Drive
- B** Bridge Connections
  - B1 Connection from Lower Erskine Avenue to Rezanof Drive
  - B2 Pave sidewalk on Rezanof Drive
- M** Mission Road Connections
  - M1 Sidewalk Along Mission Road
  - M2 Trail from Mission Road to Bridge Connection
- O** Other Opportunities
  - O1 Wayfinding

The goal of the Plan is to illustrate projects that provide a network of pedestrian links that allow people to move safely and efficiently to points of interest within the Study Area. Each project is described in more detail in the following sections.



Figure 10: Isometric sketch of the Study Area and recommended projects.



# Overall Schematic



Figure 11: Overall schematic plan.

# Project Ranking Criteria

It is unlikely that funding will be available to complete all the proposed projects at one time. Therefore the projects have been ranked to provide guidance regarding projects with the highest use or are the quickest to implement. The ranking categories include: use intensity, cost of effort, level of effort, public input, and maintenance. The ranking criterion used has been adopted and modified from the 2011 Kodiak Road System Trails Master Plan. Each project is assigned a score within each category and scores are totaled to rank each project. Highest scores equaling top priority.

Below is an outline of the scores within each ranking criteria. The outline describes how each criteria was weighed, and a description of each criteria. Scores are based on observations and professional judgment, therefore the scoring was subjective.

## RANKING CRITERIA:

### Use Intensity

An estimate of how frequently the connections might be used by pedestrians based on observations, public feedback, and surrounding amenities. The goal is to better provide pedestrian connections for cruise ship passengers, therefore a project that directly connects to a tourist destination would have a higher rank than a project that does not. Projects were also ranked high if there were signs of high-use, such as social trails (informal dirt trails that were not built or legally designated, but became distinct overtime by repeated use).

Ranking Criteria	Rank	Description	Weighted Multiplier	Score
Use Intensity	2	High	3	6
	1	Moderate		3
	0	Little		0
Design & Construction Costs	2	Low (<\$200,000)	3	6
	1	Moderate (\$200,000 - \$400,000)		3
	0	High(>\$400,000)		0
Level of Effort	2	Short time frame: COK right-of-way	2	4
	1	Moderate time frame: COK right-of-way/ property, DOT & PF right-of-way, or a short segment of private property		2
	0	Long time frame: private or federal property		0
Public Input	2	Highest scores at 2nd Open House voting (4+ votes)	1	2
	1	Second highest scores at 2nd Open House voting (2-3 votes)		1
	0	No votes at the 2nd Open House		0
Maintenance	2	Low maintenance	1	2
	1	Moderate maintenance		1
	0	High maintenance		0

Table 4: How ranking criteria is scored and weighted.

# Project Description & Scoring Tables

This section includes enlarged figures showing the four focus areas and two tables for each of the ten recommended projects. The first table is a 'Project Description' table that outlines:

- project description,
- purpose and need (this describes how each project was derived, whether from previous plans, site observations, public feedback, or a combination),
- design considerations,
- design and construction costs, and
- other comments.

The second table is a 'Project Scoring Table'. This outlines the scores assigned to each project based on the criteria outlined in the 'Project Ranking Criteria' section. The rankings have been assigned across the full suite of 10 projects.

The projects are grouped as follows:

- L: Library Connections (5 projects);
- B: Bridge Connections (2 projects);
- M: Mission Road Connections (2 projects); and
- O: Other Opportunities (1 project).

The purpose of providing the proposed projects in this format is to have descriptions for each project that can be used alongside the enlarged figures as a tool:

- to seek funding,
- for assistance with permitting processes,
- to assist with further studies, and
- for property acquisition discussions.

## Design and Construction Costs

Preliminary cost estimates for the design and construction were completed for each project based on information observed on-site and on a very preliminary design concept of what the project might entail. No surveys of existing conditions, utilities, or topography were conducted to develop these estimates. The projects were divided into three costs categories:

1. Low: less than \$200,000
2. Moderate: \$200,000 – \$400,000
3. High: more than \$400,000

## Level of Effort

This is an estimate of the time required to complete a project based on factors such as property acquisition, design, and construction. For example, a pedestrian project in COK right-of-way of relatively short length would receive a high score since no property acquisition is required and the design and construction requirements would be minimal. A project in DOT&PF right-of-way would require a longer time-frame due to the permitting processes required. A project on private or federal property could potentially have the longest time frame due to the acquisition process.

## Public Input

At the second Open House, attendees voted for the projects they thought were the highest priority. Projects with four or more votes received the highest score, project with 2-3 votes received the median score and projects with no votes received the lowest score.

## Maintenance

The impact of a project on city maintenance is an important factor in understanding the long term financial impacts of a project. Impacts to the maintenance budget were estimated based on whether the project was an extension of existing maintenance practices or whether it would create new areas to maintain. A project that is an extension of existing sidewalks would be anticipated to be low maintenance; whereas a project outside of existing sidewalk and trail infrastructure, such as adding stairs, would be anticipated as a high impact on maintenance requirements.

# Library Connections

## Legend

- Parcels
- Contours (10 foot)
- Existing Sidewalks
- Potential Connections (within City of Kodiak ROW or Property)
- Potential Connections (not in City of Kodiak ROW)
- Potential Crosswalk (DOT&PF ROW analysis needed)

## Potential Projects

- L1** **Sidewalk on Egan Way**
  - Paved sidewalk
- L2** **Trail from Erskine Ave. to Library**
  - Gravel trail
  - Pedestrian lighting potentially
- L3** **Trail from Rezanof Dr. to Erskine Ave.**
  - Gravel trail
  - Pedestrian lighting recommended
- L4** **Widen Sidewalk on Rezanof Dr.**
  - Paved sidewalk
- L5** **Crosswalk Improvements**

Feasibility and location will depend on analysis

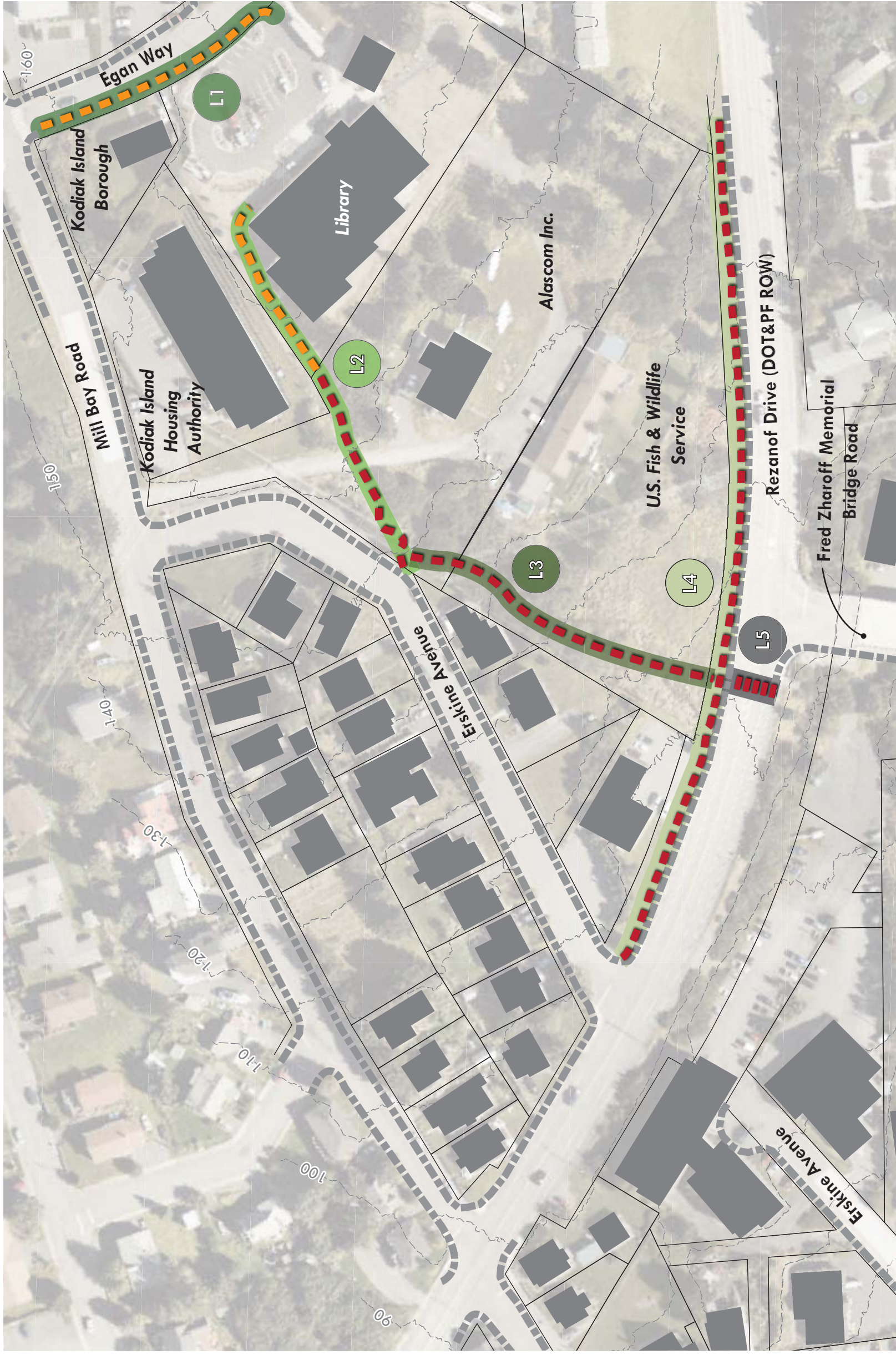


Figure 12: Library connections.

# L1 Sidewalk on Egan Way

Project Description	
Project Description	A paved sidewalk along Egan Way in front of the Kodiak Public Library.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> <li>This project was included because of on-site observations: no existing sidewalk from Mill Bay Road to the library property and social trails indicate people walk this route regularly.</li> <li>This received the second highest votes at the 2nd Open House</li> </ul>
Design Considerations	<ul style="list-style-type: none"> <li>Property: Within COK right-of-way</li> <li>Width: 6 feet</li> <li>Slope: Align running slope with Egan Way slope. Cross slopes not to exceed 2%.</li> <li>Surface: Concrete with 6-inch curb.</li> <li>Length: Approximately 250 linear feet.</li> <li>Retaining wall may be required along the Library property where there is a steep side slope.</li> <li>Include a short set of steps at the corner of the property where pedestrians currently cut through; ensure this can be tied into a pedestrian route on property.</li> <li>Include accessible curb-cut ramps.</li> </ul>
Design & Construction Costs	<p>Construction Cost: \$200,000</p> <p>Design &amp; Survey: \$50,000</p> <p>Total: \$250,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> <li>No additional right-of-way required.</li> <li>Existing curb and gutter is in serviceable condition and will be left in place.</li> <li>Existing storm drain facilities adequate.</li> <li>No new major landscaping elements.</li> </ul>
Other Comments	<ul style="list-style-type: none"> <li>Consider other sidewalk improvements along Egan outside of this Study Area.</li> <li>Public comments indicated there are drainage issues along the sidewalk on Mill Bay Road near where this proposed sidewalk would tie-in (specifically “seepage”) - investigate any drainage issues during design.</li> </ul>

Table 5: Project L1 Description



Image 19: Egan Way looking northwest from the library driveway. Proposed sidewalk would be on the left side of the road in this image



Image 20: Library entry sign. Proposed Sidewalk would wrap right in front of sign.

