

KODIAK CITY COUNCIL

WORK SESSION AGENDA

Tuesday, April 11, 2017

Kodiak Public Library Multi-Purpose Room

7:30 p.m.

Work sessions are informal meetings of the City Council where Councilmembers review the upcoming regular meeting agenda packet and seek or receive information from staff. Although additional items not listed on the work session agenda are sometimes discussed when introduced by the Mayor, Council, or staff, no formal action is taken at work sessions and items that require formal Council action are placed on a regular Council meeting agenda. Public comments at work sessions are NOT considered part of the official record. Public comments intended for the "official record" should be made at a regular City Council meeting.

Discussion Items

1. Public Comments (limited to 3 minutes)
2. Thelma C Location Presentation
3. Review Final Pedestrian Pathway Plan.....1
4. Discuss Nonprofit Grant Policy57
5. Discuss Attorney Selection Process.....59
6. Elected Officials Training/Travel Requests
7. April 13, 2017, Agenda Packet Review

To Be Scheduled

1. Special Work Session to Discuss Recommendations from the Marijuana Advisory Committee

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MEMORANDUM

To:	City of Kodiak City Council
From:	Michelle Ritter, AICP - DOWL
Date:	April 11, 2017
Subject:	Comments and Updates to the Draft Pedestrian Pathways Plan

The Kodiak Pedestrian Pathways Plan outlines pedestrian infrastructure improvements for cruise ship passengers walking to destinations surrounding the downtown area. The plan was developed over the course of 2016 with two public meetings, one in March and another in May. The draft document was posted for public review from March 16th to April 10th of 2017. The purpose of this work session is to review the draft Plan and public comments before finalizing the Plan for City Council approval. Below is an outline of recommended changes thus far:

- Update to the public involvement summary to include a description of the public posting period for the draft document.
- Update the project call outs in the executive summary graphic so that they are more visible.
- Add the following comments and responses to the comment section of the document; and make noted changes within the Plan:

	Comment	Response
1	We love the wayfinding, lighting and photo opportunities throughout the plan. They are great!	Good to hear.
2	The idea of developing better crossing paths to the library from the bridge and downtown area is definitely a must, this is used frequently.	Great, this comment is consistent with our observations and other feedback we received.
3	The plan of connecting the bridge pathway to Mission Road is really not at all important for our cruise passengers. We advocate fairly heavily that they avoid Mission Road altogether, not only because there is no walking path, but because there are no businesses. While a handful of tourists may decide to take the “unbeaten” route here, it really will not get usage enough to justify those costs. Secondly, many of our cruise passengers have trouble with stairs; so linking the paths with stairs is really a very feasibly idea, either.	<p>Thank you for this insight. Project M1 is a sidewalk on Mission Road that would end next to the St. Herman’s Chapel. We reviewed this proposed project and have determined it should remain as part of the recommendations because:</p> <ul style="list-style-type: none"> • St. Herman’s Chapel is identified as a tourist destination on the Discover Kodiak Map on page 33; • No sidewalk exists along this section of road, and • Cruise ship passengers have been observed walking in the road in this area as seen in image 32 on page 29. <p>Although Discover Kodiak currently does not direct tourists to the Chapel installing a sidewalk here will hopefully allow Discover Kodiak to feel more comfortable to direct cruise ship visitors to see this cultural destination safely.</p> <p>The recommendation for project M2 is to connect Mission Road to the bridge with a section of trails and/or stairs. Without this connection, tourists walking along Mission Road would have to back track to Mill Bay Road in order to reach the bridge. Providing this connection would create a more efficient route to the bridge and give</p>

		them unique scenic vantage points. We agreed that this link will probably be used less than other proposed projects; that is why this project received a '1' for "Use Intensity".
4	Again, the sidewalk on Mission Road will not ever have enough usage to justify spending for CRUISE passengers. What are they walking to?	See response to comment #3 above.
5	The development of a better walking path on Marine Way to the overlook park is a great idea; that spot is undeveloped and untouched, but if there was an opportunity, it would be a nice place to send visitors with a little extra time.	Agreed, this route would be a great walking alternative for visitors that would prefer a flatter route. This area is outside of our Study Area and would require coordination with a variety of private landowners. We recommend that the feasibility of this project be looked at further as a separate analysis.
6	Mission Road needs a sidewalk. The deterioration of the road and its narrow lanes make it very hard to walk to town without getting hit by a vehicle, especially when snow and ice is plowed to its sides. This is a heavily populated community and would benefit greatly with a sidewalk.	This comment is consistent with our observations and other feedback we received. Project M1 is a sidewalk on Mission Road that would end next to the St. Herman's Chapel. This project would improve pedestrian safety at this location.
7	<p>Thank you for the opportunity to commend on the draft Kodiak Pedestrian Pathway Plan.</p> <p>Although I tried to frame my comments from the viewpoint of tourists, I am also thinking of the greater community, particularly residents of Bayview Terrace, Kodiak Plaza, Kodiak Senior Center, and hundreds or perhaps thousands of people who live and work in lower downtown.</p> <p>The scoring criteria seemingly fails to recognize the value of a shorter walking routes to pedestrians. In two of the nine projects, proposed routes that are significantly shorter than existing routes score poorly in the heavily weighted "Use Intensity" category, explaining in both cases that "routes already exist." I believe pedestrians will take the shortest route available to them, particularly when wayfinding is provided, such as has been recommended. Cruise ship passengers, who are generally of an older demographic and with limited time ashore, may particularly appreciate the most efficient walking routes.</p> <p>The cost of individual projects ranges from \$100,000 to \$720,000, which groups them rather closely in terms of public works projects. Assigning a heavily weighted multiplier to a number projects over such a narrow range costs might place undue emphasis on cost, lessening the relative importance of routing the trail to serve critical historical sites, cultural assets, and tourist destinations.</p> <p>The resultant priority list, whether it intends to or not, is biased toward routing the path along the busy street Rezanof Drive between Lower Mill Bay Road</p>	<p>The two projects referred to in the first paragraph of this comment include projects L2 and L3. It is a good point that routes should not be ranked lower just because they are alternative routes, especially when they are shorter routes than those existing. In these instances, the alternative routes are shorter in distance both go through sections of terrain that are steep. The existing route to the library also passes by the Russian Cemetery which is a cultural destination for tourists. This was also factored into the "Use Intensity" score being given a "1" (moderate expected use) for these projects. We will add language in the Project Score table "Scoring Analysis" column that explains the other factors in giving a ranking of "1".</p> <p>Great points regarding the scoring and ranking system of the projects. The ranking was provided as a general guide and order to how projects could be implemented. We agree that the overall goal is to install all projects to improve the entire network. The project ranking should not be considered a strict order of how to implement projects. Therefore, we will add language that although projects are scored and ranked they should all be considered important pedestrian infrastructure improvements:</p> <p><i>"The goal of ranking projects is not to create a rigid order of how projects need to be developed, but to provide an idea of what project might be high impact or quick to implement. All projects should be considered valuable improvements that contribute to the overall pedestrian network. Given the effort, needed cooperation, and funding any project can be developed no matter the recommended order set by this scoring system."</i></p>

<p>and the Fred Zharoff memorial bridge rather than traversing the more peaceful, culturally rich lower downtown. I believe that the projects along Mission Road, Erskine and proximate to the bridge provide a more direct route that serves more tourist destinations. I am in agreement with the proposed continuation of the path across an improved crosswalk at Rezanof Drive and up borough hill toward the Kodiak Public Library.</p> <p>I hope projects will be evaluated by taking a holistic view of the best pedestrian routes through downtown, and not compromise the most sensible route for what may later seem like minimal differences in cost.</p> <p>Whatever the outcome, I am pleased to see each these projects highlighted in a city planning document. I would ask that language be included to emphasize the value in all the projects, as and any of these might become uniquely eligible for specialized funding, and investments in walkable communities provide an unbeatable return on investment.</p>	
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Kodiak Pedestrian Pathway

PLANNING AND DESIGN

**Draft Pedestrian
Pathways Plan for
Public Review**
February 2017



Prepared For: City of Kodiak
Prepared By: DOWL

Kodiak Pedestrian Pathway

PLANNING AND DESIGN



Prepared For:
City of Kodiak
Public Works Department
2410 Mill Bay Road
Kodiak, Alaska 99315



Prepared By:
DOWL
4041 B Street
Anchorage, Alaska 99503
(907)562-2000
W.O. 1124.62085.01

Contents

Executive Summary	1
Section I: Plan Background	3
Introduction	3
Plan Description	3
Planning Context.....	3
Section II: Plan Development	7
Plan Process	7
Existing Conditions.....	7
Public Involvement	10
Summary of Recommendations	15
Section III: Recommendations	15
Project Ranking Criteria	17
Project Description & Scoring Tables	17
General Considerations	32
Section IV: Implementation	35
Implementation Order.....	35
Implementation Steps	35
Conclusion	36
Section V: Appendices	37
Appendix A: Public Comment	38
Appendix B: Federal Pedestrian and Bicycle Funding Opportunities	45

List of Images

Image 1: View of the distant mountains and waterfront from the bridge to Near Island.	3
Image 2: Sidewalk constructed along Shelikof Street from Pier 2 to Jack Hinkle Way.	6
Image 3: Mission Road looking southwest from the Alutiiq Museum.....	7
Image 4: The intersection of Erskine Avenue and Mission Road southwest of the bridge to Near Island.	7
Image 5: Erskine Avenue near the intersection with Mill Bay Road.	7
Image 6: Gravel shoulder along Rezanof Drive that is used as a walkway by pedestrians.	7
Image 7: Vantage Point #1 - View from Mission Road of the Holy Resurrection Russian Orthodox Cathedral.	8
Image 8: Vantage Point #2 – View of the waterfront from Mill Bay Road and Kasheveroff Avenue looking towards the water.	8
Image 9: Vantage Point #3 – View from Mission Road looking northeast under the bridge.	8
Image 10: Vantage Point #4 – View of downtown Kodiak and the waterfront from the bridge to Near Island looking south.	8
Image 11: Vantage Point #5 – View of the water and surrounding mountains from the library social trail.	8
Image 12: Residential area along Mill Bay Road.	9
Image 13: Mission Road looking west from under the bridge to Near Island.	9
Image 14: Social trail on the embankment along Rezanof Drive near the bridge to Near Island.	9
Image 15: Social trail from the bend in Erskine Avenue to the Library.....	9
Image 16: Intersection of Mission Road and Tagura Road.	9
Image 17: Signage on Mission Road to the Alutiiq Museum and to the Senior Center.	10
Image 18: Project presentation at the second open house.....	10
Image 19: Egan Way looking northwest from the library driveway. Proposed sidewalk would be on the left side of the road in this image	19
Image 20: Library entry sign. Proposed Sidewalk would wrap right in front of sign.	19

List of Figures

Figure 1: Isometric sketch of the Study Area and recommended projects.	1
Figure 2: Overall schematic plan.	2
Figure 3: Location map and plan boundary.....	4
Figure 4: Previous and current plans and projects adjacent to the current Study Area.....	5
Figure 5: Site inventory figure of existing conditions.....	8
Figure 6: Consolidated map graphic from the first open house.....	11
Figure 7: Concept routes and votes from the first open house.....	12
Figure 8: Concept projects and votes from the second open house.	13
Figure 9: Consolidated graphic comments from the second open house.	14
Figure 10: Isometric sketch of the Study Area and recommended projects.	15
Figure 11: Overall schematic plan.	16
Figure 12: Library connections.....	18
Figure 13: Bridge connections.....	24
Figure 14: Sections of potential stair types including concrete, metal, and wood.	26
Figure 15: Rezanof Drive sidewalk section.....	27
Figure 16: Mission road connections.	28
Figure 17: Other opportunities - wayfinding.	31
Figure 18: Potential waterfront connections.	32
Figure 19: Discover Kodiak visitor map.	33
Figure 20: Diagram of how pedestrian routes on the Discover Kodiak Map could be updated once projects are implemented.	34
Figure 21: Potential pedestrian nodes with frame structures that take advantage of views.	34

List of Tables

Table 1: Broad plans reviewed for pertinent information	6
Table 2: Plans specific to pedestrian infrastructure adjacent to the study area.....	6
Table 3: Construction and design that resulted from previous pedestrian plans.....	6
Table 4: How ranking criteria is scored and weighted.	17

Table 5: Project L1 Description	19
Table 6: Project L1 Score	19
Table 7: Project L2 Description	20
Table 8: Project L2 Score	20
Table 9: Project L3 Description	21
Table 10: Project L3 Score	21
Table 11: Project L4 Description	22
Table 12: Project L4 Score	22
Table 13: Project L5 Description	23
Table 14: Project L5 Score	23
Table 15: Project B1 Description	25
Table 16: Project B1 Score	26
Table 17: Project B2 Description	27
Table 18: Project B2 Score	27
Table 19: Project M1 Description	29
Table 20: Project M1 Score.....	29
Table 21: Project M2 Description	30
Table 22: Project M2 Score.....	30
Table 23: Project O1 Description	31
Table 24: Project O1 Score.....	31
Table 25: Port of Kodiak, cruise ship schedule, 2016.	33

List of Acronyms

ADF&G	Alaska Department of Fish and Game
BIA	Bureau of Indian Affairs
COK	City of Kodiak
DOT&PF	Department of Transportation and Public Facilities (State of Alaska)
FLH	Office of Federal Lands Highway
FHWA	Federal Highway Administration
FTA	Federal Transit Administration
ITN	Island Trails Network
KIB	Kodiak Island Borough
NEPA	National Environmental Protection Agency
ROW	Right-of-way
TIGER	Transportation Investment Generating Economic Recovery
US DOT	United States Department of Transportation
USFWS	United States Fish and Wildlife Service
WFLHD	Western Federal Lands Highway Division

EXECUTIVE SUMMARY

A waterfront town tucked along the coast of Kodiak Island, the City of Kodiak (COK) is a destination for cruise ships that tour Alaska's waters. The city receives an annual influx of tourists during the cruise-ship season, April through September. Over this six-month period in 2016, the city welcomed 19,224 cruise ship passengers, according to the 2016 cruise ship schedule. In order to accommodate these visitors the State of Alaska distributes the Commercial Passenger Vessel Excise Tax, a cruise-ship "head tax", to coastal cities hosting cruise ships. As a recipient, the COK has completed plans and projects aimed at improving the pedestrian infrastructure starting at Pier 2, where cruise ships dock.