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	High use is expected based on the existing social trail, and that it provides a direct connection to the library.	2	3	6	
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000- \$400,000.	1	3	3	
Level of Effort	Short time frame; the project falls within COK right-of-way and the segment of sidewalk is only 250 feet in length, therefore a short design and construction time period is anticipated.	2	2	4	
Public Input	This project received four votes at the 2nd Open House.	2	1	2	
Maintenance	This sidewalk will be an extension of existing pedestrian facilities; therefore low additional maintenance is expected.	2	1	2	
Total Score				17	
Project Rank				3	

Table 6: Project L1 Score



Image 21: Egan Way looking southeast from the Mill Bay Road. Proposed sidewalk would be on the right side of the road in this image

Project Description	
Project Description	Trail from the bend in Erskine Avenue to the Kodiak Public Library.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> <li>Improving this area would provide an alternative route to the library creating more direct access.</li> <li>Feedback at 1st Open House: <ul style="list-style-type: none"> <li>Route G' (representing an alternative trail to the library) received the second highest votes.</li> <li>The public depicted this layout on several of the diagrams.</li> </ul> </li> <li>On-site Observations: <ul style="list-style-type: none"> <li>This route has a social trail indicating that it is well traveled.</li> <li>This route provides scenic views of the downtown waterfront and mountains beyond.</li> </ul> </li> </ul>
Design Considerations	<ul style="list-style-type: none"> <li>Property: Beginning at Erskine Avenue the trail would start in COK right-of-way, cross private property (Alascom Inc.), then be on COK property (library). Access length across the Alascom Inc. property is for approximately 250 linear feet.</li> <li>Width: 6 feet.</li> <li>Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations.</li> <li>Surface: Compactable gravel (meeting accessible trail standards), or asphalt.</li> <li>Length: Approximately 425 linear feet.</li> <li>Consider lighting for improved security (may not be feasible on private property but possibly on library property).</li> <li>Signage directing people to the trail.</li> </ul>
Design & Construction Costs	Construction Cost: \$135,000 Design & Survey: \$40,000 Total: \$175,000  Assumptions: <ul style="list-style-type: none"> <li>Assumed 6-foot gravel surface.</li> <li>Three luminaires on public property and within COK right-of-way.</li> <li>Stop signs at driveway crossing.</li> <li>Private property access costs not included.</li> </ul>
Other Comments	The trail crosses private property (Alascom Inc.) therefore an access agreement will need to be acquired.

Table 7: Project L2 Description



Image 22: West side of library looking south. The existing social trail can be seen in the foreground. The proposed trail would follow the social trail south towards Erskine Avenue.

Project Score				
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	This would provide an alternative route to the library; since routes already exist moderate use is expected.	1	3	3
Design & Construction Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6
Level of Effort	Moderate time frame; this time frame is an estimate based on the need to coordinate private property access. Since it is a short segment of trail a short design and construction time line is anticipated.	1	2	2
Public Input	This project received two votes at the 2nd Open House and was discussed with several attendees at the 1st Open House.	1	1	1
Maintenance	This is outside of existing right-of-way and would have moderate slopes; therefore higher additional maintenance is expected.	0	1	0
Total Score				12
Project Rank				6

Table 8: Project L2 Score

Project Description	
Project Description	A trail from Rezanof Drive to the bend in Erskine Avenue via federal property.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> <li>This connection would create a direct path between the bridge and potential trail to the library (see project: Trail from Erskine Avenue to the library).</li> <li>Alternative paths outside of the road network provide tranquil walking options away from vehicle traffic and can provide scenic views.</li> </ul>
Design Considerations	<ul style="list-style-type: none"> <li>Property: Starts at DOT&amp;PF right-of-way, crosses federal property (U.S. Fish &amp; Wildlife Service) and private property (Alascom Inc.).</li> <li>Width: 6 feet.</li> <li>Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations.</li> <li>Surface: Compactable gravel (meeting accessible trail standards), or asphalt.</li> <li>Length: Approximately 425 linear feet (length will depend on design slope, this distance is estimated based on including switchbacks to achieve an average slopes less than 8%).</li> <li>Consider lighting for improved security.</li> <li>Existing vegetation includes dense alders and large evergreen trees. Preserve existing evergreens and avoid having to clear alders to reduce future maintenance.</li> </ul>
Design & Construction Costs	<p>Construction Cost: \$125,000</p> <p>Design &amp; Survey: \$40,000</p> <p>Total: \$165,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> <li>Assumed 6 foot gravel surface.</li> <li>Bollards at either end of the trail, two solid wood on either side, removable wood in the middle.</li> <li>Two luminaires (one of the luminaires on the L2 project would illuminate the junction between L2 and L3).</li> <li>Right-of-way costs not included.</li> </ul>
Other Comments	<ul style="list-style-type: none"> <li>Due to the process of creating easements or access agreements on federal land the project would be a long term endeavor.</li> <li>If the land is ever developed for public purposes, the owners should consider including this public access connection to improve routes to and from the library.</li> <li>Consider offsetting the trail to keep the vegetative buffer between the residences, to allow greater space for the trail to meander, and create a more direct connection to the bridge.</li> </ul>

Table 9: Project L3 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This would provide an alternative route to the library; since routes already exist, moderate use is expected.	1	3	3	
Design & Construction Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6	
Level of Effort	Long time frame; due to the process of creating easements or acquiring federal land the project would be a long term endeavor.	0	2	0	
Public Input	This project received no votes at the second open house.	0	1	0	
Maintenance	This is outside of existing right-of-way and would have moderate slopes; therefore higher additional maintenance is expected.	0	1	0	
Total Score				9	
Project Rank				7	

Table 10: Project L3 Score



Image 23: Intersection of Rezanof Drive and the bridge to Near Island. The proposed trail from Rezanof (L3) would start on the left side of the image on the west side of Rezanof Drive.

Project Description	
Project Description	Widen the existing sidewalk on Rezanof Drive to an 8-foot wide paved sidewalk.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> <li>Mentioned in the 2002 Community Design Workshop Final Report.</li> <li>Widening this sidewalk would provide for a more comfortable walking experience and allow larger groups of pedestrians, such as tourists, to walk together.</li> </ul>
Design Considerations	<ul style="list-style-type: none"> <li>Property: DOT&amp;PF right-of-way (may require private property depending on how the right-of-way and sidewalk align).</li> <li>Width: 8 feet (6 feet minimum).</li> <li>Slope: Align running slope with Rezanof Drive slope. Cross slopes not to exceed 2%.</li> <li>Surface: Concrete with 6-inch curb.</li> <li>Length: Approximately 820 linear feet.</li> </ul>
Design & Construction Costs	<p>Construction Cost: \$320,000</p> <p>Design and Survey: \$65,000</p> <p>Total: \$385,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> <li>Does not include right-of-way acquisition costs.</li> <li>Remove and replace existing sidewalk.</li> <li>Potentially replace 25% of existing curb and gutter.</li> <li>Sidewalk retaining wall required for approximately half of full length (due adjacent steep slopes and since offset from right-of-way is currently unknown).</li> </ul>
Other Comments	<ul style="list-style-type: none"> <li>May require grading outside of right-of-way or retaining walls along some sections.</li> <li>Public comments indicated there are drainage issues along the hillside adjacent to this sidewalk (specifically “seepage”) - investigate any drainage issues during design.</li> <li>This recommendation is for widening the portion of sidewalk on Rezanof Drive that falls within the Study Area of this plan. Consider widening the sidewalk on Rezanof Drive along other sections of road outside of this Study Area. For example the 2002 Community Design Workshop Final Report recommends widening the Rezanof sidewalk from Lower Mill Bay Road past the Erskine Avenue intersection. Also consider sidewalk widening on the south-side of Rezanof Drive outside of this Study Area.</li> </ul>

Table 11: Project L4 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This is an area that tourists and locals currently walk and provides a connection from neighborhood areas to the bridge and library, therefore moderate pedestrian traffic is expected.	1	3	3	
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000-\$400,000.	1	3	3	
Level of Effort	Long time frame; due to the DOT&PF permitting and the potential coordination with private property.	0	2	0	
Public Input	This project received no votes at the 2nd Open House.	0	1	0	
Maintenance	This would be an improvement to an existing sidewalk therefore there would be little impact to current maintenance practices.	2	1	2	
Total Score				8	
Project Rank				10	

Table 12: Project L4 Score



Image 24: West side of Rezanof Drive looking north.



Image 25: West side of Rezanof Drive looking south.

# L5 Crosswalk on Rezanof Drive



Image 26: Pedestrians crossing Rezanof Drive to access the bridge to Near Island. This is where one of the proposed crosswalks would be located.

Project Description	
Project Description	Install striped and signed crosswalk on Rezanof Drive near the bridge to Near Island.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> <li>Crossing Rezanof Drive can feel unsafe due to the width of the road, the speed of the cars, and sight distance issues along some sections; this sentiment was reiterated by the public at both open houses.</li> <li>Voted the top priority project at the second open house.</li> </ul>
Design Considerations	<ul style="list-style-type: none"> <li>Property: DOT&amp;PF right-of-way.</li> <li>Width: 10 feet (as recommended by DOT&amp;PF).</li> <li>Surface: White paint on existing asphalt.</li> <li>Length: Approximately 45 linear feet.</li> <li>Signage as warranted.</li> <li>An analysis is needed to determine whether a cross-walk at this location meets the standard criteria.</li> </ul>
Design & Construction Costs	<p>Construction Cost: \$60,000</p> <p>Engineering: \$40,000</p> <p>Total: \$100,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> <li>Assumes warrant analysis of \$30,000 as a part of engineering costs.</li> <li>No flashing beacon assumed in construction costs.</li> <li>No right-of-way or utility issues.</li> </ul>
Other Comments	Pedestrians also cross Rezanof Drive at Erskine Avenue and Mill Bay Road. The crossing at Mill Bay Road currently has a pedestrian crossing sign for east bound traffic as seen in the image on the right. These crossings did not appear to have appropriate sight distances for pedestrians to cross safely. Any crosswalk study should analyze the conditions at these crossings as well. The proper location and signage of a crosswalk will need to be determined through full analysis.

Table 13: Project L5 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	High use is expected based on observations of people crossing and comments received at both open houses.	2	3	6	
Design & Construction Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6	
Level of Effort	Short time frame; the warrant analysis is expected to take approximately 3 months and a short design and construction time frame is expected due to the small amount of improvements.	2	2	4	
Public Input	This project received 7 votes at the 2nd Open House.	2	1	2	
Maintenance	Little additional maintenance is expected, this may include restriping.	2	1	2	
				Total Score	20
				Project Rank	1

Table 14: Project L5 Score



Image 27: As noted in the "Other Comments" row there is a pedestrian sign for crossing Rezanof Drive at the intersection of Mill Bay Road. But as seen in this image the crest in the hill above this crossing makes it hard to see oncoming traffic.



## Potential Projects

### Connection from Lower Erskine Ave. to Rezanof Dr.

#### Two Potential Options:

Option will depend on cost of effort, public feedback, and coordination with DOT&PF

#### Option A B1-A Sloped Walkway

- Paved trail
- Sloped trail with rest spot mid-way
- Fill and retaining wall as needed
- Design coordination with DOT&PF along existing road bed

#### Option B B1-B Sidewalk & Stair Near Bridge

- Approximately 20-foot tall stair with multiple landings
- Enters U.S. Fish & Wildlife Property / DOT&PF ROW

#### B2

### Pave Sidewalk on Rezanof

- Paved sidewalk along existing shoulder

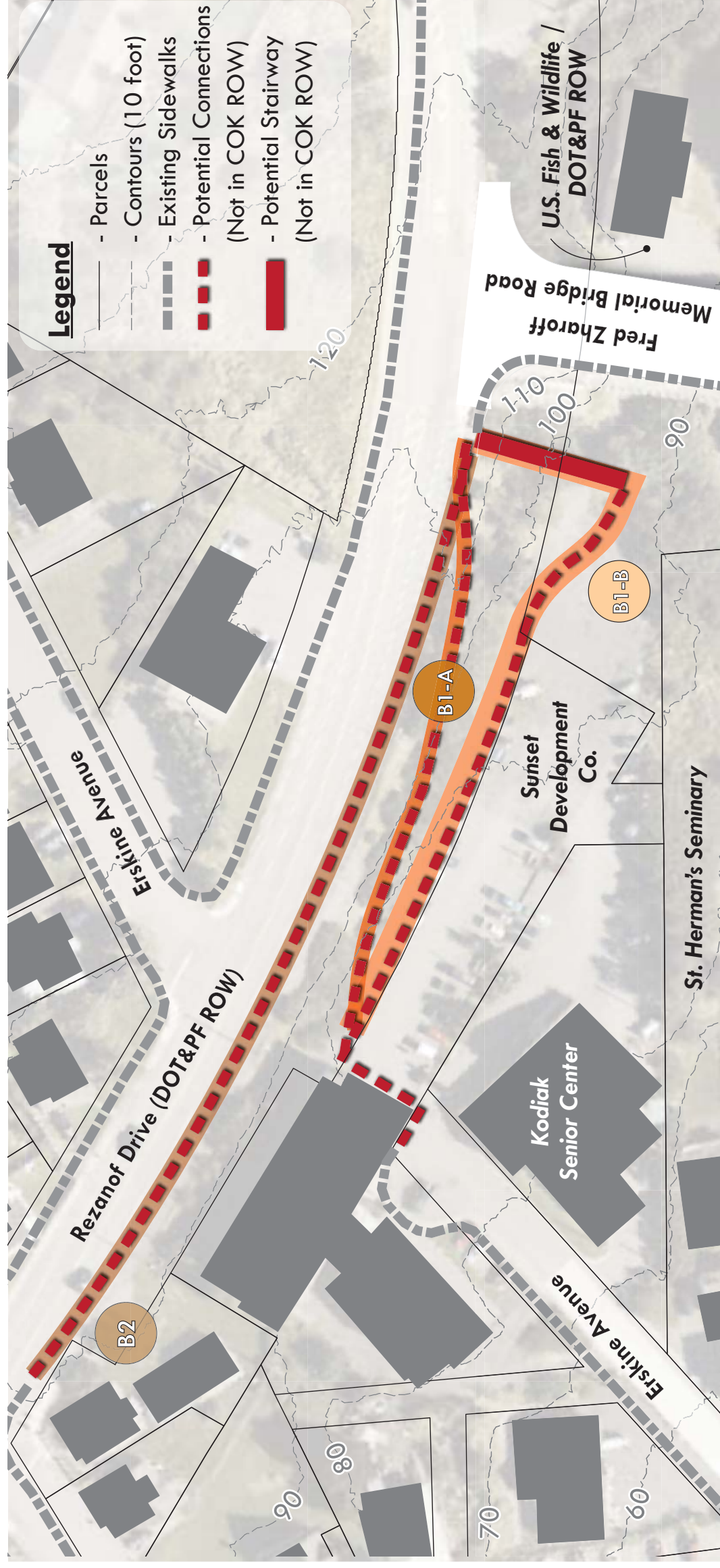


Figure 13: Bridge connections.

Project Description	
Project Description	A pathway (B1-A) or stairway (B1-B) connecting the dead end of lower Erskine Avenue to the bridge to Near Island.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> <li>Identified as a project in the 2002 Community Design Workshop Final Report (but as a stairway on the northeast side of the bridge).</li> <li>Several social trails exist in this area.</li> <li>Would provide an alternative trail to the bridge, which passes by a tourist destination (Alutiiq Museum).</li> <li>Is a vantage point providing views of the waterfront and downtown.</li> </ul>
	<p><b>General</b></p> <ul style="list-style-type: none"> <li>Incorporate wayfinding and interpretive signage at vantage points.</li> <li>Either option will need to be permitted by DOT&amp;PF.</li> <li>Protect large trees to extent feasible.</li> <li>Provide adequate lighting.</li> </ul> <p><b>B1-A Sloped Pathway</b></p> <ul style="list-style-type: none"> <li>Property: Private property (Sunset Development Company), and DOT&amp;PF right-of-way.</li> <li>Width: 6 feet.</li> <li>Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations.</li> <li>Surface: Paved with asphalt.</li> <li>Length: 460 linear feet consisting of 72 feet crossing private property and 388 feet of sloped trail (based on an average 6% slope) in DOT&amp;PF right-of-way.</li> </ul> <p><b>B1-B Stairway</b></p> <ul style="list-style-type: none"> <li>Property: Private property (Sunset Development Company), DOT&amp;PF right-of-way, and U.S. Fish &amp; Wildlife Service property on which DOT&amp;PF is a permitted use for operation and maintenance of the bridge to Near Island.</li> <li>Width: Trail segment: 6 feet; Stairs segment: 8 feet minimum</li> <li>Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations. Follow standard riser-to-tread ratios for outdoor stairways. Minimum riser height: 4.5 inches, maximum riser height: 7 inches. Minimum tread length 11 inches. Provide landings every 9-11 risers.</li> <li>Surface: Potential options include: open grate, concrete, or wood. Open grate or concrete will be longer lasting.</li> <li>Length: 460 linear feet of flat trail; 20-foot tall stairway with 6-inch risers and landings every 10 steps.</li> <li>If using open grate steps, provide a sign warning people walking with strap-on cleats to remove them.</li> <li>Consider a covered stairway to reduce long term maintenance.</li> <li>Provide a channel for bicyclists to walk their bike up and down the steps.</li> <li>Stairway could be shortened by elevating the trail to the base of the stairway.</li> </ul>
Design Considerations	

Design & Construction Costs	Other Comments
<p><b>B1-A Sloped Sidewalk</b></p> <p>Construction Cost: \$460,000                      Design &amp; Survey: \$80,000                      Total: \$540,000</p> <p><b>Assumptions:</b></p> <ul style="list-style-type: none"> <li>Assumed existing ground at a 2 to 1 slope.</li> <li>Assumed use of cast in place sidewalk retaining wall (modular block wall was investigated, but appeared more costly).</li> <li>This will be difficult to construct, as 2 to 1 embankment will require extensive excavation to build retaining wall and pathway structural fill.</li> <li>Four feet of concrete pathway and four feet of asphalt pathway was assumed.</li> <li>This will be difficult to design and construct due to the existing steep embankment; stairs may be the best solution (and likely least cost).</li> <li>Does not include right-of-way or easement acquisition.</li> </ul> <p><b>B1-B Stairway</b></p> <p>Construction Cost: \$440,000                      Design &amp; Survey: \$80,000                      Total: \$520,000</p> <p><b>Assumptions:</b></p> <ul style="list-style-type: none"> <li>Utilized a previous stairs design and estimate from 2004 and applied an inflation cost.</li> <li>The sidewalk along the existing parking area would be installed with concrete curb and gutter.</li> <li>This may be the more appropriate solution from an engineering standpoint due to challenges around constructing a sloped sidewalk on a steep embankment.</li> </ul>	<ul style="list-style-type: none"> <li>Two design options for this connection were included to show potential alternatives. Both have advantages and disadvantages; the sloped sidewalk is ideal for cyclists but may be more difficult to design and construct. The stairs are more direct but exclude wheelchairs and cyclists. Any scenario should be investigated for engineering feasibility and permitting potential with DOT&amp;PF.</li> <li>For both scenarios the first section of sidewalk crosses private property (Sunset Development Co.) for approximately 52 feet of the property (for 72 linear feet of sidewalk); therefore any development will need to be coordinated with the property owner.</li> <li>The stair option will require crossing the U.S. Fish &amp; Wildlife Service property on which DOT&amp;PF is a permitted use and operation for the bridge to Near Island. DOT&amp;PF should be coordinated with to confirm the required permitting, and if additional entities need to be notified.</li> </ul>

Table 15: Project B1 Description



Image 28: West side of the bridge abutment.



Image 29: Approach to the west side of the bridge.

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This area has several social trails and provides views of downtown Kodiak and the waterfront, therefore high use is expected.	2	3	6	
Design & Construction Costs	This project estimate falls within the high range of costs: > \$400,000.	0	3	0	
Level of Effort	Long time frame; this will require DOT&PF permitting and the coordination with a private property owner.	0	2	0	
Public Input	This project received four votes at the 2nd Open House.	2	1	2	
Maintenance	This would require additional maintenance outside of the current infrastructure; therefore higher additional maintenance is expected.	0	1	0	
Total Score				8	
Project Rank				8	

Table 16: Project B1 Score



Image 30: Existing social trail to the bridge.

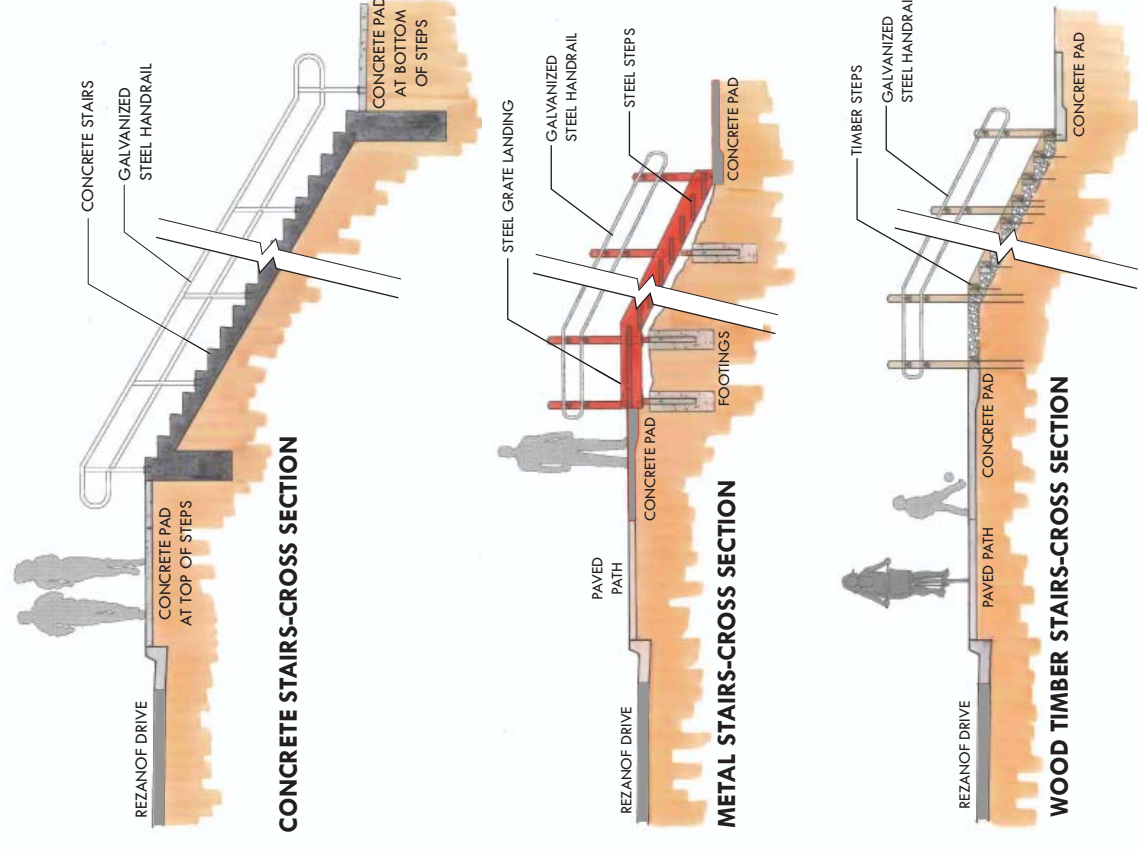


Figure 14: Sections of potential stair types including concrete, metal, and wood. Given the steep existing slope the concrete and wood stairs would require additional fill. The B1-B project design and construction cost estimate is based on an estimate completed in 2004 for a metal stair design.

Project Description	
Project Description	Pave the existing gravel shoulder on Rezanof Drive.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> <li>Increases accessibility; a paved surface is more amenable to accessible means of travel than the current gravel surface.</li> </ul>
Design Considerations	<ul style="list-style-type: none"> <li>Property: DOT&amp;PF right-of-way.</li> <li>Width: 6 feet minimum; feasible width will need to be determined based further investigations and coordination with DOT&amp;PF.</li> <li>Slope: Align running slope with Rezanof Drive. Cross slope maximum 2%</li> <li>Surface: Paved with either concrete or asphalt.</li> <li>Length: Approximately 625 linear feet.</li> </ul>
Design & Construction Costs	Construction Cost: \$160,000 Design & Survey: \$50,000 Total: \$210,000  Assumptions: <ul style="list-style-type: none"> <li>Assumed 6-foot asphalt pavement and 2-foot gravel due to proximity of steep slope.</li> <li>May require handrail in certain locations due to proximity of steep slope (not included in estimate).</li> </ul>

Table 17: Project B2 Description

Project Score				
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	This is the current link between the downtown area and the access to the bridge, therefore high use is expected.	2	3	6
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000-\$400,000.	1	3	3
Level of Effort	Short time frame; this project would require DOT&PF permitting which would take time, but a short time frame for design and construction is expected since there is an existing gravel surface.	2	2	4
Public Input	This project received two votes at the 2nd Open House.	1	1	1
Maintenance	This would be an improvement within an existing roadway, therefore there would be little impact to current maintenance practices.	2	1	2
Total Score				16
Project Rank				4

Table 18: Project B2 Score

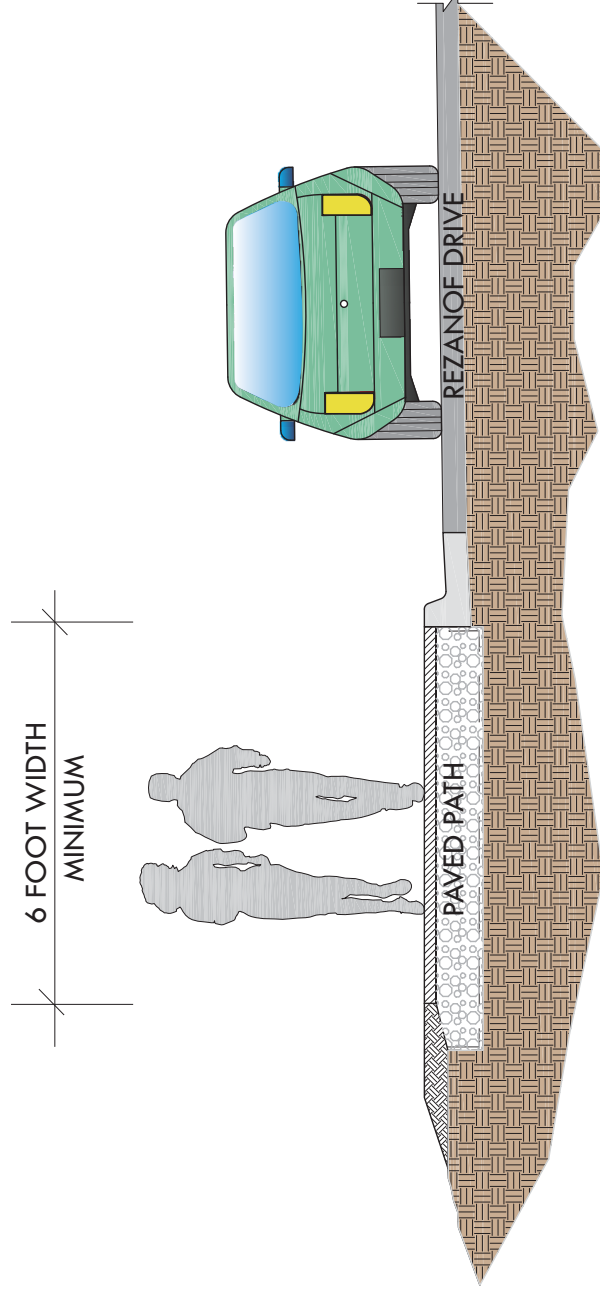


Figure 15: Rezanof Drive sidewalk section.