This Pedestrian Pathways Plan builds on previous efforts by focusing on pedestrian improvements between downtown and periphery tourist attractions; specifically a Study Area from Center Avenue, to the Fred Zharoff Memorial Bridge (referenced as the bridge to Near Island), and to the Kodiak Public Library (referenced as the library).

The steps to develop this plan included: researching previous plans in and around the area, walking the site to inventory existing conditions, analyzing opportunities and constraints, collecting feedback through a public involvement process, developing a list of potential projects, and analyzing that list to determine an implementation strategy.

The public involvement process included two open houses, collecting feedback through an online comment form and project email, and discussions with stakeholders. Based on research and feedback, recommendations for proposed projects within the Study Area were developed within four categories: library connections, bridge connections, Mission Road connections, and other opportunities.

Ten specific projects were identified:

- L** Library Connections
 - Sidewalk on Egan Way (L1)
 - Trail from Erskine Avenue to the Kodiak Public Library (L2)



Figure 1: Isometric sketch of the Study Area and recommended projects.

The ten projects were then scored based on these criteria:

- Trail from Rezanof Drive to Erskine Avenue (L3)
 - Widen the Sidewalk on Rezanof Drive (L4)
 - Crosswalk Improvements on Rezanof Drive (L5)
- B** Bridge Connections
- Connection from Lower Erskine Avenue to Rezanof Drive (B1)
 - Pave the Sidewalk on Rezanof Drive (B2)

- M** Mission Road Connections
- Sidewalk on Mission Road (M1)
 - Trail from Mission Road to the Bridge Connection (M2)

- O** Other Opportunities
- Wayfinding (O1)

- 8. Connection from Lower Erskine Avenue to Rezanof Drive (B1)
- 9. Trail from Mission Road to the Bridge Connection (M2)
- 10. Widen the Sidewalk on Rezanof Drive (L4)

Beyond the 10 (ten) specific projects these general recommendations were also outlined: connections outside of the Study Area (specifically waterfront connections), coordination with Discover Kodiak, and pedestrian design recommendations.

This Pedestrian Pathways Plan provides recommendations which, if implemented, can improve safety and the pedestrian experience for tourists and locals walking from downtown, to the bridge to Near Island, to the library, and to the surrounding amenities.

The ranking criteria resulted in the following order:

1. Crosswalk Improvements on Rezanof Drive (L5)
2. Wayfinding (O1)
3. Sidewalk on Egan Way (L1)
4. Pave the Sidewalk on Rezanof Drive (B2)
5. Sidewalk on Mission Road (M1)
6. Trail from Erskine Avenue to the Kodiak Public Library (L2)
7. Trail from Rezanof Drive to Erskine Avenue (L3)

Overall Schematic



Library Connections

- L1 - Sidewalk on Egan Way
- L2 - Trail from Erskine Ave. to the Kodiak Public Library
- L3 - Trail from Rezanof Dr. to Erskine Ave.
- L4 - Widen the Sidewalk on Rezanof Dr.
- L5 - Crosswalk Improvements on Rezanof Dr.



Bridge Connections

- B1 - Connection from Lower Erskine Ave. to Rezanof Dr. (Options A & B)
- B2 - Pave the Sidewalk on Rezanof Dr.



Mission Road Connections

- M1 - Sidewalk on Mission Rd.
- M2 - Trail from Mission Rd. to the Bridge Connection



Other Recommendations

- O1 - Wayfinding

- Study Area
- Potential Connections
- Existing Sidewalks or Connections

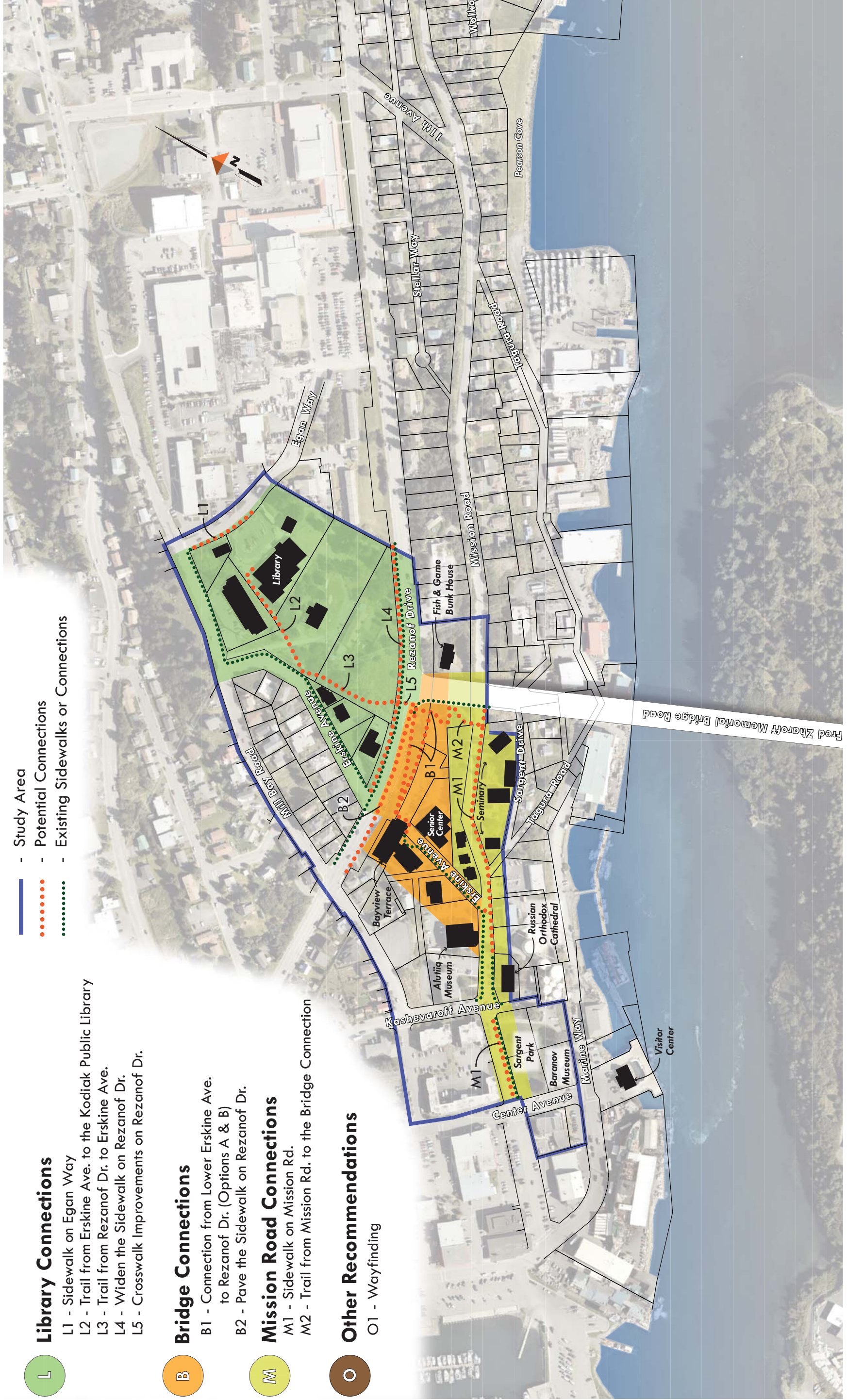


Figure 2: Overall schematic plan.

SECTION I: PLAN BACKGROUND

Introduction

The City of Kodiak (COK) is a coastal town located on the northeast shoreline of Kodiak Island. The city is the most populated area of the island, with a population of 6,253¹. As such it is the island's main transportation hub, with a nearby airport and extensive dock infrastructure including Piers 1 and 2, which serve visitors. Pier 1 serves the Alaska Marine Highway vessels, which are used by residents and visitors, many of which are pedestrians, while Pier 2 is the docking point for visiting cruise ships.

In 2016, the COK was scheduled to receive 19,224 cruise ship passengers over a six-month period, from April through September. The 2016 schedule accounted for thirteen vessels, operated by three different cruise lines, with passenger totals ranging from 382 to 1,898 per ship. In order to assist in accommodating this influx of visitors, the State of Alaska collects and distributes the Commercial Passenger Vessel Excise Tax, a cruise-ship "head tax", to towns like the COK. This excise tax is intended to be used for improvements to cruise ship passenger related infrastructure. Past projects in coastal Alaskan towns include improvements to docks and harbors, sidewalks and sea walks (waterfront pedestrian facilities), and the development of waterfront master plans among others².

As a recipient the COK has used the tax to fund plans and projects aimed at improving pedestrian connections for these visitors. Also funded by the cruise ship tax, this Pedestrian Pathways Plan continues this effort to improve facilities by analyzing and making recommendations regarding pedestrian connections to visitor destinations within walking distance of the COK's downtown.

¹ United States Census. *Quick Facts Population Estimate as of July 1, 2015*. Retrieved from: <http://www.census.gov/quickfacts/table/P5T045215/0240950>

² Alaska Department of Commerce, Community, and Economic Development. "Commercial Passenger Vessel Excise Tax: Community Needs, Priorities, Shared Revenue, and Expenditures Fiscal Years 2007 – 2014". January 2014.

Plan Description

The purpose of the Pedestrian Pathways Plan is to evaluate options and to make recommendations for improved pedestrian links from Center Avenue to the Fred Zharoff Memorial Bridge (referenced as the bridge to Near Island), and to the Kodiak Public Library (library). The Study Area, as outlined in Figure 1, is bordered by Center Avenue to the southwest, Mill Bay Road to the west, Egan Way to the north, and Mission Road to the east and south. Adjacent to the downtown area and waterfront, this Study Area includes several of the COK's main attractions: the Alutiiq Museum, the library, and the pedestrian link to Near Island. Because of these attractions, tourists and locals alike circulate this area on foot. Many of the streets within the area have sidewalks. Some walks are narrow and some streets lack sidewalks.

Planning Context

PREVIOUS PLANS & PROJECTS

To better understand the context of the Study Area we reviewed previous plans and projects relevant to this plan.

These plans and projects fall into two categories:

- Plans that are broad in scope, covering regional and city-wide areas and topics.
- Previous plans and projects specific to pedestrian infrastructure near the current Study Area. These plans focused on specific pedestrian improvements from Pier 2, where cruise ship passengers disembark, through downtown.

The plans specific to pedestrian improvements resulted in projects including:

- The construction of a sidewalk from Pier 2 to Jack Hinkle Way.
- Additional bulk head parking off of Shelikof Street, along St. Paul Harbor, is currently in the design process.



Image 1: View of the distant mountains and waterfront from the bridge to Near Island.

Figure 2 outlines the areas that previous plans adjacent to the Study Area covered. Tables 1, 2 and 3 outline the plans and projects that were reviewed for relevant information.

- Title 15 – Buildings and Construction
- Title 16 – Subdivision
- Title 17 – Zoning Code.

and design are in the Kodiak Island Borough Code (KIBC). Sections of code that are relevant to this plan include:

REGULATION & CODE

The City of Kodiak (COK) is an incorporated home-rule city in the Kodiak Island Borough (KIB). The KIB is classified as a Second Class Borough with area-wide powers that include planning, platting and land use regulation. Therefore, ordinances pertinent to planning

For this plan to be adopted as an official Guiding Document it will need to be approved by the Planning and Zoning Commission of KIB.