Image 31: Existing gravel edge along Rezanof Drive. View from the bridge looking west on Rezanof Drive.

# Mission Road Connections

## Legend

- - - Parcels
- - - Contours (10 foot)
- - - Existing Sidewalks
- - - Potential Connections (within City of Kodiak ROW or property)
- - - Potential Connections (not in City of Kodiak ROW)
- - - Potential Crosswalk (Depends on recommendations of an analysis)

## Potential Projects

- M1** **Sidewalk on Mission Road**
  - Widen existing sidewalk and add new paved sidewalk along Mission
- M2** **Connection from Mission to Bridge Connection**
  - Gravel trail and/or stairs and retaining walls as needed
  - Pedestrian lighting recommended



Figure 16: Mission road connections.



Image 32: Cruise ship passengers walking down Mission Road near the bridge to Near Island.



Image 33: Existing conditions on Mission Road.

**Project Description**

Paved sidewalk along the south-side of Mission Road. Widen the existing sidewalk along Sargent Park, and construct new sidewalk from the Russian Orthodox Church to just under the bridge to Near Island.

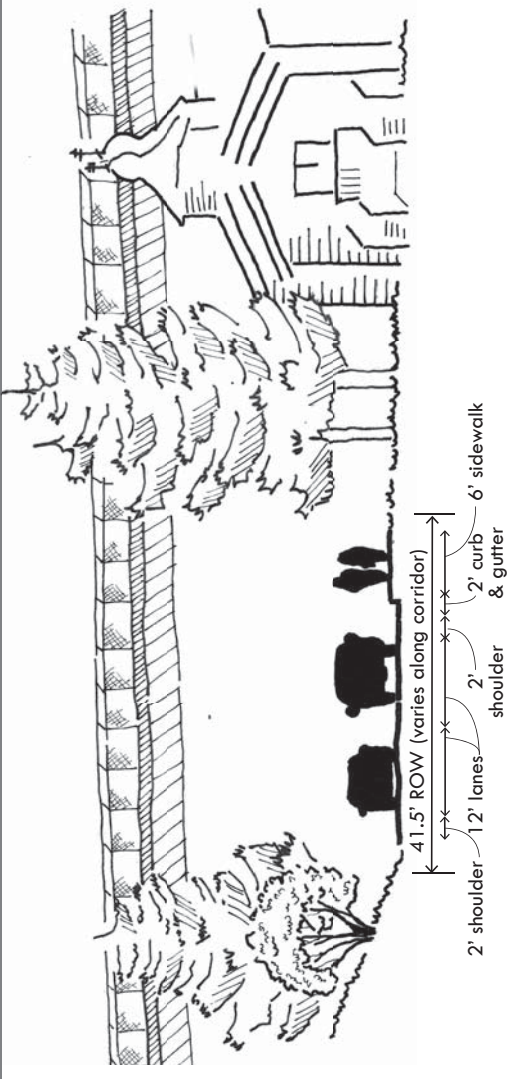
- Identified as an area for pedestrian improvements in previous plans: 2002 Community Design Workshop, 2011 Kodiak Road System Trails Master Plan.
- Social trails along roadway imply frequent pedestrian use.
- Observation of tourists and locals walking the area in the street corridor.
- Property: COK right-of-way, may require some right-of-way acquisition.
- Width: 6 feet minimum.
- Slope: Align running slope with Mission Road, cross slope maximum 2%.
- Surface: Paved with concrete and 6" curb.
- Length: 288 linear feet of widened sidewalk (4.5 feet to 8 feet); 800 linear feet of new sidewalk (6 feet wide).
- Potential short retaining wall (approximately 2-3 feet tall for up to 125 linear feet).
- Will need to accommodate driveways and potentially on-street parking across from Erskine Avenue.

Construction Cost: \$620,000  
 Design and Survey: \$100,000  
 Total: \$720,000

- Design & Construction Costs**
- Assumptions:**
- Does not include right-of-way acquisition costs.
  - Existing storm drain system to tie into with new curb and gutter.
  - May require some right-of-way acquisition and parking re-configuration at the properties west of Tagura Road.
  - Re-use existing curb and gutter along Mission Road.

Due to the scope of this study this recommendation shows the sidewalk terminating at the bridge; consider extending pedestrian improvements further along Mission Road. The 2011 Kodiak Road System Trails Master Plan includes creating a trail on Mission Road all the way out to Mission Beach.

Table 19: Project M1 Description



**Mission Road Section**

In front of St. Herman's Seminary facing the bridge to Near Island

**Project Score**

Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	High use is expected based on observed pedestrian traffic, and that this sidewalk will pass the St. Herman Catholic Seminary, a destination for locals and tourists.	2	3	6
Design & Construction Costs	This project estimate falls within the high range of costs: >\$400,000.	0	3	0
Level of Effort	Short time frame; the majority of the project is in COK right-of-way and would be along the edge of an existing roadway, therefore relatively short design and construction would be anticipated.	2	2	4
Public Input	This project received two votes at the 2nd Open House and was discussed with several attendees at the 1st Open House.	1	1	1
Maintenance	This sidewalk will be an extension of existing pedestrian facilities but is for a relatively long distance compared to other projects in this plan, therefore moderate additional maintenance is expected.	1	1	1
<b>Total Score</b>				12
<b>Project Rank</b>				5

Table 20: Project M1 Score

Project Description	
Project Description	Pathway connecting Mission Road to the improvements between lower Erskine Avenue and the bridge (see project: Lower Erskine Avenue to Bridge).
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> <li>Existing social trails.</li> <li>Provides a route for tourists to walk down Mission Road then directly up to the bridge to connect to Near Island or to the library.</li> </ul>
Design Considerations	<ul style="list-style-type: none"> <li>Property: starts on COK right-of-way then continues in DOT&amp;PF right-of-way, and U.S. Fish &amp; Wildlife Service property on which DOT&amp;PF is a permitted use for operation and maintenance of the bridge to Near Island.</li> <li>Width: 6 feet.</li> <li>Slope: Follow accessible trail design recommendations.</li> <li>Surface: Compactable gravel (meeting accessible trail standards), or asphalt.</li> <li>Length: 230 linear feet (based on an average slope of 7%-8%) – this would be the length required to meet the base of a stairway connecting to the bridge. Additional trail and/or stairs would be needed if a sloped sidewalk were developed from Erskine Avenue.</li> <li>May require short segments of retaining wall, potentially 100 linear feet long total to reduce the amount of cut. Recommended material: dry stack boulder wall. (i.e. rockery wall).</li> <li>Consider lighting for improved security.</li> <li>Provide crosswalk from sidewalk improvements on Mission Road if warranted.</li> </ul>
Design & Construction Costs	<p>Construction Cost: \$260,000</p> <p>Design &amp; Survey: \$50,000</p> <p>Total: \$310,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> <li>Assumed use of class II Rip Rap for slope protection at 1.5 to 1 slope. Stairs, gabion baskets or concrete retaining wall may be a more suitable option.</li> <li>This will require fairly extensive excavation and re-grading of area.</li> <li>This will be difficult to construct if some right-of-way is not acquired from St. Herman's Seminary, unless stairs are used.</li> <li>Asphalt pavement used in cost estimate for trail surface.</li> <li>Crosswalk on Mission Road is not included in estimate, as an analysis is needed.</li> <li>Cost to obtain easement or right-of-way not included.</li> </ul>
Other Comments	<ul style="list-style-type: none"> <li>The portion of improvements that cross U.S. Fish &amp; Wildlife Service property, on which DOT&amp;PF is a permitted use for operation and maintenance of the bridge, will need to be permitted by DOT&amp;PF.</li> <li>This property is currently used as a State of Alaska Fish &amp; Game Bunk House. It will be important to maintain separation between the existing driveway to the bunk house and the trail alignment.</li> </ul>

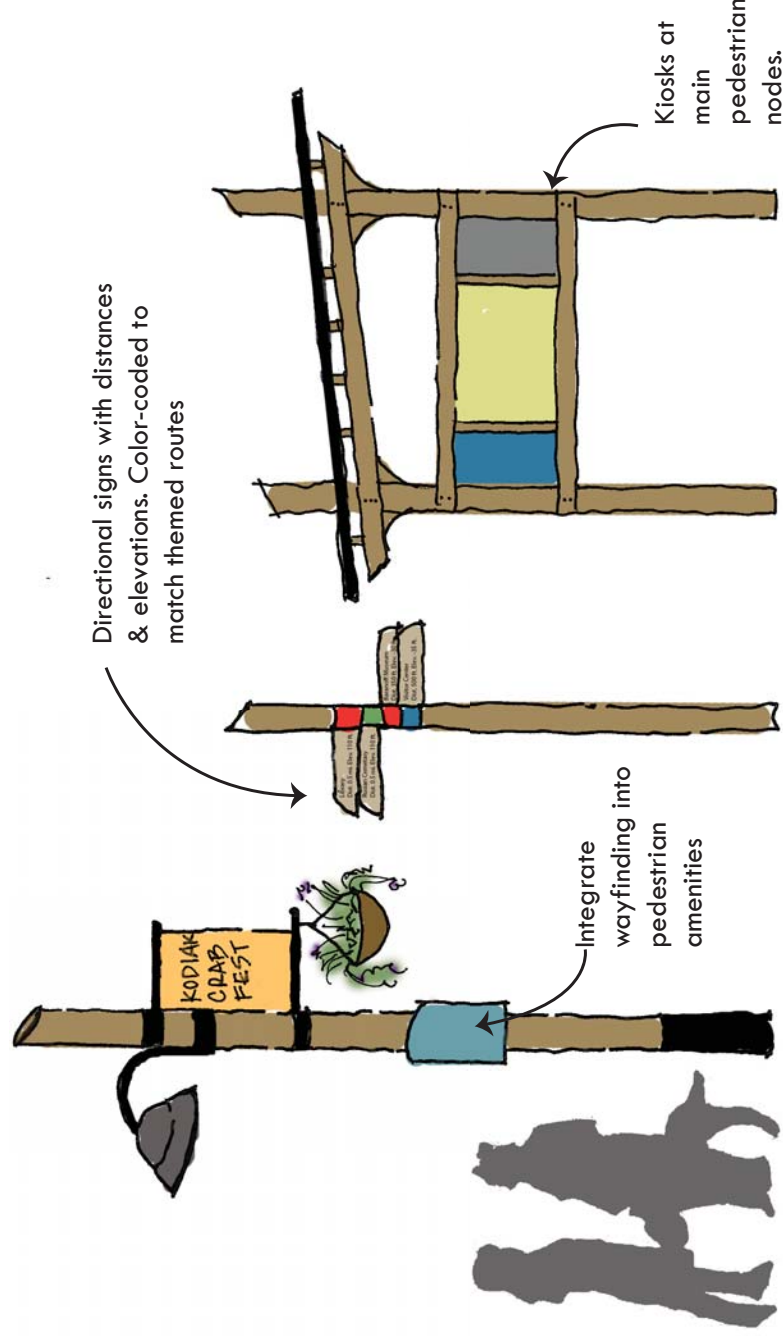
Table 21: Project M2 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	Based on social trails this area is used by pedestrians; moderate use is expected since there are other means of access to the bridge.	1	3	3	
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000 - \$400,000.	1	3	3	
Level of Effort	Moderate time frame; this project would require DOT&PF permitting but since the length of trail is short, a shorter design time frame is anticipated.	1	2	2	
Public Input	This project received no votes at the 2nd Open House.	0	1	0	
Maintenance	This trail would require maintenance outside of existing right-of-way.	0	1	0	
Total Score				8	
Project Rank				9	

Table 22: Project M2 Score



Image 34: Existing driveway off of Mission Road. The driveway runs under the bridge to the ADF&G bunk house. The proposed trail would start here and switch back to the left and connect to a connection to the bridge.



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Image Source: left: <http://nutshell.com.au/projects/shire-of-central-goldfields-interpretive-trail/>; middle: <https://www.smashingmagazine.com/street-and-wayfinding-signs-part-4/>; right: [susanjweit.com](http://susanjweit.com) (Monarch Spur Trailhead)

Above Figure 17: Other opportunities - wayfinding.  
 Above Right Table 23: Project 01 Description  
 Right Table 24: Project 01 Score

Project Description				
Project Description	Develop a wayfinding plan for the City of Kodiak, specifically for the core areas that tourists walk. A wayfinding system consists of directional signs that help locals and visitors orient themselves. They should be designed to reflect the community's aesthetic and character.			
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> <li>• Navigating the pedestrian routes from downtown to the bridge and library is not intuitive and the use of maps is required, wayfinding would help navigation significantly.</li> <li>• Several comments from the public stressed the need for improved wayfinding signage.</li> <li>• This project received four votes at the 2nd Open House.</li> </ul>			
Design Considerations	<ul style="list-style-type: none"> <li>• Signs that give distances and elevations to destinations.</li> <li>• Are color coded based on themed routes (themed routes adopted from Discover Kodiak Map and updated to reflect projects as they get implemented).</li> <li>• Are subtle in design and use materials and styles reflective of the historic and contemporary character of the COK and the surrounding natural beauty.</li> <li>• Include nodes with larger contextual maps and interpretive signs, and directional arrow signs in-between destinations.</li> <li>• Signs should direct people to main attractions including the library and attractions on Near Island (including the small boat harbor).</li> </ul>			
Plan Costs	Wayfinding Plan: +/- \$100,000			
Assumptions:	<ul style="list-style-type: none"> <li>• The wayfinding plan would be for the area from Pier 2, through downtown and up to the library.</li> <li>• This estimate is just for the planning portion of developing wayfinding, the costs of detailed design and construction would depend on the outcomes of the plan.</li> </ul>			
Other Comments	Consider doing a city branding plan prior to a wayfinding effort. A branding plan is a way for Kodiak to help identify and brand itself, it can be used for marketing, developing design standards, and developing wayfinding that matches and complements branding themes.			
Project Score				
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	Wayfinding is an amenity that can be used by visitors to navigate to areas of interest. This could be highly used by all types of visitors.	2	3	6
Plan Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6
Level of Effort	Short time frame; developing the wayfinding plan could be done relatively quickly. The actual implementation would be a longer time frame.	2	2	4
Public Input	This project was within the highest group of votes at the 2nd Open House (4 total).	2	1	2
Maintenance	This would add a whole new set of signage that would need to be maintained from vandalism and general weathering; therefore higher additional maintenance is expected.	0	1	0
Total Score		18		
Project Rank		2		



# General Considerations

Beyond the ten specific projects outlined in the previous section this section provides additional recommendations that should be considered. These are either:

- outside of the Study Area of this plan, but were included based on public feedback (waterfront connections, and pedestrian nodes);
- programming considerations, such as continued coordination with Discover Kodiak and cruise ships; or
- general considerations that apply to all or most of the ten specified projects, such as pedestrian design recommendations.

## WATERFRONT CONNECTIONS

During the public involvement process feedback regarding potential pedestrian connections outside of the Study Area was provided. People were interested in pedestrian routes from:

- the bridge to Marine Way (under the bridge), and
- Center Avenue to Pearson Cove, a small park along the waterfront.

Both of these projects would provide great walking opportunities for visitors.

The connection under the bridge provides a direct pedestrian route from the bridge all the way to waterfront. This connection includes various scenic vantage points and would direct visitors to businesses along the waterfront.

The pedestrian connection along Marine Way would provide an opportunity for visitors to see the working waterfront with an easy, flat walk to Pearson Cove. The cove is a small park offering views of Near Island, boat activity, and marine life. Both improvements fall outside of this plan's Study Area, but should be considered for further study.

### Potential Projects

-  - Concept Potential Connections
-  - Parcels

-  **W1**  
**Sidewalk on Marine Way From Center Avenue to Pearson Cove**  
 - 6 to 8-foot wide paved sidewalk

-  **W2**  
**Connection from Mission to Marine Way (Under Bridge)**  
 - Combination of stairs and trails to provide a pedestrian connection from Mission Road to Marine Way



Image 35: Pearson Cove Park. View of waterfront looking towards Near Island.



Image 36: Marine Way, view looking northeast towards Pearson Cove.

Figure 18: Potential waterfront connections.

## CONTINUED COORDINATION BETWEEN DISCOVER KODIAK & CRUISE LINES

Discover Kodiak is a nonprofit 501(c) 6 organization that works to promote tourism within the KIB and works directly with cruise-ship operators to provide visitor information to passengers. Discover Kodiak publishes a map of walking routes through downtown Kodiak. As connections and wayfinding infrastructure are developed, the maps should be updated to reflect changes. Figure 16 is the existing Discover Kodiak map which outlines themed routes, and Figure 17 shows how this map could be updated to reflect new routes that this plan outlines. The updates should also include information on the distance and elevation change of the various walks so that visitors can gauge their time and level of effort of the walk.

Local business owners expressed concern that cruise ship passengers are bussed directly from Pier 2 to destinations, thus missing opportunities to walk through the town and visit local businesses. Although Discover Kodiak passes out walking information at a booth at Pier 2 during docking, by the time the passengers debark many have already determined their plans on shore. To help address this, Discover Kodiak should continue to work with the cruise ship companies to ensure passengers are aware of the walking opportunities that connect to shopping, scenic, and cultural destinations prior to debarking. As shown in Table 23, the 2016 cruise ship schedule, some boats are only docked for seven hours; this may not be sufficient time for activities outside of the city but could be enough to walk from Pier 2 to destinations in and around downtown.

## PEDESTRIAN FACILITIES DESIGN RECOMMENDATIONS

### Sidewalk and Trail Design Standards

Sidewalks and trails should be designed in a sustainable way that will minimize long term maintenance and degradation. Wherever possible, pedestrian pathways should be designed to accommodate multiple types of users (i.e., for walking, jogging, cycling); this may not be feasible where steep grades exist. In such cases Americans with Disabilities Act (ADA) accessible pathway



Figure 19: Discover Kodiak visitor map.

Table 25: Port of Kodiak, cruise ship schedule 2016

Vessel Name	PAX	Cruise Line	Length	Arrival Date	Day	Arrival Time	Dep Time	Location
Crystal Serenity	922	Crystal Cruise Lines	781	4/25/2016	Monday	8:00 AM	5:00 PM	P-2
Volendam	1,432	Holland America	781	4/28/2016	Thursday	8:00 AM	5:00 PM	P-2
Silver Shadow	382	Silver Seas	610	5/17/2016	Tuesday	8:00 AM	6:00 PM	P-2
Maasdam	1,898	Holland America	610	6/1/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	6/15/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	6/29/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	7/13/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	7/27/2016	Wednesday	7:00 AM	2:00 PM	P-2
Crystal Serenity	922	Crystal Cruise Lines	610	8/10/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	8/17/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	8/24/2016	Wednesday	7:00 AM	2:00 PM	P-2
Silver Shadow	382	Silver Seas	610	9/20/2016	Tuesday	9:00 AM	3:00 PM	P-2

Table 25: Port of Kodiak, cruise ship schedule, 2016.

and stairway design standards should be followed. This includes providing level stopping points on long sections of steep trail and providing adequate handrails on any steps or stairways.

Sidewalk widths should vary based on expected pedestrian volume and the character of location. For example downtown sidewalks should be wider than sidewalks within residential areas. Typically, sidewalks should provide a five-foot minimum width; this would allow two people moving in opposite directions to pass

each other. Wider sidewalks are desirable in areas where people are expected to walk in pairs or groups. Having wider sidewalks in areas with higher volumes of vehicle traffic provides pedestrians more separation so that they are not right next to traffic. All the sidewalk and pathway improvements recommended in this plan provide a six-foot minimum width for paths on routes that lead to destinations where visitors and locals are expected to walk in larger groups.

Several comments from the public noted issues with ice and snow on sidewalks. Icy sidewalks can create a hazard and cause pedestrians to walk in the streets. The City should maintain sidewalks as snow and ice free as possible. During site visits it was observed that cars parked on sidewalks with rolled curbs. Cars blocks pedestrians from using the sidewalk facilities. The city should enforce parking violations that create conflicts with pedestrian use of sidewalks and paths.

### Create Pedestrian Amenity Nodes

Create pedestrian destination nodes for tourists. These could be specific points along pedestrian routes that have amenities such as benches, informative/

interpretive signs, and provide scenic views. A recommendation from Discover Kodiak is to create structures that frame views of Kodiak that visitors can take photos through. This element is adopted from the National Parks Service (NPS) “#FindYourPark” campaign in which frame structures are provided at tourist destinations offering a “framed” photograph opportunity for visitors.

### Standardized Pedestrian Lighting

Pedestrian lighting can transform a street from a corridor just for cars to a comfortable route for pedestrians. Lighting at the pedestrian scale is both an aesthetic improvement and safety improvement. It enhances visibility between people and cars. Pedestrian lighting should be considered along pedestrian routes that are not along roadways (i.e., trails or stairways), and on sidewalks where street lights are not sufficient or that pedestrians frequently use. The COK should consider adopting a standard pedestrian scale light fixture and luminaire.

Benefits of having a standard style of lighting include:

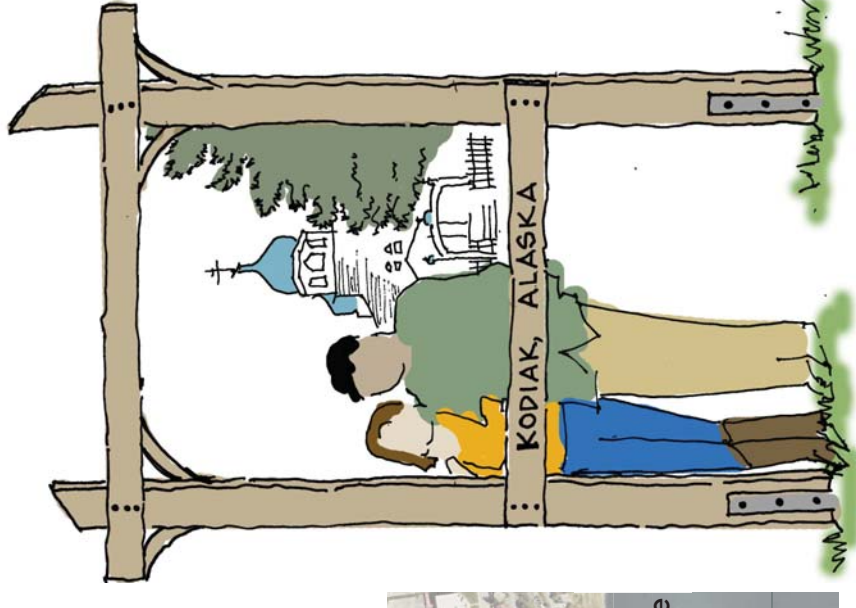
- maintenance personnel can become familiar and

- efficient with one type of fixture;
- the City will only need to keep spare parts for one product;
- the City can make bulk boarders which can reduce costs; and
- one style of lighting will provide a uniform appearance throughout walking areas.

### Pedestrian Nodes



Figure 21: Potential pedestrian nodes with frame structures that take advantage of views.



## Update Themed Walking Routes On The Discover Kodiak Map

--- Cultural    - - - - - Shopping    — Nature Hike    — Waterfront



Figure 20: Diagram of how pedestrian routes on the Discover Kodiak Map could be updated once projects are implemented.



Image 37: View of where a pedestrian amenity node could be.