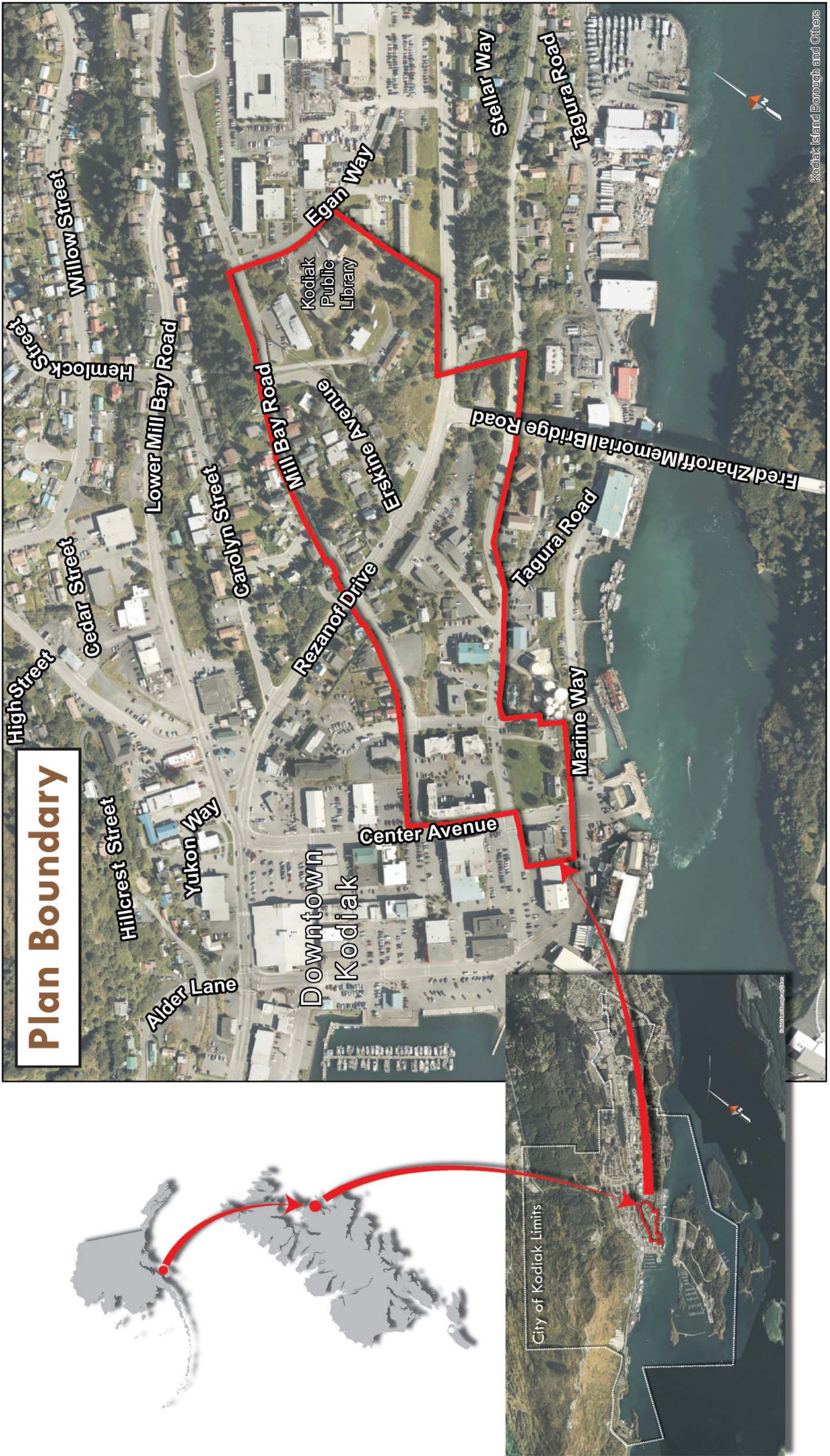


Figure 3: Location map and plan boundary.

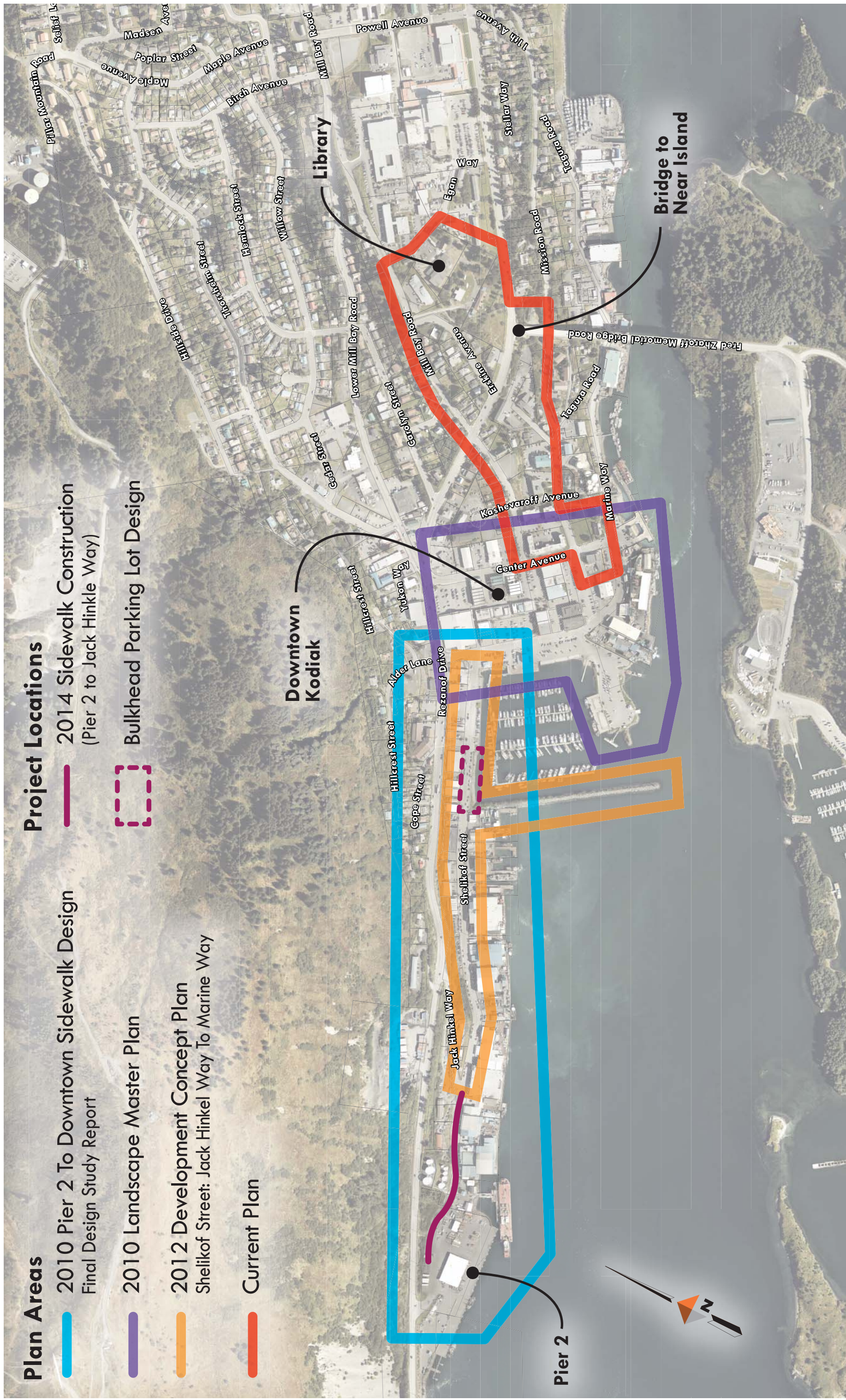


Figure 4: Previous and current plans and projects adjacent to the current Study Area.

Broad Plans Reviewed for Pertinent Information

<p>2002 Community Design Workshop Final Report</p>	<ul style="list-style-type: none"> Design charrette focused on developments in downtown Kodiak and Near Island; included recommendations specific to pedestrian enhancements: <ul style="list-style-type: none"> Crosswalk and sidewalk improvements within the Study Area. Sidewalk along Mission Road from Erskine Avenue to under the bridge to Near Island. Trail from the new Mission sidewalk under the bridge up to bridge entrance (stairway improvement right along either side of bridge abutment). Sidewalk on south-side of Rezanof Drive from Mill Bay Road to bridge. Widen sidewalks on north-side of Rezanof Drive from downtown past this Study Area. Improve pedestrian safety by narrowing roadways, providing curb extensions, and more striped crosswalks.
<p>2008 Kodiak Island Borough Comprehensive Plan</p>	<ul style="list-style-type: none"> Comprehensive plan describing the cultural, political, economic and environmental aspects of the Island, adopted by the Borough to guide planning decisions. Broad goals specific to pedestrian facilities such as: “incorporate creation of bicycle lanes or paths into road construction or improvement projects, where feasible, and incorporate creation of bicycle and pedestrian pathways in residential and other development projects”.
<p>2010 COK Waterfront Master Plan</p>	<ul style="list-style-type: none"> Overview of waterfront facilities’ existing conditions and desired improvements. Recommendations for open space development including amenity improvements to Pearson Cove.
<p>2011 Kodiak Road System Trails Master Plan</p>	<ul style="list-style-type: none"> Trails and pedestrian circulation inside and outside the urban context throughout Kodiak Island. Specific Project Recommendations (From Map 11 Proposed Improvements – Urban System, Proposed Trail Classification): <ul style="list-style-type: none"> Trail between Mission Road and Marine/Tagura under the bridge (Class 3b trail¹). Pedestrian Trail along Mission Road to Mission Beach (Class 5 trail²). <p>¹Trail classes are defined by many factors, please reference the Kodiak Road System Trails Master Plan for a complete description. In general the classes referenced here are:</p> <ol style="list-style-type: none"> Class 3b Trail: Developed/improved trail, typically constructed of native materials, trail experience is natural/semi-primitive. Class 5 Trail: Fully developed trail, rural to urban, constructed of asphalt or hardened surface.
<p>2015 Downtown Kodiak Revitalization Action Initiatives</p>	<ul style="list-style-type: none"> Key projects noted by the Downtown Kodiak Revitalization Special Committee, including the following within the Study Area: <ul style="list-style-type: none"> Install stairs up from Senior Citizens of Kodiak parking lot to bridge and crosswalk from bridge across Rezanof. Make a lot at Kashevaroff and Mill Bay into a small children’s park. Increase crosswalks as designed in 2002 Community design workshop report. Convert block of Marine Way between Sargent Park and Refuge into pedestrian historic core area.

Table 1: Broad plans reviewed for pertinent information

Plans Specific to Pedestrian Infrastructure Adjacent to the Study Area

<p>2010 Pier 2 to Downtown Sidewalk Design Final Design Study Report</p>	<ul style="list-style-type: none"> Analyzed the potential sidewalk improvements to allow cruise ship passengers to walk the waterfront to downtown. Prioritized a series of projects within the area. Catalyst for sidewalk construction and bulkhead parking improvements.
<p>2010 COK Landscape Master Plan for the Downtown Water, Sewer and Storm Drain Master Plan</p>	<ul style="list-style-type: none"> Master plan of pedestrian facilities in the downtown core of Kodiak. Identifies pedestrian and bicycle routes that link to this Study Area.
<p>2012 Development Concept Plan Shellikof Street: Jack Hinkle Way to Marine Way</p>	<ul style="list-style-type: none"> Developed design specific to pedestrian and parking improvements in area of focus. Concept design for bulkhead parking lot.

Table 2: Plans specific to pedestrian infrastructure adjacent to the study area.

Construction & Design That Resulted From Previous Pedestrian Plans

<p>2014 Sidewalk Construction</p>	<ul style="list-style-type: none"> Construction of the sidewalk from Pier 2 to Jack Hinkle Way (see the image below).
<p>2016 Bulkhead Parking Lot Design</p>	<ul style="list-style-type: none"> Current design for the construction of bulkhead parking off Shellikof Drive near downtown.

Table 3: Construction and design that resulted from previous pedestrian plans.



Image 2: Sidewalk constructed along Shellikof Street from Pier 2 to Jack Hinkle Way.

SECTION II: PLAN DEVELOPMENT

Plan Process

The development of this plan included research of previous plans, on-site visits, public meetings and analysis of the area to determine specific projects that will improve pedestrian circulation. The planning process started at the end of 2015 and will run through beginning of 2017. The steps to develop this plan have included:

- Research of the Study Area
 - Reviewing Previous Plans
 - Site Walks
 - Inventory of Existing Conditions
- Collecting feedback on existing conditions and preferred areas of focus. This included feedback from the first public open house on March 8, 2016 and online feedback.
- Developing draft recommendations.
- Presenting the draft recommendations and collecting feedback. Starting with the second open house on May 23, 2016, and continuing through the end of September 2016.

15

- Presenting the draft recommendations to the City Council on August 9, 2016.
 - Developing the Draft Kodiak Pedestrian Pathways Plan.
- This document is the draft plan for public review. Moving forward the next steps include:
- Collect feedback on the draft plan through January 2017 (current document).
 - Incorporate feedback.
 - Develop final plan.

Existing Conditions

To understand the opportunities and constraints related to pedestrian infrastructure, information on existing conditions was collected from public data, from site visits, and from public feedback.

Figure 3 and the subsections below outline and depict the existing conditions information cataloged in six categories:

Street character was used as an evaluation category because it can help determine the best route for tourists. Street character is a subjective description of how a road might feel as a pedestrian and can be the result of many things: how wide a road is; whether or not there are pedestrian amenities; or the existence of street trees. For this study two street attributes were considered significant character contributors: adjacent landuse and traffic.