# SECTION IV: IMPLEMENTATION

## Implementation Order

The ranking that resulted from project scores is a recommended order of project implementation. The project scores, resulted in the following Rank Order:

1. Crosswalk Improvements on Rezanof Drive- L5 (score 20)
2. Wayfinding Plan- O1 (score 18)
3. Sidewalk on Egan Way- L1 (score 17)
4. Pave Sidewalk on Rezanof Drive- B2 (score 16)
5. Sidewalk along Mission Road- M1 (score 12)
6. Trail from Erskine Avenue to Library- L2 (score 12)
7. Trail from Rezanof Drive to Erskine Avenue- L3 (score 9)
8. Connection from Lower Erskine Avenue to Rezanof Drive- B1 (score 8)
9. Trail from Mission Road to Bridge Connection- M2 (score 8)
10. Widen Sidewalk on Rezanof Drive- L4 (score 8)

**14** Ranked at number one with an overall score of twenty is providing a crosswalk between the bridge to Near Island and Rezanof Drive's north side. This could be a quick, low cost, and potentially highly used project. This project requires a study to determine if this area warrants a crosswalk and what appropriate signs and lighting would be required by DOT&PF. If a study does show a need, a permit from DOT&PF would be required.

Two projects tied with a score of twelve: the sidewalk along Mission Road and the trail from Erskine Avenue to the Library. The Mission Road sidewalk was ranked higher due to a higher expected use by pedestrians. It would serve a section of road that currently has no sidewalk, whereas, the trail to the library would be an additional alternative connection to the library.

Three projects received a score of eight:

- the connection from lower Erskine Avenue to Rezanof Drive,
- the trail from Mission Road to the bridge connection, and
- widening the sidewalk on Rezanof Drive.

the KIB, COK, DOT&PF, Alaska Department of Fish and Game (ADF&G), United States Fish and Wildlife Service (USFWS), and private property owners.

A majority of the proposed pedestrian facilities will likely be permissible within the various rights-of-way held by KIB, COK, and DOT&PF. There is one parcel where multiple entities may have to agree to trail construction and use, parties include the USFWS and DOT&PF. The remaining areas include a few properties where public use easements will need to be acquired from private property owners.

## FUNDING

There are several funding sources available for bicycle and pedestrian projects. "Pedestrian and Bicycle Funding Opportunities U.S. Department of Transportation Transit, Highway, and Safety Funds", is attached to this Plan as an appendix for reference. Given that recent program budgets have been shrinking nationwide, funding partners are seeking to leverage funds<sup>1</sup> from multiple sources to complete transportation projects. The funding sources discussed below are those most commonly involved in funding bike and pedestrian-related projects statewide.

## FEDERAL:

### U.S. Department of Transportation - Transportation Investment Generating Economic Recovery (TIGER)

The U.S. Department of Transportation (U.S. DOT) has made nearly \$500 million available annually for transportation projects since 2010 through the TIGER grant program. Applications under this program are successful if the project being applied for demonstrates construction readiness (National Environmental Protection Agency (NEPA) and design work is complete), the ability to leverage funds, create jobs, and enhance the economic well-being within a community. The TIGER grant program supports innovative projects,

<sup>1</sup> *Leveraging funding is the process of using multiple funding sources for one project. For example, a grant program may require the grant applicant provide a 20% cost share (match). The grant applicant can use local funds or other funding sources to provide the cost share for the project. Funding agencies are more likely to fund a project if other funds are assigned to the project already.*

including multi-modal and multi-jurisdictional projects; bicycle lanes, parking, transit, bus shelters and benches, crosswalks; sidewalk improvements such as lighting, curb cuts and American with Disabilities Act (ADA) ramps; and paved shoulders for pedestrian and bicyclist use.

The TIGER grant program is focused on capital projects that generate economic development and improve access to reliable, safe and affordable transportation for communities, both urban and rural. TIGER grant program funding opportunities are highly competitive, and are typically announced in late spring.

### U.S. Department of Transportation - Federal Transit Administration (FTA)

The FTA provides formula funding to help cities, towns, and rural areas invest in bicycle infrastructure that can improve mobility and help people access public transportation. A local transit provider, such as Kodiak Area Transit System (KATS), may be interested in partnering with a local government to improve sidewalks and bicycle paths so that residents have better access to their transit system. This opportunity is developed through partnerships and leveraging funding. There is no solicitation for this opportunity.

### U.S. Economic Development Administration

The Economic Development Administration (EDA) solicits applications from applicants in rural and urban areas to provide investments that support construction, non-construction, technical assistance, and revolving loan fund projects under EDA's Public Works and other programs. Grants and cooperative agreements made under these programs are designed to leverage existing regional assets and support the implementation of economic development strategies that advance new ideas and creative approaches to advance economic prosperity in distressed communities. EDA provides strategic investments on a competitive- merit-basis to support economic development, foster job creation, and attract private investment in economically distressed areas of the United States. This opportunity is open year round. A grant applicant can meet with the local EDA Program Manager out of Anchorage to determine eligibility.

## Federal Highway Administration

The Federal Highway Administration (FHWA) administers what is often referred to as Chapter 1 funds authorized under current transportation legislation. (Chapter 1 of 23 USC is where the regulations governing these funds are located.) This agency works with each state to expend the state's share of these federal transportation dollars for programs such as the Surface Transportation Program, Transportation Alternatives Program, Tribal Transportation Program, and Tribal Transportation Safety programs, and all transit programs. In Alaska, FHWA's partner agency is the Alaska Department of Transportation & Public Facilities (DOT&PF). Funding is administered through DOT&PF.

## Western Federal Lands Highway Division

Western Federal Lands Highway Division (WFLHD) of the Office of Federal Lands Highway (FLH) jointly administers the Tribal Transportation Program with the Bureau of Indian Affairs (BIA). Each federally recognized tribe is eligible for this funding and is allocated an annual dollar amount based on a codified formula that takes into account tribal population, road mileage, and average tribal shares. These funds are often referred to as Chapter 2 funds and can be used by tribes as local match funds on projects funded with Chapter 1 funding.

WFLHD also administers Alaska's Federal Lands Access (FLAP) Program; a program for surface transportation facilities providing access to, or within, federally-owned lands. This program is designed to encourage cooperation and coordination among federal land management agencies, state agencies, and local and tribal governments. Funding is administered through DOT&PF.

## Bureau of Indian Affairs

The Tribal Transportation Program (TTP) provides tribes with a percentage of funding allocated based on highway legislation to plan, design, construct, and maintain their local transportation system. TTP funds can be used for safety, transit, administration, maintenance, bike and pedestrian facilities, and planning. These funds can be leveraged and used as non-federal match. This opportunity is developed through partnerships and leveraging funding. There is no solicitation for this opportunity.

## STATE:

### Alaska Surface Transportation Block Grant (STBG) Set-Aside Program

The Fixing America's Surface Transportation (FAST) Act of 2015 includes the Surface Transportation Block Grant (STBG) Program, which sets aside funds for use for a broad range of projects, including walking and bicycling projects. Funding levels in the STBG Set-Aside Program are set at \$835 million for financial years 2016 and 2017, rising to \$850 million in financial years 2018-2020. This funding is distributed to the state and local level on a formula-set apportionment basis. A local match is required for all projects. Funding opportunities are typically made available in late spring.

## Legislature

Each year the Alaska Legislature develops both capital and operating budgets for the state. In years when the state's fiscal situation allows, transportation projects for areas across the state are often included as line items in the capital budget. Additionally, the legislature periodically drafts bond bills that are then voted on by state residents during general elections. Unlike capital budget line items, items identified in an approved bond bill are funded through the sale of general obligation bonds, which are repaid at a later date using specified state revenues.

## Department of Transportation & Public Facilities

As FHWA's partner agency for the State of Alaska, DOT&PF is responsible for the planning and programming of funding under the purview of FHWA. Several types of funding DOT&PF administers allow tribal governments, municipal governments, and other similar entities to nominate projects for inclusion in the Statewide Transportation Improvement Program (STIP) or compete for grant-like funding to complete projects. The State of Alaska also has a State Highway Safety Improvement Program (HSIP). The HSIP is a core Federal-aid program with the purpose of achieving a significant reduction in fatalities and serious injuries on all public roads. Funding is administered through DOT&PF.

## Department of Commerce, Community, and Economic Development

Alaska's Department of Commerce, Community, and Economic Development (DCCED) administers several programs of interest in terms of developing and maintaining transportation infrastructure vital to a community's success. Most notably, it administers the Community Development Block Grant (CDBG) program, funded by the U.S. Department of Housing and Urban Development (HUD). Once each year, municipal governments are able to apply for CDBG funding for an array of project types, which include transportation improvements such as bicycle and pedestrian facilities. In addition to capital projects, HUD also allows CDBG funding to be used for planning efforts. Funding is administered through the State of Alaska.

## DESIGN & CONSTRUCTION

The level of design and construction for each project varies. Some projects are technically complex, requiring retaining walls or footings in the case of elevated stairs. Others have relatively minimal design and construction efforts, such as short sections of gravel trail. In either case the design will need to be reviewed and permitted by the appropriate authority such as the COK Building Safety Department or DOT&PF.

Depending on the complexity of the project, the construction work will likely need to be contracted out to professional contractors. In cases where construction is minimal, there may be opportunities for volunteers to assist with construction. This may be possible for short sections of gravel trail that have minimal grading restrictions, or sign installation.

# Conclusion

As a coastal town the COK has a unique, authentic working industrial waterfront with ocean and mountainous views. By improving pedestrian access to the waterfront, downtown, the Study Area in this plan and beyond, tourists and locals will have opportunities to walk to local businesses, cultural sites and community events. A pedestrian network improves alternative transportation, provides health benefits, contributes to the local economy, and can contribute to an overall sense of place.



## Appendix A: Public Comment

The following table outlines the public comment received over the duration on the project. The comments are categorized into two categories:

- Specific Route Comments
- General Comments

Within those two categories the comments are further divided into topics, this includes:

- Specific Route Comments:
  - Crosswalks
  - Connection at the Bridge
  - Connections Under the Bridge (to Marine Way)
  - Mission Road
  - Marine Way/Park at Pearson Cove
  - Connection to the Library
  - Other Route Considerations
- General Comments:
  - Wayfinding/Signage
  - Visitor Circulation & Cruise Ship Passenger Drop-Off
  - Miscellaneous

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
<b>Specific Route Improvements</b>				
<b>Crosswalk</b>				
1	Crosswalks on Rezanof should have light up crossing signs to alert motorists on this blind corner.	Web	1	A crosswalk is proposed across Rezanof to the bridge (project L5). Whether or not a crosswalk is feasible and what features are appropriate will need to be determined with appropriate analysis as noted in the L5 project description table.
2	Near the intersection of Rezanof Drive and the bridge, an inlaid crosswalk is needed. Inlaid crosswalks may cost more upfront, but they last much longer so the cost is recovered.	Comment Form	1	See response to comment 1.
3	Flashing sign for crosswalk. (Arrow pointing to crossing on Upper Mill Bay Road near the intersection with Wilson Ave.)	Map Comments (Open House #2)	1	Since this plan is for improving the pedestrian connections for tourist destinations this was not included as a potential pedestrian improvement because it does not appear to connect to a tourist destination or route.
4	I worry about elderly people crossing Rezanof. Is there some way to connect route "G" directly to the Bridge and route "C" to downtown? At least this is a major intersection with excellent visibility both up and down Rezanof.	Web	1	A series of projects are proposed to create this connection between route "C" and "G"; this would include projects: L2, L3, L4, L5, and B1 (A or B). See the overall schematic for how each project is linked.
5	"Route B" is my preferred route to Near Island so I would suggest a pedestrian walkway on the south side of Rezanof."	Web	1	Project B2 is to pave a sidewalk along this section of Rezanof.
6	Seepage from natural spring down the hill from the library can make the sidewalk slick.	Map Exercise (Open House #1)	1	This has been noted on the public comment map, and noted as a design consideration in the L4 sidewalk project.
7	Dirt path along Rezanof prevents ice.	Map Exercise (Open House #1)	1	Project B2 is to pave a sidewalk along this section of Rezanof. Currently the surface is dirt and gravel which is less accessible than a paved sidewalk. Although this section of sidewalk seems less slick than the north sidewalk it may be because there appears to be drainage seeping from the hillside at that location as noted by other members of the public.
8	Easy and helpful improvement would be a crosswalk on Rezanof at Upper Mill Bay. People walk to and from the Borough buildings, an high school/middle school, main elementary to the downtown area and that is where they cross.	General Comments (Open House #2)	1	Project L5 recommends a crosswalk at the bridge, but the best location for a crosswalk will need to be determined with proper analysis. The crosswalk option at Mill Bay Road has been noted in the L5 project table under other comments.
9	Crosswalk school side. (Intersection of Upper Mill Bay Road and Rezanof Drive.)	Map Comments (Open House #2)	1	See response to comment 8.
10	Crosswalk to walkway side of bridge. (Across Rezanof)	Map Comments (Open House #2)	1	A crosswalk is proposed across Rezanof to the bridge (project L5).
11	Heavy pedestrian traffic, including school students on lunch break, yet it has no caution signs for motorists. At least one senior citizen pedestrian fatality happened here. Note proximity of this nasty intersection to the Senior Center. (X2)	Map Comments (Open House #2)	1	A cross-walk is proposed across Rezanof to the bridge (project L5). Whether or not a cross-walk is feasible and what features are appropriate will need to be determined with full analysis.
12	Sidewalk! (Arrow pointing to intersection of Upper Mill Bay Road and Rezanof Drive.)	Map Comments (Open House #2)	1	This has been noted on the public comment map.



Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
<b>Connection at Bridge</b>				
13	We should avoid doing a stair set if at all possible to maintain access to bicycles.	Web	1	As an alternative to a stairway Project B1-A, a sloped trail, has been included in the potential projects for this area.
14	Route C- it's used already but muddy, steep, unsafe.	Comment Form	1	Project B1 is proposed to improve these conditions; this would either be a sloped sidewalk or stairway.
15	There is a social trail that cuts up the hill to Rezanof from the end of Erskine.	"Map Exercise (Open House #1)"	1	See response to comment 14.
16	Walk up from Senior Center parking to the bridge can feel unsafe due to brush and poor visibility.	"Map Exercise (Open House #1)"	1	See response to comment 14.
17	Stairs at bridge.	"Map Exercise (Open House #1)"	1	See response to comment 14.
18	Stairs with lighting (at bridge).	"Map Exercise (Open House #1)"	1	See response to comment 14.
19	Information, signage, views (at bridge stairs).	"Map Exercise (Open House #1)"	1	Noted in the project description of B1.
20	Possible connection from end of lower Erskine up the hill to Rezanof.	"Map Exercise (Open House #1)"	1	See response to comment 19.
21	New Mortuary along lower Erskine.	"Map Exercise (Open House #1)"	1	This has been noted on the public comment map.
22	Route pedestrians around parking lot at Senior Center.	"Map Exercise (Open House #1)"	1	Project B1 can address this by tying a new sidewalk into the existing one at the end of Erskine Avenue.
23	Difficult to cross parking lot near Senior Center.	"Map Exercise (Open House #1)"	1	See response to comment 22.
24	Busy intersection: boat traffic, gravel trucks, etc. (intersection at bridge to Near Island)	Map Comments (Open House #2)	1	This has been noted on the public comment map.
25	(Arrow pointing out metal stairs, galvanized steel treads and landings.)	Map Comments (Open House #2)	1	This is included in the design considerations for project B1-B.
26	Many people go up and down this bank (slope along Rezanof Dr. north of the Senior Center). Need steel stairs a la Best Western to Shellikof.	Map Comments (Open House #2)	1	Included as a recommendation in project B1-B.
27	Routes from Erskine to Rezanof could be potential emergency route for residents of housing near Senior Center.	Map Comments (Open House #2)	1	This has been noted on the public comment map.
28	Steel grate type of stairs might nag certain types of ice cleats, causing falls. Sign at top and bottom of these (new) stairs asking pedestrians to remove them (cleats) might help.	Map Comments (Open House #2)	1	Noted in the project description of B1.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
<b>Connections Under Bridge (to Marine Way)</b>				
29	What about access from Marine Way, down by F&A and Power house?	Comment Form	1	This is outside of the Study Area of this plan but has been noted in the General Consideration section for further exploration.
30	It would be nice to have a pathway that connects Marine Way (under the bridge) to the bridge.	Comment Form	1	See response to comment 29.
31	KEA easement? (noted under bridge near Marine Way).	Map Exercise (Open House #1)	1	See response to comment 29.
32	Link from Mission under bridge to Powerhouse drawn on map.	Map Exercise (Open House #1)	2	See response to comment 29.
33	Route from Marine Way to bridge drawn.	Map Exercise (Open House #1)	4	See response to comment 29.
34	Good idea (trail under the bridge connecting Mission Road to Marine Way).	Map Comments (Open House #2)	1	See response to comment 29.
<b>Mission Road</b>				
35	Old sidewalk under dirt/gravel along Mission Road past the church.	Map Exercise (Open House #1)	1	This has been noted on the public comment map.
36	Poor or no lighting along Mission.	Map Exercise (Open House #1)	1	Under the General Considerations section there are recommendations to enhance pedestrian lighting where existing lighting is nonexistent or insufficient.
37	Mission Road "not pedestrian friendly".	Map Exercise (Open House #1)	1	Project M1 is included to improve pedestrian connections along Mission Road.
38	Ideal route because already sidewalks (along Mission near Center).	Map Exercise (Open House #1)	1	The Mission Road projects include M1 & M2. M1 is a sidewalk along the southeast side of the road and M2 is a trail that connects up to the bridge pedestrian connection.
39	Two houses along Mission near Erskine are condemned.	Map Exercise (Open House #1)	2	This has been noted on the public comment map.
40	Potential hoop house as a part of St. Herman's Chapel on Mission.	Map Exercise (Open House #1)	1	See response to comment 39.
41	Properties along Mission belong to Seminary.	Map Exercise (Open House #1)	1	See response to comment 39.
42	House between two condemned buildings along Mission belongs to Seminary.	Map Exercise (Open House #1)	1	See response to comment 39.
43	Main Seminary parking lot along Mission.	Map Exercise (Open House #1)	2	Because of the Seminary location the sidewalk along Mission Road is recommended to be on the southeast side of the road to improve pedestrian access to the Seminary (project M1).
44	Sidewalk on Mission after Tagura would require retaining wall.	Map Exercise (Open House #1)	1	This has been considered for the preliminary cost estimate for project M1.
45	High pedestrian traffic along Mission from Seminary.	Map Exercise (Open House #1)	1	Noted, this is one of the reasons that the sidewalk (project M1) was recommended to be on the Seminary side of Mission Road.
46	Existing driveway below seminary potential- pedestrian path.	Map Exercise (Open House #1)	1	This is outside of the study area but has been noted on the public comment map.
47	Safety concern- sidewalk on Mission.	General Comments (Open House #2)	1	A sidewalk (project M1) has been proposed to improve safety.

	Comment	Comment Source	Number of Times Comment Appeared	Response
<b>Marine Way/Park at Pearson Cove</b>				
48	Marine Way drawn as a route on comment map notes; "Marine Way is a great walk".	Map Exercise (Open House #1)	1	This is outside of the Study Area of this plan but has been noted in the General Consideration section for further exploration.
49	Pearson Cove Park drawn on map.	Map Exercise (Open House #1)	1	See response to comment 48.
50	Route drawn along Marine Way.	Map Exercise (Open House #1)	1	See response to comment 48.
51	Connection to park at Pearson Cove drawn.	Map Exercise (Open House #1)	1	See response to comment 48.
52	Route drawn from Pearson Cove drawn to Steller Way.	Map Exercise (Open House #1)	1	See response to comment 48.
<b>Connection to Library</b>				
53	Coming down "A" or "B" there is very little to see except a graveyard and possibly a view of Kodiak with mountains in the background, which I imagine you can get from "G".	Web	1	Route "G" developed into projects L2 & L3 which provide scenic views of the water and surrounding mountains.
54	Route from Mill Bay Road to Library noted as "Trail?"	Map Exercise (Open House #1)	1	We did not see a trail at this location but did see one coming south from the library which was proposed as project L2.
55	Route drawn from Rezanof to Library via trail.	Map Exercise (Open House #1)	1	This social trail was located and has been included as project L2.
56	Route drawn from curve in Erskine to Library.	Map Exercise (Open House #1)	6	See response to comment 55.
57	Free wifi, tourist/cruise destination (Library)	Map Comments (Open House #2)	1	This has been noted on the public comment map.
58	Fix drainage (existing seepage). (Sidewalk on Upper Mill Bay Road near intersection with Egan Way)	Map Comments (Open House #2)	1	This has been noted on the public comment map, and noted as a design consideration in the L1 sidewalk project.
59	Pubic Radio KMXT noted on plan next to Library.	Map Comments (Open House #2)	1	See response to comment 58.
60	Already a foot path along fence that allows loop around Library. (Along the east side of the U.S. Fish & Wildlife Service property and Alascom property).	Map Comments (Open House #2)	1	Noted, projects L2 & L3 would formalize a trail through this area.
<b>Other Route Considerations</b>				
61	Route drawn from Upper Mill Bay Road going northwest to Post Office.	Map Exercise (Open House #1)	1	This is outside of the Study Area but has been noted on the public comment map.
62	Students from High School go down Mill bay to Subway almost every day.	Map Exercise (Open House #1)	1	This has been noted on the public comment map.
63	Route on Tagura Road drawn.	Map Exercise (Open House #1)	2	See response to comment 61.
64	Trident workers walk from housing down Center Avenue and Mission Road to Trident facility.	Map Exercise (Open House #1)	2	See response to comment 62.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
<b>General Considerations</b>				
<b>Wayfinding/Signage</b>				
65	How long they (tourists) spend in Kodiak to see the sights is key, thus every attraction should be marked with how long it takes to walk there and perhaps the stress involved e.g. uphill.	Web	1	This has been included in the Wayfinding project O1.
66	The weather will play a big part in what visitors get to see and do, so you need to divide attractions and the routes to them by indoors/outdoors.	Web	1	This has been noted on the public comment map.
67	Rather than putting all this information on signs, it would be best to produce a "walking map/brochure" that the cruise ships can give to their passengers before they dock. This will give passengers time to plan.	Web	1	Discover Kodiak publishes a map that is distributed to visitors. Part of the recommendations of this plan is to update that map with new routes as they are developed.
68	Plan on incorporating interpretive signs into all pathways.	Comment Form	1	The design considerations for project O1 (wayfinding) include a recommendation to have interpretive signs.
69	Easy signs directing people to the library on existing sidewalk on upper Mill Bay to Egan.	General Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to have signage to the library.
70	Inexpensive to upgrade existing walking paths with signage and lighting.	General Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to develop signage. The General Recommendations section recommends pedestrian lighting improvements.
71	Small Boat Harbor wayfinding. (at Rezanof intersection with bridge)	Map Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to have signage to attractions on Near Island.
72	Sign for Library (at intersection of upper Erskine Ave. with Rezanof Dr.).	Map Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to have signage to the library.
73	Library wayfinding sign needed in area. (Area near Mill Bay Road north of Rezanof Drive.	Map Comments (Open House #2)	1	See response to comment 72.
74	Sign "Near Island Trails" & "Dog Bay Small Boat Harbor". Sign @ Y/Alutiiq directing people toward Small Boat Harbor.	Map Comments (Open House #2)	1	See response to comment 72.
<b>Visitor Circulation Considerations &amp; Cruise Ship Passenger Drop-off</b>				
75	If visitors are dropped off at the Alutiiq Museum there is a lot of uphill to reach the library or the bridge. Maybe taxis and vans could drop visitors at the library and then its all downhill from there.	Web	1	This has been noted on the public comment map.
76	Many of the visitors I have seen appear to be elderly, so unless you plan on building a tunnel or overpass, I think that the safest route to cross Rezanof would be coming downhill.	Web	1	See response to comment 75.
77	From the visitors perspective, presumably most will come from cruise ships.	Web	1	See response to comment 75.
78	Cruise ship passenger bus stop passenger load/unload, 60-80 people, in front of Alutiiq Museum.	Map Exercise (Open House #1)	1	See response to comment 75.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
<b>Miscellaneous</b>				
79	The City should enforce its ordinance prohibiting parking on sidewalks.	Comment Form	1	This has been noted on the public comment map.
80	Love the bulkhead parking along Shelikof and I would love even more to incorporate the Following: interpretive signs, covered bike shelter, public art.	Comment Form	1	See response to comment 79.
81	There aren't sidewalks in places that are actually useful for pedestrians, especially downtown.	Comment Form	1	The goal of this plan is to highlight potential projects that will improve the network of pedestrian connections. If implemented these projects will add more useful sidewalks and trails.
82	There aren't useful pathways that connect Rezanof/Mission/Mill Bay, in fact, there are only (maybe) 3 roads that directly connect what are the major arteries in Kodiak. How can we create paths that will do what roads can't? I think we can.	Comment Form	1	The projects proposed in this plan will increase pedestrian options in the Study Area. See the overall schematic for the proposed network of paths.
83	"Old Dorms" at corner with Library.	Map Exercise (Open House #1)	1	This has been noted on the public comment map.
84	Future bronze bear statue at the Kodiak National Wildlife Refuge Visitor Center summer 2016.	Map Exercise (Open House #1)	1	See response to comment 83.
85	Bicyclists and skateboarders almost run over pedestrians when the former don't use bike paths like they're supposed to.	Map Comments (Open House #2)	1	See response to comment 83.
86	Snow, ice removal from sidewalks traditionally get last priority, I've noticed in the 40 years I've lived here. This forces pedestrians off the sidewalk and into the road shoulder. I'm amazed that nobody has been killed yet!	Map Comments (Open House #2)	1	Maintaining the sidewalks free of snow and ice was included in the General Recommendations section.
87	Flashing light @ crosswalk @ McDonald's	Map Comments (Open House #2)	1	This is outside of the Study Area of this plan but has been noted on the public comment map.
88	Bridge needs tall barrier on both sides, as a teenage suicide almost happened here. Companions with would be victim grabbed her before she jumped.	Map Comments (Open House #2)	1	See response to comment 87.
89	Noted AK Fish & Game Bunkhouse on building near bridge.	Map Comments (Open House #2)	1	This has been noted on the public comment map.