Based on these criteria streets in the Study Area fall into three groups:

1. High volume/fast traffic surrounded by mostly commercial landuses
 - Rezanof Drive
2. Low volume/slow traffic surrounded by mostly commercial and institutional landuses
 - Mission Road
 - Egan Way
 - Center Avenue
 - Erskine Avenue (south of Rezanof Drive)
3. Low volume/slow traffic surrounded by mostly residential landuses
 - Erskine Avenue (north of Rezanof Drive)
 - Mill Bay Road
 - Kasheveroff Avenue

Low volume/slow traffic provide a context for a safe and comfortable walk. While residential streets might not warrant additional tourist foot-traffic. The preferable routes for tourists will most likely be streets with low volume/slow traffic and with adjacent commercial and institutional landuses.

TERRAIN & VANTAGE POINTS

The Study Area is a south-facing slope between the COK's waterfront and the northeast base of Pillar Mountain. There is an overall elevation change of approximately 150 feet. The elevation starts at 20 feet (adjacent to the Visitor Center), and rises to above 170 feet (at the library). The bridge to Near Island, halfway between these high and low points, sits at approximately 115 feet. This elevation change provides scenic vantage points, but steep terrain can make pedestrian connections challenging.



Top Left:
Image 3: Mission Road looking southwest from the Alutiq Museum.



Top Right:
Image 4: The intersection of Erskine Avenue and Mission Road southwest of the bridge to Near Island.



Bottom Left:
Image 5: Erskine Avenue near the intersection with Mill Bay Road.



Bottom Right:
Image 6: Gravel shoulder along Rezanof Drive that is used as a walkway by pedestrians.

Vantage Points

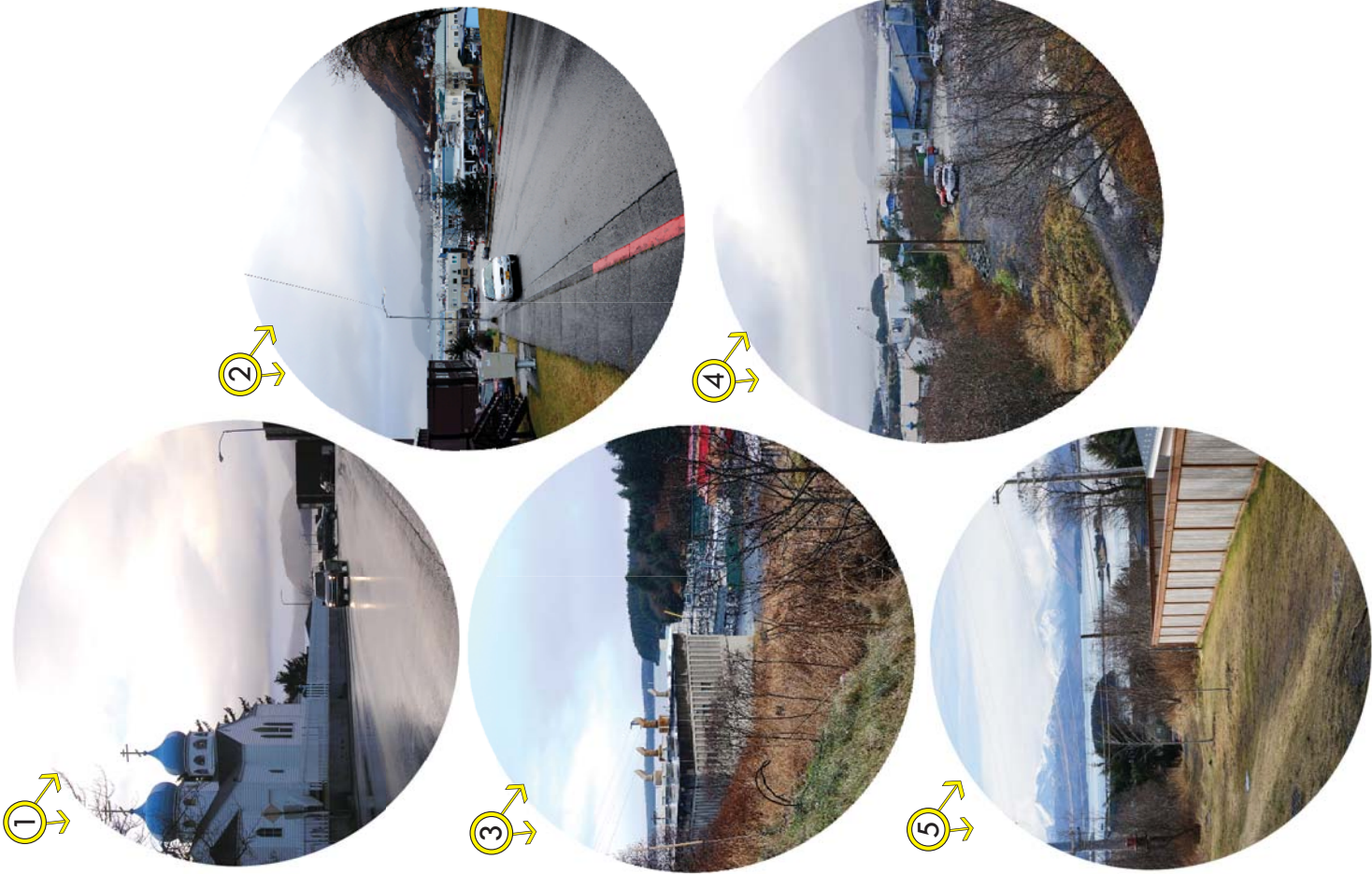


Image 7: Vantage Point #1- View from Mission Road of the Holy Resurrection Russian Orthodox Cathedral.

Image 8: Vantage Point #2 – View of the waterfront from Mill Bay Road and Kasheveroff Avenue looking towards the water.

Image 9: Vantage Point #3 – View from Mission Road looking northeast under the bridge.

Image 10: Vantage Point #4 – View of downtown Kodiak and the waterfront from the bridge to Near Island looking south.

Image 11: Vantage Point #5 – View of the water and surrounding mountains from the library social trail.

Existing Conditions



Figure 5: Site inventory figure of existing conditions.

PEDESTRIAN CIRCULATION

Tourists and residents circulate through the Study Area on foot to access attractions within it and beyond. Pedestrian routes are either along established paved sidewalks or on undeveloped paths through right-of-way, public land, or private properties. Figure 3 shows existing paved sidewalks, pedestrian related signage, shoulders along roads that are used by pedestrians, and social trails. Social trails are informal dirt trails that were not constructed or legally designated, but became distinct overtime by repeated use. The locations of social trails indicate connections that people are making where no established trail or sidewalk exists. The Study Area has several of these social trails as depicted by the dotted orange lines in Figure 3. The social trails indicate where a new sidewalk or trail improvements would formalize connectivity.

ZONING, LAND USE AND OWNERSHIP

Zoning, land use, and ownership information comes from the Kodiak Peninsula Borough Geographic Information System data. The five zoning districts found in the Study Area include the Retail Business (RB) District, Public Use Lands (PL) District, Conservation (C) District, Two-Family Residential (R-2) District, and Multi-Family (R-3) District. The zoning districts are generally established to guide land use and development decisions.

The RB District is established for the purpose of providing for a wide range of retail and service businesses for the consumer population of large segments of the community. Because of the potential for heavy traffic and the appearance and performance of these uses, this district is located on the periphery of residential areas and at the intersections of arterial and major collector streets and roads.

Within the Study Area, the RB District is generally located south of Rezanof Drive and includes a mix of land uses, including multi-family residential, institution, church, business residential, and business.

The PL District is established as a land use district for publicly owned land containing recreational, educational and institutional uses. There are several areas zoned PL District within the Study Area: the Baranov Museum and surrounding parcels and the Kodiak Public Library, owned by the COK, are examples of institution land use.

The Kodiak Island Housing Authority owns a multi-family residential building adjacent to the library. The parcel on the corner of Egan Way and Mill Bay Road is owned by the KIB and used for institutional land use. Alascom Incorporated owns a parcel south of the library, which is designated institution use. The Federal Government owns a parcel north of Rezanof Drive. Additionally, the Federal Bureau of Land Management owns a vacant parcel on Mill Bay Road.

The C District is established for the purpose of maintaining open space areas while providing for single-family residential, and limited commercial land uses. Within the Study Area, the C District includes a parcel below the bridge to Near Island owned by the U.S. Fish and Wildlife Service. This parcel is currently permitted to the State of Alaska Department of Transportation and Public Facilities (DOT&PF) for the use and maintenance of the bridge. The bunk house on this parcel is used to house State of Alaska Fish and Game employees.

The R-3 District is established as a land use district for one-, two-, and multi-family dwellings and limited office uses where public water and sewer services are available. The purpose of the zoning district is to encourage residential land use, prohibit commercial and industrial uses, and discourage uses that are incompatible with residential uses.

Within the Study Area, the R-3 District is generally located southeast of the intersection of Mill Bay Road and Rezanof Drive. Several parcels are privately owned and developed with single-family residences. Two parcels are owned by the Community Baptist Church and developed with a church and parking lot.

The R-2 District is established as a land use district for single-family and two-family residential dwellings and limited office uses where public water and sewer services are available. The purpose of the zoning district is to encourage low density residential land use, prohibit commercial and industrial uses, and discourage uses that are incompatible with residential use.

Within the Study Area, the R-2 District is located northeast of the intersection of Mill Bay Road and Rezanof Drive and along Erskine Avenue. The parcels in the Study Area are privately owned single-family residences.



Image 12: Residential area along Mill Bay Road.



Image 13: Mission Road looking west from under the bridge to Near Island.

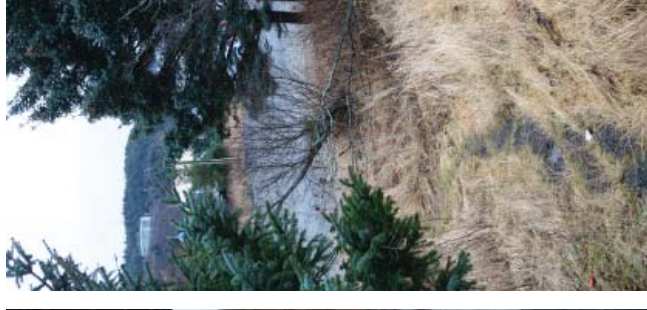


Image 14: Social trail on the embankment along Rezanof Drive near the bridge to Near Island.



Image 15: Social trail from the bend in Erskine Avenue to the Library.



Image 16: Intersection of Mission Road and Tagura Road.

UTILITIES

Street lighting exists in several locations within the Study Area, such as along Mill Bay Road and Rezanof Drive. The section of Mission Road, between Center Avenue and Kashevaroff Avenue, has street lights. Center Avenue between Mission Road and Marine Way also has street lighting. Erskine and Mission Road, north of Kashevaroff Avenue, do not have street lighting. There is no pedestrian-scale lighting developed within the Study Area.

In general, the Study Area is served with basic utilities. Water and sewer service is provided by the COK, Public Works Department. The Kodiak Electric Association provides power to the COK. Overhead power-lines run through the Study Area from the transmission station located northwest of the Study Area. Telecommunication services are provided by General Communications, Incorporated.

POINTS OF INTEREST

The Study Area includes some of the most notable attractions in the COK. Center Street is a main street through downtown. At its southeast end it passes the Kodiak National Wildlife Refuge Visitor Center, the Baranov Museum and dead-ends at the Kodiak Chamber of Commerce Visitor Center. Mission Road, which runs perpendicular to Center Avenue, provides access to Sargent Park, the Holy Resurrection Russian Orthodox Cathedral, that Alutiiq Museum, and St. Herman's Chapel. Buses drop cruise-ship visitors along this section of Mission Road due to the density and variety of attractions (as indicated by the 'Points of Interest' on Figure 3). The north end of the Study Area includes the library, the Russian Cemetery, and government offices which draw tourists and locals alike. This area also plays a role in access to nearby festivals and markets, including:

- Kodiak Brown Bear Festival, in March, hosted by the Kodiak Wildlife Refuge.
- Kodiak Whale Festival, from April through May, observed in a variety of venues including the Kodiak Senior Center, the Baranov Museum, the Kodiak Wildlife Refuge.
- Kodiak Crab Festival, May (Memorial Day Weekend), at venues located throughout downtown Kodiak.
- 1st Friday Art Crawls, observed monthly, in coffee shops and galleries.
- Pilgrimage to St. Herman, on to Spruce Island. Special services are held at the Holy Resurrection Orthodox Cathedral.
- Kodiak Bear Town Art & Crafts Market, operates periodically throughout the year, at the Su'naq Tribe bingo Hall.
- Ongoing Senior Center events, including the Arts & Crafts Fair in November.



Image 17: Signage on Mission Road to the Alutiiq Museum and to the Senior Center.

EXISTING CONDITIONS SUMMARY

The information on existing conditions provided a baseline on what pedestrian infrastructure exists and what is lacking. For example, although several streets have sidewalks, there are areas where people walk that lack a formal connection (i.e. social trails that cut through vegetated areas outside of right-of-way or streets that lack sidewalks). The existing conditions information was shared at the first public open house. The public provided feedback and added their own observations on the area. This information also served as baseline for determining areas of need and in which areas tourists might prefer to walk. Each proposed project in the Recommendations section has a description of the existing conditions that factored into the project being included in the list.