# Appendix B: Federal Pedestrian and Bicycle Funding Opportunities

## Pedestrian and Bicycle Funding Opportunities U.S. Department of Transportation Transit, Highway, and Safety Funds

Revised August 12, 2016

This table indicates potential eligibility for pedestrian and bicycle projects under U.S. Department of Transportation surface transportation funding programs. Additional restrictions may apply. See notes and basic program requirements below, and see program guidance for detailed requirements. Project sponsors should fully integrate nonmotorized accommodation into surface transportation projects. Section 1404 of the Fixing America's Surface Transportation (FAST) Act modified 23 U.S.C. 109 to require federally-funded projects on the National Highway System to consider access for other modes of transportation, and provides greater design flexibility to do so.

Activity or Project Type	U.S. Department of Transportation Transit, Highway, and Safety Funds														
	TIGER	TIFIA	FTA	ATI	CMAQ	HSIP	NHPP	STBG	TA	RTP	SRTS	PLAN	NHTSA 402	NHTSA 405	FLTP
Access enhancements to public transportation (includes benches, bus pads)	\$	\$	\$	\$	\$		\$	\$	\$						\$
ADA/504 Self Evaluation / Transition Plan								\$	\$			\$			\$
Bicycle plans			\$					\$	\$		\$	\$			\$
Bicycle helmets (project or training related)								\$	\$SRTS		\$		\$*		
Bicycle helmets (safety promotion)								\$	\$SRTS		\$				
Bicycle lanes on road	\$	\$	\$	\$	\$	\$	\$	\$	\$	\$	\$				\$
Bicycle parking	~\$	~\$	\$	\$	\$		\$	\$	\$	\$	\$				\$
Bike racks on transit	\$	\$	\$	\$	\$			\$	\$						\$
Bicycle share (capital and equipment; not operations)	\$	\$	\$	\$	\$		\$	\$	\$						\$
Bicycle storage or service centers at transit hubs	~\$	~\$	\$	\$	\$			\$	\$						\$
Bridges / overcrossings for pedestrians and/or bicyclists	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Bus shelters and benches	\$	\$	\$	\$	\$		\$	\$	\$						\$
Coordinator positions (State or local)					\$ 1 per State			\$	\$SRTS		\$				
Crosswalks (new or retrofit)	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Curb cuts and ramps	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Counting equipment			\$	\$		\$	\$	\$	\$	\$	\$	\$*			\$
Data collection and monitoring for pedestrians and/or bicyclists			\$	\$		\$	\$	\$	\$	\$	\$	\$*			\$
Historic preservation (pedestrian and bicycle and transit facilities)	\$	\$	\$	\$				\$	\$						\$
Landscaping, streetscaping (pedestrian and/or bicycle route; transit access); related amenities (benches, water fountains); generally as part of a larger project	~\$	~\$	\$	\$			\$	\$	\$						\$
Lighting (pedestrian and bicyclist scale associated with pedestrian/bicyclist project)	\$	\$	\$	\$		\$	\$	\$	\$	\$	\$				\$
Maps (for pedestrians and/or bicyclists)			\$	\$	\$			\$	\$			\$*			
Paved shoulders for pedestrian and/or bicyclist use	\$	\$			\$*	\$	\$	\$	\$	\$	\$				\$

Key: \$ = Funds may be used for this activity (restrictions may apply). \$\* = See program-specific notes for restrictions. ~\$ = Eligible, but not competitive unless part of a larger project.

Pedestrian and Bicycle Funding Opportunities															
U.S. Department of Transportation Transit, Highway, and Safety Funds															
Activity or Project Type	TIGER	TIFIA	FTA	ATI	CMAQ	HSIP	NHPP	STBG	TA	RTP	SRTS	PLAN	NHTSA 402	NHTSA 405	FLTP
Pedestrian plans			\$						\$		\$	\$			\$
Recreational trails	~\$	~\$							\$		\$				\$
Road Diets (pedestrian and bicycle portions)	\$	\$				\$			\$						\$
Road Safety Assessment for pedestrians and bicyclists						\$			\$			\$			\$
Safety education and awareness activities and programs to inform pedestrians, bicyclists, and motorists on ped/bike safety									\$SRTS		\$	\$*	\$*	\$*	
Safety education positions									\$SRTS		\$		\$*		
Safety enforcement (including police patrols)									\$SRTS		\$		\$*	\$*	
Safety program technical assessment (for peds/bicyclists)									\$SRTS		\$	\$*	\$		
Separated bicycle lanes	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Shared use paths / transportation trails	\$	\$	\$	\$	\$*	\$	\$	\$	\$		\$				\$
Sidewalks (new or retrofit)	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Signs / signals / signal improvements	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Signed pedestrian or bicycle routes	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Spot improvement programs	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Stormwater impacts related to pedestrian and bicycle projects	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Traffic calming	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Trail bridges	\$	\$	\$	\$	\$*	\$	\$	\$	\$		\$				\$
Trail construction and maintenance equipment									\$RTP	\$RTP	\$				
Trail/highway intersections	\$	\$	\$	\$	\$*	\$	\$	\$	\$		\$				\$
Trailside and trailhead facilities (includes restrooms and water, but not general park amenities; see guidance)	~\$*	~\$*							\$*	\$*	\$*				\$
Training					\$	\$	\$	\$	\$		\$	\$*	\$*		
Training for law enforcement on ped/bicyclist safety laws									\$SRTS	\$SRTS	\$			\$*	
Tunnels / undercrossings for pedestrians and/or bicyclists	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$

**Abbreviations**

ADA/504: Americans with Disabilities Act of 1990 / Section 504 of the Rehabilitation Act of 1973  
TIGER: Transportation Investment Generating Economic Recovery Discretionary Grant program  
TIFIA: Transportation Infrastructure Finance and Innovation Act (loans)  
FTA: Federal Transit Administration Capital Funds  
ATI: Associated Transit Improvement (1% set-aside of FTA)  
CMAQ: Congestion Mitigation and Air Quality Improvement Program  
HSIP: Highway Safety Improvement Program  
NHPP: National Highway Performance Program  
STBG: Surface Transportation Block Grant Program  
TA: Transportation Alternatives Set-Aside (formerly Transportation Alternatives Program)  
RTP: Recreational Trails Program  
SRTS: Safe Routes to School Program / Activities  
PLAN: Statewide Planning and Research (SPR) or Metropolitan Planning funds  
NHTSA 402: State and Community Highway Safety Grant Program  
NHTSA 405: National Priority Safety Programs (Nonmotorized safety)  
FLTP: Federal Lands and Tribal Transportation Programs (Federal Lands Access Program, Federal Lands Transportation Program, Tribal Transportation Program, Nationally Significant Federal Lands and Tribal Projects)

**Program-specific notes**

Federal-aid funding programs have specific requirements that projects must meet, and eligibility must be determined on a case-by-case basis. For example:

- TIGER: Subject to annual appropriations.
- TIFIA: Program offers assistance only in the form of secured loans, loan guarantees, or standby lines of credit, but can be combined with other grant sources, subject to total Federal assistance limitations.
- FTA/ATI: Project funded with FTA transit funds must provide access to transit. See [Bikes and Transit](#) and the FTA Final Policy Statement on the [Eligibility of Pedestrian and Bicycle Improvements under Federal Transit Law](#).
  - Bicycle infrastructure plans and projects funded with FTA funds must be within a 3 mile radius of a transit stop or station, or if further than 3 miles, must be within the distance that people could be expected to safely and conveniently bike to use the particular stop or station.
  - Pedestrian infrastructure plans and projects funded with FTA funds must be within a ½ mile radius of a transit stop or station, or if further than ½ mile, must be within the distance that people could be expected to safely and conveniently walk to use the particular stop or station.
  - FTA funds cannot be used to purchase bicycles for bike share systems.
  - FTA encourages grantees to use FHWA funds as a primary source for public right-of-way projects.
- CMAQ projects must demonstrate emissions reduction and benefit air quality. See the CMAQ guidance at [www.fhwa.dot.gov/environment/air\\_quality/cmaq/](http://www.fhwa.dot.gov/environment/air_quality/cmaq/) for a list of projects that may be eligible for CMAQ funds. Several activities may be eligible for CMAQ funds as part of a bicycle and pedestrian-related project, but not as a highway project. CMAQ funds may be used for shared use paths, but may not be used for trails that are primarily for recreational use.
- HSIP projects must be consistent with a State’s [Strategic Highway Safety Plan](#) and either (1) correct or improve a hazardous road location or feature, or (2) address a highway safety problem.
- NHPP projects must benefit National Highway System (NHS) corridors.
- STBG and TA Set-Aside: Activities marked “\$SRTS” means eligible only as an SRTS project benefiting schools for kindergarten through 8<sup>th</sup> grade. Bicycle transportation nonconstruction projects related to safe bicycle use are eligible under STBG, but not under TA (23 U.S.C. 217(a)).
- RTP must benefit recreational trails, but for any recreational trail use. RTP projects are eligible under TA and STBG, but States may require a transportation purpose.
- SRTS: FY 2012 was the last year for SRTS funds, but SRTS funds are available until expended.
- Planning funds must be used for planning purposes, for example:
  - Maps: System maps and GIS;
  - Safety education and awareness: for transportation safety planning;
  - Safety program technical assessment: for transportation safety planning;
  - Training: bicycle and pedestrian system planning training.
- Federal Lands and Tribal Transportation Programs (FLTTP) projects must provide access to or within Federal or tribal lands:
  - Federal Lands Access Program (FLAP): Open to State and local entities for projects that provide access to or within Federal or tribal lands.
  - Federal Lands Transportation Program: For Federal agencies for projects that provide access within Federal lands.
  - Tribal Transportation Program: available for federally-recognized tribal governments for projects within tribal boundaries and public roads that access tribal lands.
- NHTSA 402 project activity must be included in the State’s Highway Safety Plan. Contact the State Highway Safety Office for details: <http://www.ghsa.org/html/about/shsos.html>
- NHTSA 405 funds are subject to State eligibility, application, and award. Project activity must be included in the State’s Highway Safety Plan. Contact the State Highway Safety Office for details: <http://www.ghsa.org/html/about/shsos.html>

### **Cross-cutting notes**

- FHWA Bicycle and Pedestrian Guidance: [http://www.fhwa.dot.gov/environment/bicycle\\_pedestrian/](http://www.fhwa.dot.gov/environment/bicycle_pedestrian/)
- **Applicability of 23 U.S.C. 217(i) for Bicycle Projects:** 23 U.S.C. 217(i) requires that bicycle facilities “be principally for transportation, rather than recreation, purposes”. However, sections 133(b)(6) and 133(h) list “recreational trails projects” as eligible activities under STBG. Therefore, the requirement in 23 U.S.C. 217(i) does not apply to recreational trails projects (including for bicycle use) using STBG funds. Section 217(i) continues to apply to bicycle facilities other than trail-related projects, and section 217(i) continues to apply to bicycle facilities using other Federal-aid Highway Program funds (NHPP, HSIP, CMAQ). The transportation requirement under section 217(i) is applicable only to bicycle projects; it does not apply to any other trail use or transportation mode.
- There may be occasional DOT or agency incentive grants for specific research or technical assistance purposes.
- Aspects of many DOT initiatives may be eligible as individual projects. For example, activities above may benefit Ladders of Opportunity; safe, comfortable, interconnected networks; environmental justice; equity; etc.