Public Involvement

The public involvement process included two open houses, collecting comments from an online project comment form, and from a project email address, as well as from discussions with stakeholders. These forums were used to:

- inform the public about the plan,
- listen to local perspectives on the area,
- define needed improvements, and
- collect feedback on the plan.

OPEN HOUSE #1

The first open house took place on March 8, 2016, from 6-9pm at the downtown Kodiak Harbor Convention Center. The meeting was attended by 24 residents as well as project representatives from DOWL and the COK. The goal of this meeting was to introduce the project, and collect feedback on existing conditions, determine the preferred routes for upgrades, and understand what types of enhancements are needed.

Two feedback exercises were conducted during the meeting: collecting comments on maps, and a 'Concept Route' voting exercise. For the mapping exercise attendees wrote and drew what they saw as issues and opportunities on aerials of the study area. These comments are consolidated in Figure 4.



Image 18: Project presentation at the second open house.

For the 'Concept Route' voting exercise, a map was provided showing various routes, some existing and some proposed. Attendees were asked to vote for their two preferred routes for pedestrian infrastructure improvements. Attendees were encouraged to provide additional routes if their preferred route was not shown. Vote tallies revealed the top three preferred routes for improvements were:

- 'Route C': From Center Street, to Mission Road, to Erskine Avenue, to the Near Island bridge
- 'Route D': From Center Street, to Mission Road, to the Near Island bridge
- 'Route G': Crossing Rezanof Drive, then to the library via an alternative trail

Comments and information collected during this first phase of feedback coalesced into pedestrian project priorities, including:

- Specific Location Improvements:
 - Route C; improvements from Mission Road, up Erskine Ave, up to the bridge

- Route D; improvements along Mission Road, then up under the bridge
- Route G; alternative trail from Rezanof Drive to the library.
- Improve crossing safety at Rezanof Drive from the bridge.
- Route to Pearson Cove

- General Improvement Considerations:
 - Improve wayfinding
 - General concern for pedestrian safety
 - Cruise-ship passenger drop-off should be considered and programmed accordingly

Based on the comments from this open house and online we developed a list of potential projects which were presented at the second open house.

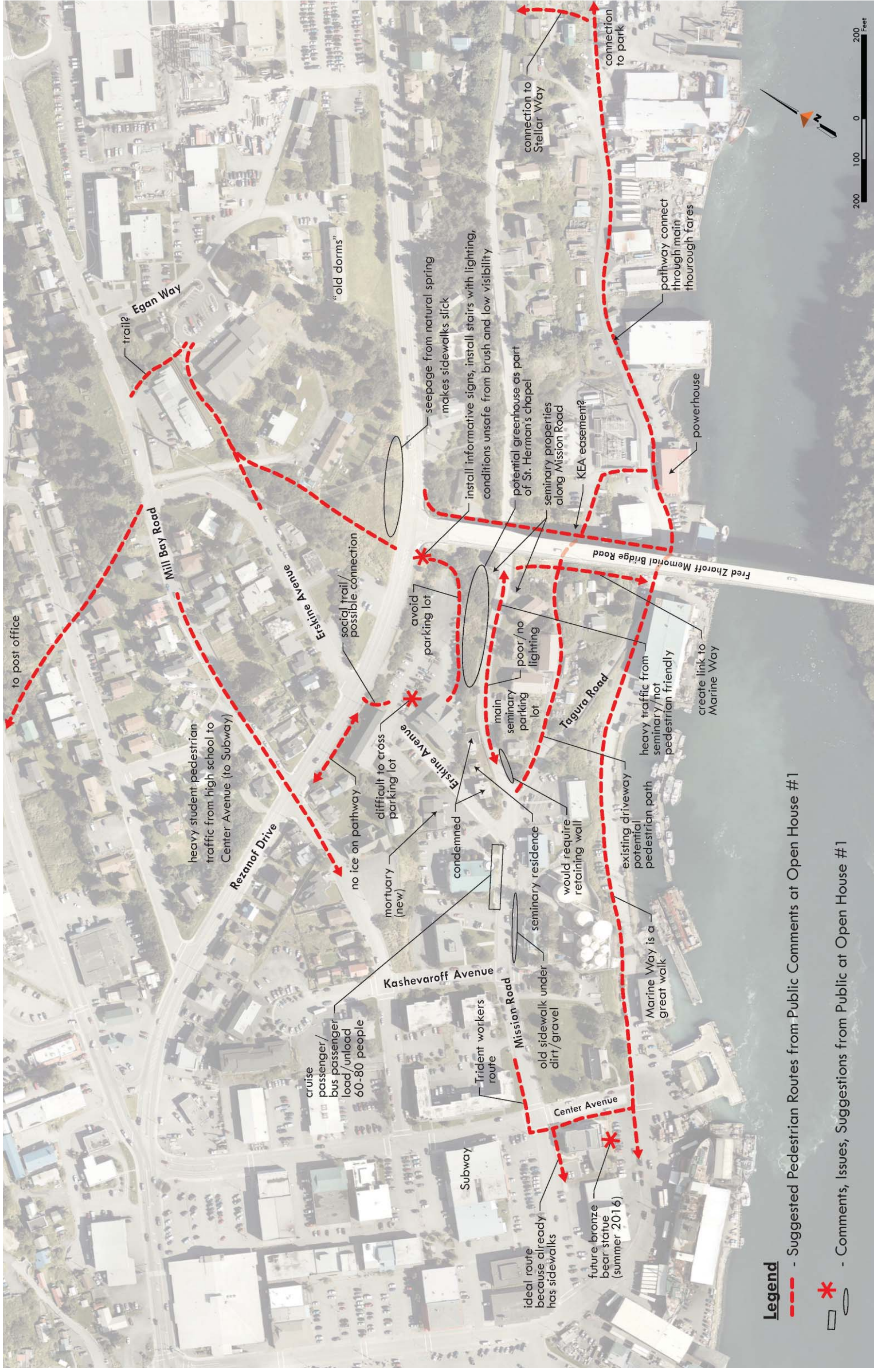
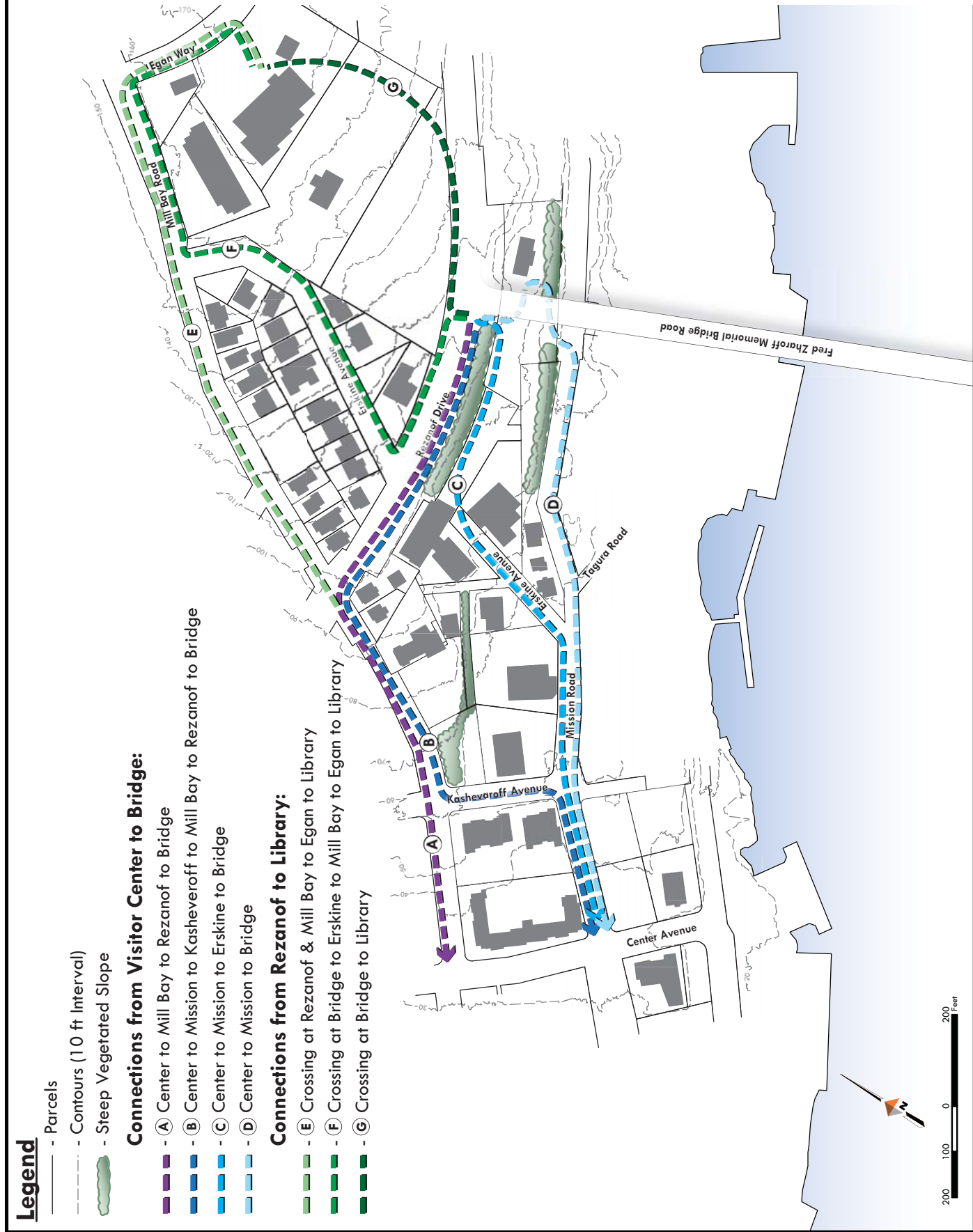


Figure 6: Consolidated map graphic from the first open house.



● - Votes from Meeting
 ■ - Votes from Comments

 Concept Routes Place dot next to identified route below to indicate which you feel should be considered highest priority for improvements.		Total
(A) Center to Mill Bay to Rezanof to Bridge		0
(B) Center to Mission to Kasheveroff to Mill Bay to Rezanof to Bridge		2
(C) Center to Mission to Erskine to Bridge		11
(D) Center to Mission to Bridge		8
(E) Crossing at Rezanof & Mill Bay to Egan to Library		1
(F) Crossing at Bridge to Erskine to Mill Bay to Egan to Library		0
(G) Crossing at Bridge to Library		8
(H) Preferred Route Not Shown Place dot(s) in this row, then indicate /comment on the table plot with your preferred route		1

Figure 7: Concept routes and votes from the first open house.

OPEN HOUSE #2

The second open house was held on May 23rd, again from 6-9pm at the downtown Kodiak Harbor Convention Center. There were 12 attendees, some had been to the first open house, but the majority were participating for the first time. Drafts of potential pedestrian improvement projects were presented. The proposed pedestrian improvements included adding sidewalks and trails, and were developed based on open house and online feedback.

Using the same format as the first meeting, two interactive exercises were held as detailed in the following paragraphs. During the first exercise, attendees participated in a voting exercise. Each attendee was provided with three sticker dots and asked to place the dots next to the projects they felt should be highest priority. Prior to the exercise, it was explained to participants that this would be one of several ranking criteria that would be included in the scoring matrix. The top priority projects identified through the voting exercise were:

1. Crosswalk on Rezanof Drive (7 votes)
2. Connection from Lower Erskine Avenue to Rezanof Drive (4 votes)
3. Sidewalk on Egan Way (4 votes)
4. Wayfinding (4 votes)

For the second exercise attendees were asked to write down any issues or concerns regarding the draft Overall Schematic Map depicting all the potential projects, as shown on Figure 7. General feedback included:

- Safety concerns regarding crossing Rezanof Drive
- Limited right-of-way along Marine Way might make it hard to provide a pedestrian facility (route outside of Study Area)
- Safety concerns for pedestrians and bike riders along Mission Road
- Better wayfinding needed, especially to get tourists to the library

SUMMARY OF PLAN DEVELOPMENT

The Pedestrian Pathways Plan was developed using background research, inventory of existing conditions and the public involvement processes.

Steps in developing the Plan included:

1. Site Inventory: provided a baseline understanding of the opportunity and constraints within the Study Area.
2. Site Walk: site observations and seeing socials trails indicated areas of need. Those observations were shared at the first open house.
3. First Open House: the public provided feedback on areas to for the planners focus on and what specific projects should be considered.
4. Feedback: feedback was used to establish a list of potential projects. The list was presented at the second public open house.
5. Second Open House: feedback from the second open house was used to develop the final list of recommendations.

Each step contributed to the final list of recommendations.



  Vote for Your Top Projects Place dot next to projects you feel should be considered highest priority	
Library Connections	
L1	Sidewalk on Egan Way ★★★★★
L2	Trail from Erskine Ave. to Library ★★
L3	Trail from Rezanof Dr. to Erskine Ave.
L4	Widen Sidewalk on Rezanof Dr.
L5	Rezanof Crosswalk Improvements ★★★★★★
Bridge Connections	
B1	Connection from Lower Erskine Ave. to Rezanof Dr. ★★★★★★
B2	Pave Sidewalk on Rezanof ★★
Mission Connections	
M1	Sidewalk on Mission Road ★★
M2	Connection from Mission to Bridge Connection
Waterfront Connections	
W1	Connection from Mission to Marine Way (Under Bridge) ★★★★★
W2	Sidewalk on Marine Way From Center Ave. to Pearson Cove
Other Opportunities	
O1	Branding
O2	Wayfinding ★★★★★
O3	Photo Opportunities
O4	Park at Mill Bay & Kashevaroff
O5	Historic Core Area
Other Projects Not Shown Write note next to dot.	

Figure 8: Concept projects and votes from the second open house.

Public Comments From Open House #2

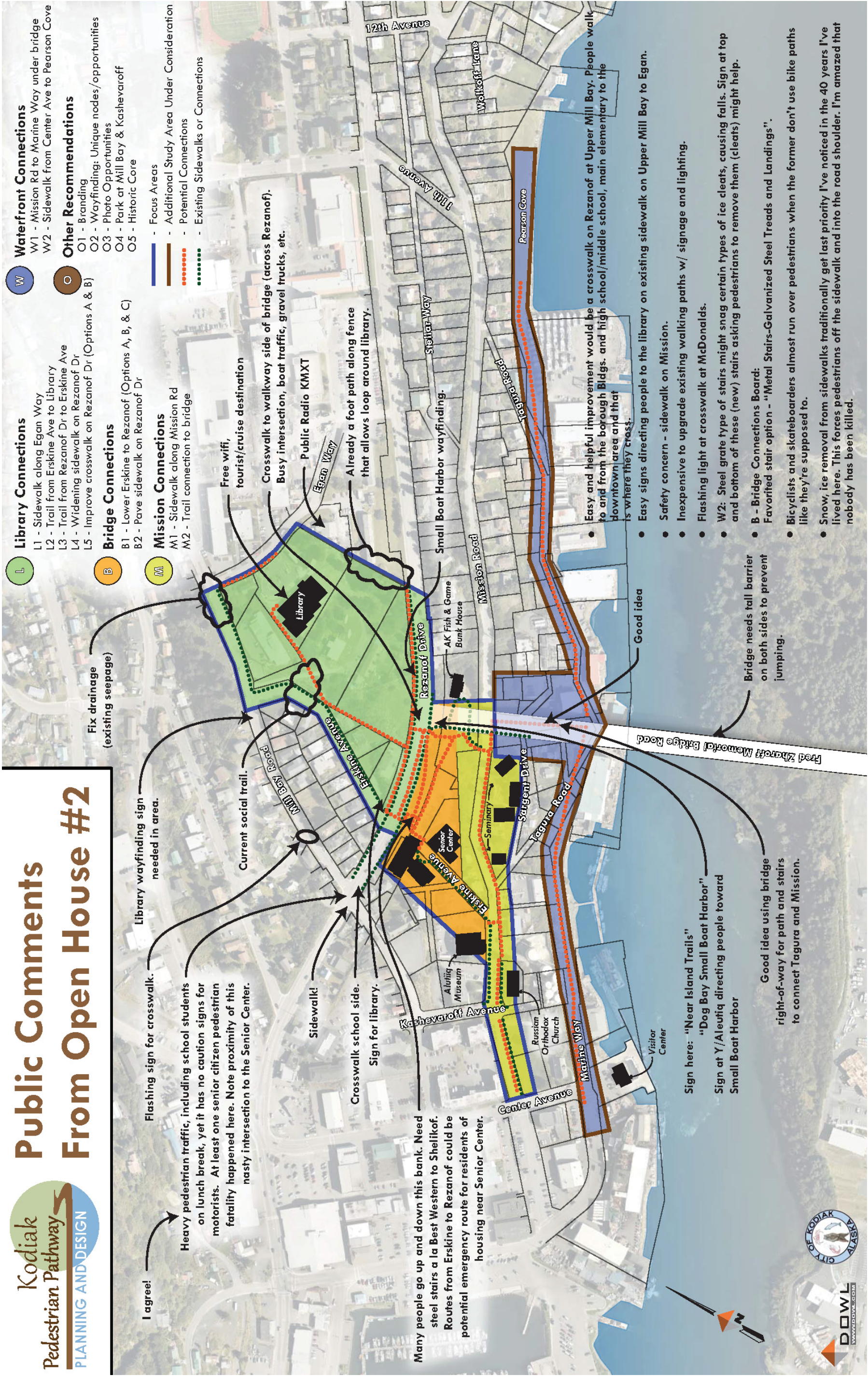


Figure 9: Consolidated graphic comments from the second open house.

SECTION III: RECOMMENDATIONS

Summary of Recommendations

The Overall Schematic Plan (Figure 9) shows the network of proposed pedestrian improvement projects in the Study Area. The Study Area is broken down into four smaller focus areas. Within each area is a list of potential projects. Potential projects include:

- L** Library Connections
 - L1 Sidewalk on Egan Way
 - L2 Trail from Erskine Avenue to the Kodiak Public Library
 - L3 Trail from Rezanof Drive to Erskine Avenue
 - L4 Widen Sidewalk on Rezanof Drive
 - L5 Crosswalk Improvements on Rezanof Drive
- B** Bridge Connections
 - B1 Connection from Lower Erskine Avenue to Rezanof Drive
 - B2 Pave sidewalk on Rezanof Drive
- M** Mission Road Connections
 - M1 Sidewalk Along Mission Road
 - M2 Trail from Mission Road to Bridge Connection
- O** Other Opportunities
 - O1 Wayfinding

The goal of the Plan is to illustrate projects that provide a network of pedestrian links that allow people to move safely and efficiently to points of interest within the Study Area. Each project is described in more detail in the following sections.



Figure 10: Isometric sketch of the Study Area and recommended projects.

Overall Schematic



Figure 11: Overall schematic plan.

Project Ranking Criteria

It is unlikely that funding will be available to complete all the proposed projects at one time. Therefore the projects have been ranked to provide guidance regarding projects with the highest use or are the quickest to implement. The ranking categories include: use intensity, cost of effort, level of effort, public input, and maintenance. The ranking criterion used has been adopted and modified from the 2011 Kodiak Road System Trails Master Plan. Each project is assigned a score within each category and scores are totaled to rank each project. Highest scores equaling top priority.

Below is an outline of the scores within each ranking criteria. The outline describes how each criteria was weighed, and a description of each criteria. Scores are based on observations and professional judgment, therefore the scoring was subjective.

RANKING CRITERIA:

Use Intensity

An estimate of how frequently the connections might be used by pedestrians based on observations, public feedback, and surrounding amenities. The goal is to better provide pedestrian connections for cruise ship passengers, therefore a project that directly connects to a tourist destination would have a higher rank than a project that does not. Projects were also ranked high if there were signs of high-use, such as social trails (informal dirt trails that were not built or legally designated, but became distinct overtime by repeated use).

Ranking Criteria	Rank	Description	Weighted Multiplier	Score
Use Intensity	2	High	3	6
	1	Moderate		3
	0	Little		0
Design & Construction Costs	2	Low (<\$200,000)	3	6
	1	Moderate (\$200,000- \$400,000)		3
	0	High(>\$400,000)		0
Level of Effort	2	Short time frame: COK right-of-way	2	4
	1	Moderate time frame: COK right-of-way/ property, DOT & PF right-of-way, or a short segment of private property		2
	0	Long time frame: private or federal property		0
Public Input	2	Highest scores at 2nd Open House voting (4+ votes)	1	2
	1	Second highest scores at 2nd Open House voting (2-3 votes)		1
	0	No votes at the 2nd Open House		0
Maintenance	2	Low maintenance	1	2
	1	Moderate maintenance		1
	0	High maintenance		0

Table 4: How ranking criteria is scored and weighted.

Project Description & Scoring Tables

This section includes enlarged figures showing the four focus areas and two tables for each of the ten recommended projects. The first table is a 'Project Description' table that outlines:

- project description,
- purpose and need (this describes how each project was derived, whether from previous plans, site observations, public feedback, or a combination),
- design considerations,
- design and construction costs, and
- other comments.

The second table is a 'Project Scoring Table'. This outlines the scores assigned to each project based on the criteria outlined in the 'Project Ranking Criteria' section. The rankings have been assigned across the full suite of 10 projects.

The projects are grouped as follows:

- L: Library Connections (5 projects);
- B: Bridge Connections (2 projects);
- M: Mission Road Connections (2 projects); and
- O: Other Opportunities (1 project).

The purpose of providing the proposed projects in this format is to have descriptions for each project that can be used alongside the enlarged figures as a tool:

- to seek funding,
- for assistance with permitting processes,
- to assist with further studies, and
- for property acquisition discussions.

Design and Construction Costs

Preliminary cost estimates for the design and construction were completed for each project based on information observed on-site and on a very preliminary design concept of what the project might entail. No surveys of existing conditions, utilities, or topography were conducted to develop these estimates. The projects were divided into three costs categories:

1. Low: less than \$200,000
2. Moderate: \$200,000 – \$400,000
3. High: more than \$400,000

Level of Effort

This is an estimate of the time required to complete a project based on factors such as property acquisition, design, and construction. For example, a pedestrian project in COK right-of-way of relatively short length would receive a high score since no property acquisition is required and the design and construction requirements would be minimal. A project in DOT&PF right-of-way would require a longer time-frame due to the permitting processes required. A project on private or federal property could potentially have the longest time frame due to the acquisition process.

Public Input

At the second Open House, attendees voted for the projects they thought were the highest priority. Projects with four or more votes received the highest score, project with 2-3 votes received the median score and projects with no votes received the lowest score.

Maintenance

The impact of a project on city maintenance is an important factor in understanding the long term financial impacts of a project. Impacts to the maintenance budget were estimated based on whether the project was an extension of existing maintenance practices or whether it would create new areas to maintain. A project that is an extension of existing sidewalks would be anticipated to be low maintenance; whereas a project outside of existing sidewalk and trail infrastructure, such as adding stairs, would be anticipated as a high impact on maintenance requirements.

Library Connections

Legend

- Parcels
- Contours (10 foot)
- Existing Sidewalks
- Potential Connections (within City of Kodiak ROW or Property)
- Potential Connections (not in City of Kodiak ROW)
- Potential Crosswalk (DOT&PF ROW analysis needed)

Potential Projects

- L1** **Sidewalk on Egan Way**
 - Paved sidewalk
- L2** **Trail from Erskine Ave. to Library**
 - Gravel trail
 - Pedestrian lighting potentially
- L3** **Trail from Rezanof Dr. to Erskine Ave.**
 - Gravel trail
 - Pedestrian lighting recommended
- L4** **Widen Sidewalk on Rezanof Dr.**
 - Paved sidewalk
- L5** **Crosswalk Improvements**

Feasibility and location will depend on analysis

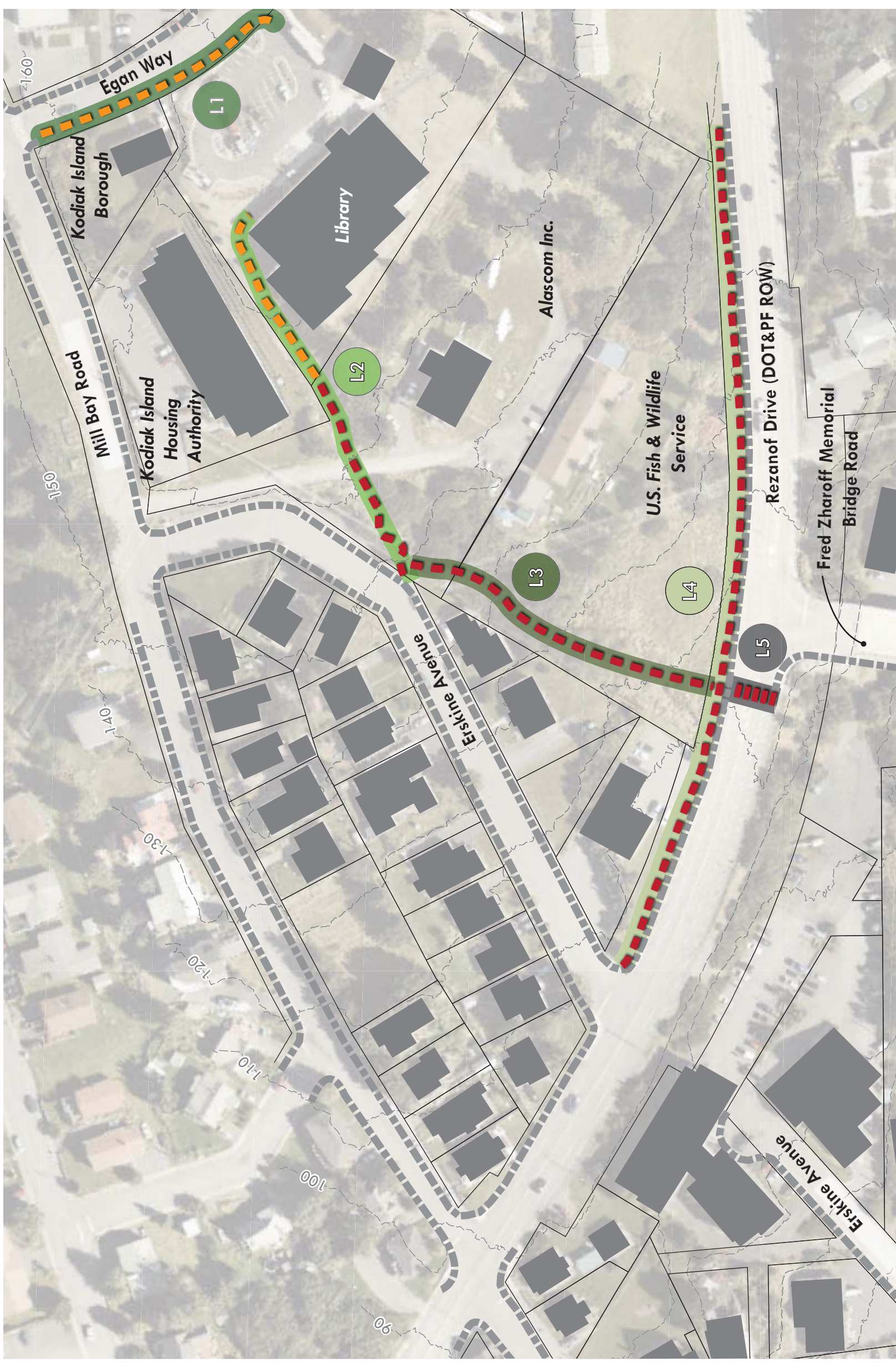


Figure 12: Library connections.

L1 Sidewalk on Egan Way

Project Description	
Project Description	A paved sidewalk along Egan Way in front of the Kodiak Public Library.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> This project was included because of on-site observations: no existing sidewalk from Mill Bay Road to the library property and social trails indicate people walk this route regularly. This received the second highest votes at the 2nd Open House
Design Considerations	<ul style="list-style-type: none"> Property: Within COK right-of-way Width: 6 feet Slope: Align running slope with Egan Way slope. Cross slopes not to exceed 2%. Surface: Concrete with 6-inch curb. Length: Approximately 250 linear feet. Retaining wall may be required along the Library property where there is a steep side slope. Include a short set of steps at the corner of the property where pedestrians currently cut through; ensure this can be tied into a pedestrian route on property. Include accessible curb-cut ramps.
Design & Construction Costs	<p>Construction Cost: \$200,000</p> <p>Design & Survey: \$50,000</p> <p>Total: \$250,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> No additional right-of-way required. Existing curb and gutter is in serviceable condition and will be left in place. Existing storm drain facilities adequate. No new major landscaping elements.
Other Comments	<ul style="list-style-type: none"> Consider other sidewalk improvements along Egan outside of this Study Area. Public comments indicated there are drainage issues along the sidewalk on Mill Bay Road near where this proposed sidewalk would tie-in (specifically “seepage”) - investigate any drainage issues during design.

Table 5: Project L1 Description



Image 19: Egan Way looking northwest from the library driveway. Proposed sidewalk would be on the left side of the road in this image



Image 20: Library entry sign. Proposed Sidewalk would wrap right in front of sign.

Project Score				
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	High use is expected based on the existing social trail, and that it provides a direct connection to the library.	2	3	6
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000- \$400,000.	1	3	3
Level of Effort	Short time frame; the project falls within COK right-of-way and the segment of sidewalk is only 250 feet in length, therefore a short design and construction time period is anticipated.	2	2	4
Public Input	This project received four votes at the 2nd Open House.	2	1	2
Maintenance	This sidewalk will be an extension of existing pedestrian facilities; therefore low additional maintenance is expected.	2	1	2
Total Score				17
Project Rank				3

Table 6: Project L1 Score



Image 21: Egan Way looking southeast from the Mill Bay Road. Proposed sidewalk would be on the right side of the road in this image

Project Description	
Project Description	Trail from the bend in Erskine Avenue to the Kodiak Public Library.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Improving this area would provide an alternative route to the library creating more direct access. Feedback at 1st Open House: <ul style="list-style-type: none"> Route G' (representing an alternative trail to the library) received the second highest votes. The public depicted this layout on several of the diagrams. On-site Observations: <ul style="list-style-type: none"> This route has a social trail indicating that it is well traveled. This route provides scenic views of the downtown waterfront and mountains beyond.
Design Considerations	<ul style="list-style-type: none"> Property: Beginning at Erskine Avenue the trail would start in COK right-of-way, cross private property (Alascom Inc.), then be on COK property (library). Access length across the Alascom Inc. property is for approximately 250 linear feet. Width: 6 feet. Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations. Surface: Compactable gravel (meeting accessible trail standards), or asphalt. Length: Approximately 425 linear feet. Consider lighting for improved security (may not be feasible on private property but possibly on library property). Signage directing people to the trail.
Design & Construction Costs	Construction Cost: \$135,000 Design & Survey: \$40,000 Total: \$175,000 Assumptions: <ul style="list-style-type: none"> Assumed 6-foot gravel surface. Three luminaires on public property and within COK right-of-way. Stop signs at driveway crossing. Private property access costs not included.
Other Comments	The trail crosses private property (Alascom Inc.) therefore an access agreement will need to be acquired.

Table 7: Project L2 Description



Image 22: West side of library looking south. The existing social trail can be seen in the foreground. The proposed trail would follow the social trail south towards Erskine Avenue.

Project Score				
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	This would provide an alternative route to the library; since routes already exist moderate use is expected.	1	3	3
Design & Construction Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6
Level of Effort	Moderate time frame; this time frame is an estimate based on the need to coordinate private property access. Since it is a short segment of trail a short design and construction time line is anticipated.	1	2	2
Public Input	This project received two votes at the 2nd Open House and was discussed with several attendees at the 1st Open House.	1	1	1
Maintenance	This is outside of existing right-of-way and would have moderate slopes; therefore higher additional maintenance is expected.	0	1	0
Total Score				12
Project Rank				6

Table 8: Project L2 Score

Project Description	
Project Description	A trail from Rezanof Drive to the bend in Erskine Avenue via federal property.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> This connection would create a direct path between the bridge and potential trail to the library (see project: Trail from Erskine Avenue to the library). Alternative paths outside of the road network provide tranquil walking options away from vehicle traffic and can provide scenic views.
Design Considerations	<ul style="list-style-type: none"> Property: Starts at DOT&PF right-of-way, crosses federal property (U.S. Fish & Wildlife Service) and private property (Alascom Inc.). Width: 6 feet. Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations. Surface: Compactable gravel (meeting accessible trail standards), or asphalt. Length: Approximately 425 linear feet (length will depend on design slope, this distance is estimated based on including switchbacks to achieve an average slopes less than 8%). Consider lighting for improved security. Existing vegetation includes dense alders and large evergreen trees. Preserve existing evergreens and avoid having to clear alders to reduce future maintenance.
Design & Construction Costs	<p>Construction Cost: \$125,000</p> <p>Design & Survey: \$40,000</p> <p>Total: \$165,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Assumed 6 foot gravel surface. Bollards at either end of the trail, two solid wood on either side, removable wood in the middle. Two luminaires (one of the luminaires on the L2 project would illuminate the junction between L2 and L3). Right-of-way costs not included.
Other Comments	<ul style="list-style-type: none"> Due to the process of creating easements or access agreements on federal land the project would be a long term endeavor. If the land is ever developed for public purposes, the owners should consider including this public access connection to improve routes to and from the library. Consider offsetting the trail to keep the vegetative buffer between the residences, to allow greater space for the trail to meander, and create a more direct connection to the bridge.

Table 9: Project L3 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This would provide an alternative route to the library; since routes already exist, moderate use is expected.	1	3	3	
Design & Construction Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6	
Level of Effort	Long time frame; due to the process of creating easements or acquiring federal land the project would be a long term endeavor.	0	2	0	
Public Input	This project received no votes at the second open house.	0	1	0	
Maintenance	This is outside of existing right-of-way and would have moderate slopes; therefore higher additional maintenance is expected.	0	1	0	
Total Score				9	
Project Rank				7	

Table 10: Project L3 Score



Image 23: Intersection of Rezanof Drive and the bridge to Near Island. The proposed trail from Rezanof (L3) would start on the left side of the image on the west side of Rezanof Drive.

Project Description	
Project Description	Widen the existing sidewalk on Rezanof Drive to an 8-foot wide paved sidewalk.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Mentioned in the 2002 Community Design Workshop Final Report. Widening this sidewalk would provide for a more comfortable walking experience and allow larger groups of pedestrians, such as tourists, to walk together.
Design Considerations	<ul style="list-style-type: none"> Property: DOT&PF right-of-way (may require private property depending on how the right-of-way and sidewalk align). Width: 8 feet (6 feet minimum). Slope: Align running slope with Rezanof Drive slope. Cross slopes not to exceed 2%. Surface: Concrete with 6-inch curb. Length: Approximately 820 linear feet.
Design & Construction Costs	<p>Construction Cost: \$320,000</p> <p>Design and Survey: \$65,000</p> <p>Total: \$385,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Does not include right-of-way acquisition costs. Remove and replace existing sidewalk. Potentially replace 25% of existing curb and gutter. Sidewalk retaining wall required for approximately half of full length (due adjacent steep slopes and since offset from right-of-way is currently unknown).
Other Comments	<ul style="list-style-type: none"> May require grading outside of right-of-way or retaining walls along some sections. Public comments indicated there are drainage issues along the hillside adjacent to this sidewalk (specifically “seepage”) - investigate any drainage issues during design. This recommendation is for widening the portion of sidewalk on Rezanof Drive that falls within the Study Area of this plan. Consider widening the sidewalk on Rezanof Drive along other sections of road outside of this Study Area. For example the 2002 Community Design Workshop Final Report recommends widening the Rezanof sidewalk from Lower Mill Bay Road past the Erskine Avenue intersection. Also consider sidewalk widening on the south-side of Rezanof Drive outside of this Study Area.

Table 11: Project L4 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This is an area that tourists and locals currently walk and provides a connection from neighborhood areas to the bridge and library, therefore moderate pedestrian traffic is expected.	1	3	3	
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000-\$400,000.	1	3	3	
Level of Effort	Long time frame; due to the DOT&PF permitting and the potential coordination with private property.	0	2	0	
Public Input	This project received no votes at the 2nd Open House.	0	1	0	
Maintenance	This would be an improvement to an existing sidewalk therefore there would be little impact to current maintenance practices.	2	1	2	
Total Score				8	
Project Rank				10	

Table 12: Project L4 Score



Image 24: West side of Rezanof Drive looking north.



Image 25: West side of Rezanof Drive looking south.

L5 Crosswalk on Rezanof Drive



Image 26: Pedestrians crossing Rezanof Drive to access the bridge to Near Island. This is where one of the proposed crosswalks would be located.

Project Description	
Project Description	Install striped and signed crosswalk on Rezanof Drive near the bridge to Near Island.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Crossing Rezanof Drive can feel unsafe due to the width of the road, the speed of the cars, and sight distance issues along some sections; this sentiment was reiterated by the public at both open houses. Voted the top priority project at the second open house.
Design Considerations	<ul style="list-style-type: none"> Property: DOT&PF right-of-way. Width: 10 feet (as recommended by DOT&PF). Surface: White paint on existing asphalt. Length: Approximately 45 linear feet. Signage as warranted. An analysis is needed to determine whether a cross-walk at this location meets the standard criteria.
Design & Construction Costs	<p>Construction Cost: \$60,000</p> <p>Engineering: \$40,000</p> <p>Total: \$100,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Assumes warrant analysis of \$30,000 as a part of engineering costs. No flashing beacon assumed in construction costs. No right-of-way or utility issues.
Other Comments	Pedestrians also cross Rezanof Drive at Erskine Avenue and Mill Bay Road. The crossing at Mill Bay Road currently has a pedestrian crossing sign for east bound traffic as seen in the image on the right. These crossings did not appear to have appropriate sight distances for pedestrians to cross safely. Any crosswalk study should analyze the conditions at these crossings as well. The proper location and signage of a crosswalk will need to be determined through full analysis.

Table 13: Project L5 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	High use is expected based on observations of people crossing and comments received at both open houses.	2	3	6	
Design & Construction Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6	
Level of Effort	Short time frame; the warrant analysis is expected to take approximately 3 months and a short design and construction time frame is expected due to the small amount of improvements.	2	2	4	
Public Input	This project received 7 votes at the 2nd Open House.	2	1	2	
Maintenance	Little additional maintenance is expected, this may include restriping.	2	1	2	
				Total Score	20
				Project Rank	1

Table 14: Project L5 Score



Image 27: As noted in the "Other Comments" row there is a pedestrian sign for crossing Rezanof Drive at the intersection of Mill Bay Road. But as seen in this image the crest in the hill above this crossing makes it hard see oncoming traffic.

Potential Projects

Connection from Lower Erskine Ave. to Rezanof Dr.

Two Potential Options:

Option will depend on cost of effort, public feedback, and coordination with DOT&PF

Option A B1-A Sloped Walkway

- Paved trail
- Sloped trail with rest spot mid-way
- Fill and retaining wall as needed
- Design coordination with DOT&PF along existing road bed

Option B B1-B Sidewalk & Stair Near Bridge

- Approximately 20-foot tall stair with multiple landings
- Enters U.S. Fish & Wildlife Property / DOT&PF ROW

B2

Pave Sidewalk on Rezanof

- Paved sidewalk along existing shoulder

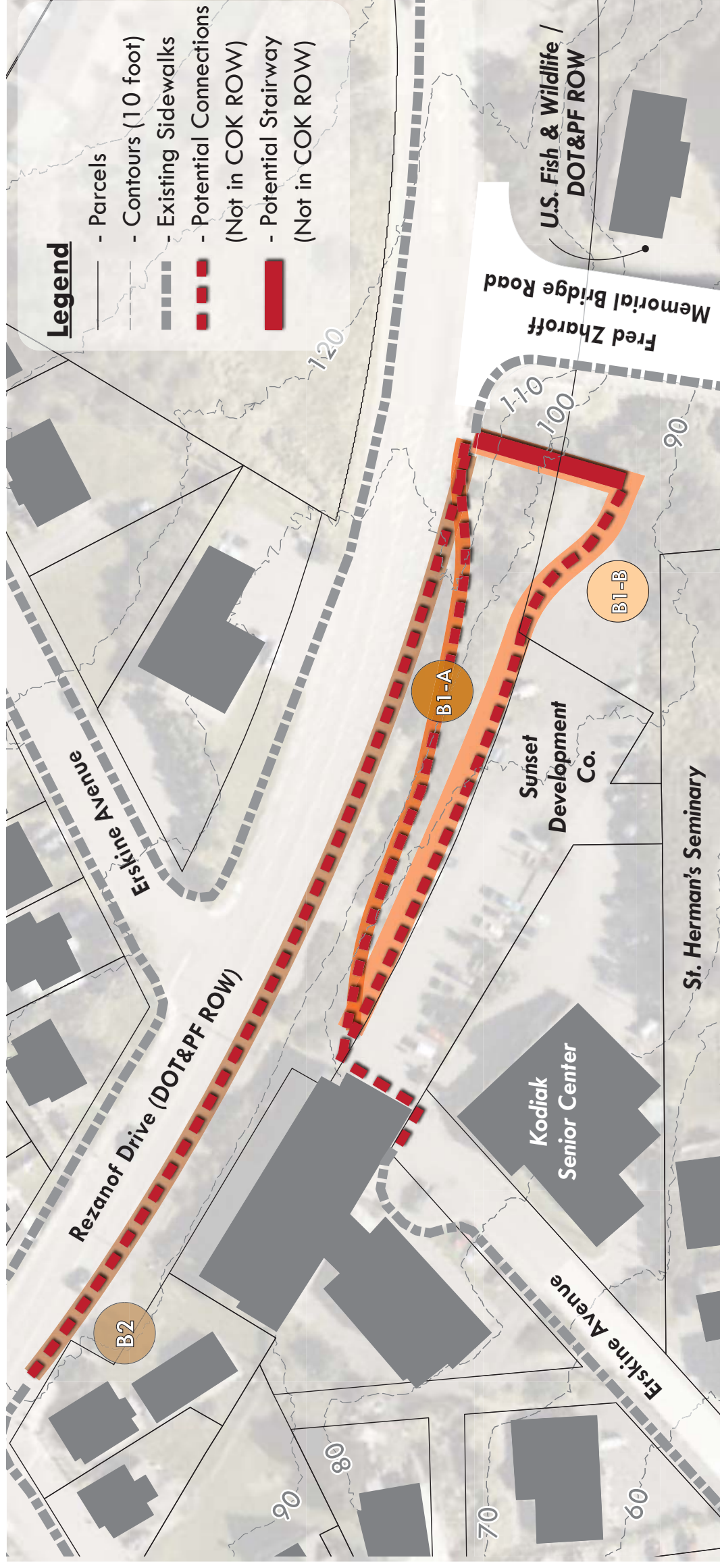


Figure 13: Bridge connections.

Project Description	
Project Description	A pathway (B1-A) or stairway (B1-B) connecting the dead end of lower Erskine Avenue to the bridge to Near Island.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Identified as a project in the 2002 Community Design Workshop Final Report (but as a stairway on the northeast side of the bridge). Several social trails exist in this area. Would provide an alternative trail to the bridge, which passes by a tourist destination (Alutiiq Museum). Is a vantage point providing views of the waterfront and downtown.
	<p>General</p> <ul style="list-style-type: none"> Incorporate wayfinding and interpretive signage at vantage points. Either option will need to be permitted by DOT&PF. Protect large trees to extent feasible. Provide adequate lighting. <p>B1-A Sloped Pathway</p> <ul style="list-style-type: none"> Property: Private property (Sunset Development Company), and DOT&PF right-of-way. Width: 6 feet. Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations. Surface: Paved with asphalt. Length: 460 linear feet consisting of 72 feet crossing private property and 388 feet of sloped trail (based on an average 6% slope) in DOT&PF right-of-way. <p>B1-B Stairway</p> <ul style="list-style-type: none"> Property: Private property (Sunset Development Company), DOT&PF right-of-way, and U.S. Fish & Wildlife Service property on which DOT&PF is a permitted use for operation and maintenance of the bridge to Near Island. Width: Trail segment: 6 feet; Stairs segment: 8 feet minimum Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations. Follow standard riser-to-tread ratios for outdoor stairways. Minimum riser height: 4.5 inches, maximum riser height: 7 inches. Minimum tread length 11 inches. Provide landings every 9-11 risers. Surface: Potential options include: open grate, concrete, or wood. Open grate or concrete will be longer lasting. Length: 460 linear feet of flat trail; 20-foot tall stairway with 6-inch risers and landings every 10 steps. If using open grate steps, provide a sign warning people walking with strap-on cleats to remove them. Consider a covered stairway to reduce long term maintenance. Provide a channel for bicyclists to walk their bike up and down the steps. Stairway could be shortened by elevating the trail to the base of the stairway.
Design Considerations	

Project Description

Design & Construction Costs	<p>B1-A Sloped Sidewalk</p> <p>Construction Cost: \$460,000</p> <p>Design & Survey: \$80,000</p> <p>Total: \$540,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Assumed existing ground at a 2 to 1 slope. Assumed use of cast in place sidewalk retaining wall (modular block wall was investigated, but appeared more costly). This will be difficult to construct, as 2 to 1 embankment will require extensive excavation to build retaining wall and pathway structural fill. Four feet of concrete pathway and four feet of asphalt pathway was assumed. This will be difficult to design and construct due to the existing steep embankment; stairs may be the best solution (and likely least cost). Does not include right-of-way or easement acquisition. <p>B1-B Stairway</p> <p>Construction Cost: \$440,000</p> <p>Design & Survey: \$80,000</p> <p>Total: \$520,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Utilized a previous stairs design and estimate from 2004 and applied an inflation cost. The sidewalk along the existing parking area would be installed with concrete curb and gutter. This may be the more appropriate solution from an engineering standpoint due to challenges around constructing a sloped sidewalk on a steep embankment.
Other Comments	<ul style="list-style-type: none"> Two design options for this connection were included to show potential alternatives. Both have advantages and disadvantages; the sloped sidewalk is ideal for cyclists but may be more difficult to design and construct. The stairs are more direct but exclude wheelchairs and cyclists. Any scenario should be investigated for engineering feasibility and permitting potential with DOT&PF. For both scenarios the first section of sidewalk crosses private property (Sunset Development Co.) for approximately 52 feet of the property (for 72 linear feet of sidewalk); therefore any development will need to be coordinated with the property owner. The stair option will require crossing the U.S. Fish & Wildlife Service property on which DOT&PF is a permitted use and operation for the bridge to Near Island. DOT&PF should be coordinated with to confirm the required permitting, and if additional entities need to be notified.

Table 15: Project B1 Description



Image 28: West side of the bridge abutment.



Image 29: Approach to the west side of the bridge.

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This area has several social trails and provides views of downtown Kodiak and the waterfront, therefore high use is expected.	2	3	6	
Design & Construction Costs	This project estimate falls within the high range of costs: > \$400,000.	0	3	0	
Level of Effort	Long time frame; this will require DOT&PF permitting and the coordination with a private property owner.	0	2	0	
Public Input	This project received four votes at the 2nd Open House.	2	1	2	
Maintenance	This would require additional maintenance outside of the current infrastructure; therefore higher additional maintenance is expected.	0	1	0	
Total Score				8	
Project Rank				8	

Table 16: Project B1 Score



Image 30: Existing social trail to the bridge.

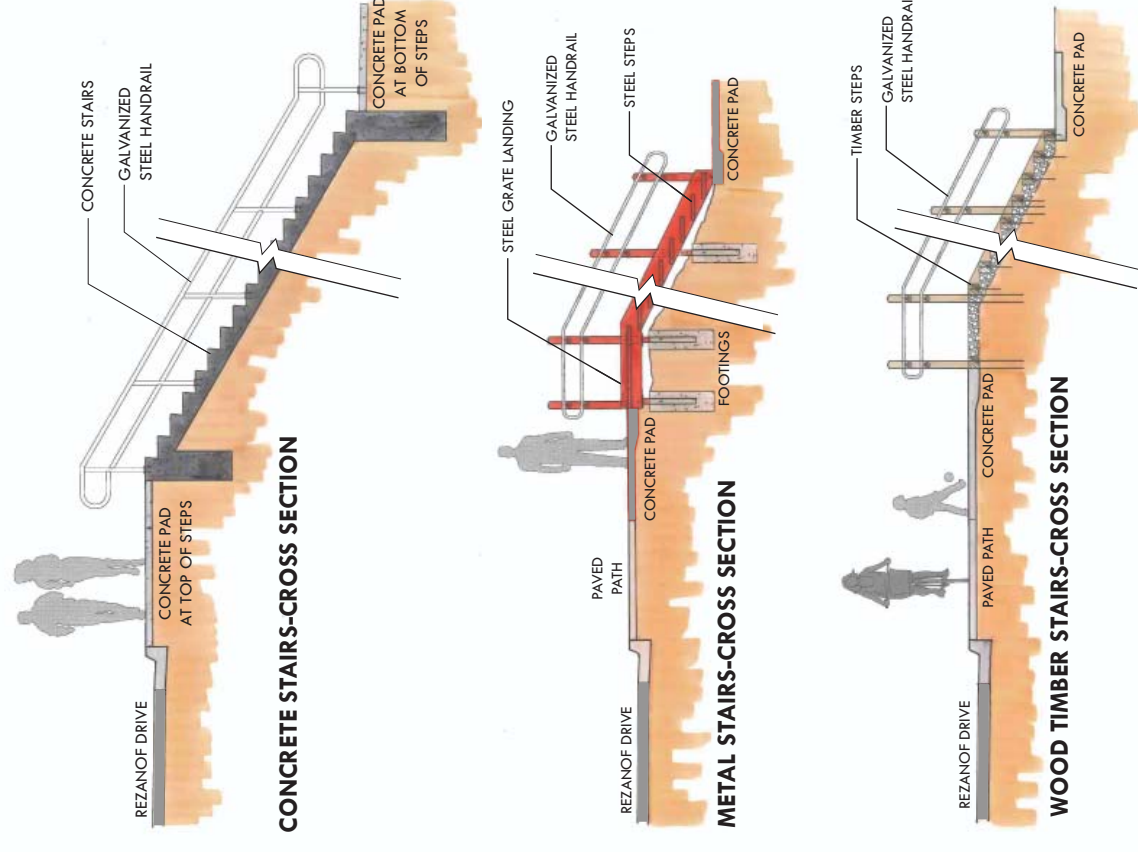


Figure 14: Sections of potential stair types including concrete, metal, and wood. Given the steep existing slope the concrete and wood stairs would require additional fill. The B1-B project design and construction cost estimate is based on an estimate completed in 2004 for a metal stair design.

Project Description	
Project Description	Pave the existing gravel shoulder on Rezanof Drive.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Increases accessibility; a paved surface is more amenable to accessible means of travel than the current gravel surface.
Design Considerations	<ul style="list-style-type: none"> Property: DOT&PF right-of-way. Width: 6 feet minimum; feasible width will need to be determined based further investigations and coordination with DOT&PF. Slope: Align running slope with Rezanof Drive. Cross slope maximum 2% Surface: Paved with either concrete or asphalt. Length: Approximately 625 linear feet.
Design & Construction Costs	Construction Cost: \$160,000 Design & Survey: \$50,000 Total: \$210,000 Assumptions: <ul style="list-style-type: none"> Assumed 6-foot asphalt pavement and 2-foot gravel due to proximity of steep slope. May require handrail in certain locations due to proximity of steep slope (not included in estimate).

35

Table 17: Project B2 Description

Project Score				
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	This is the current link between the downtown area and the access to the bridge, therefore high use is expected.	2	3	6
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000-\$400,000.	1	3	3
Level of Effort	Short time frame; this project would require DOT&PF permitting which would take time, but a short time frame for design and construction is expected since there is an existing gravel surface.	2	2	4
Public Input	This project received two votes at the 2nd Open House.	1	1	1
Maintenance	This would be an improvement within an existing roadway, therefore there would be little impact to current maintenance practices.	2	1	2
Total Score				16
Project Rank				4

Table 18: Project B2 Score

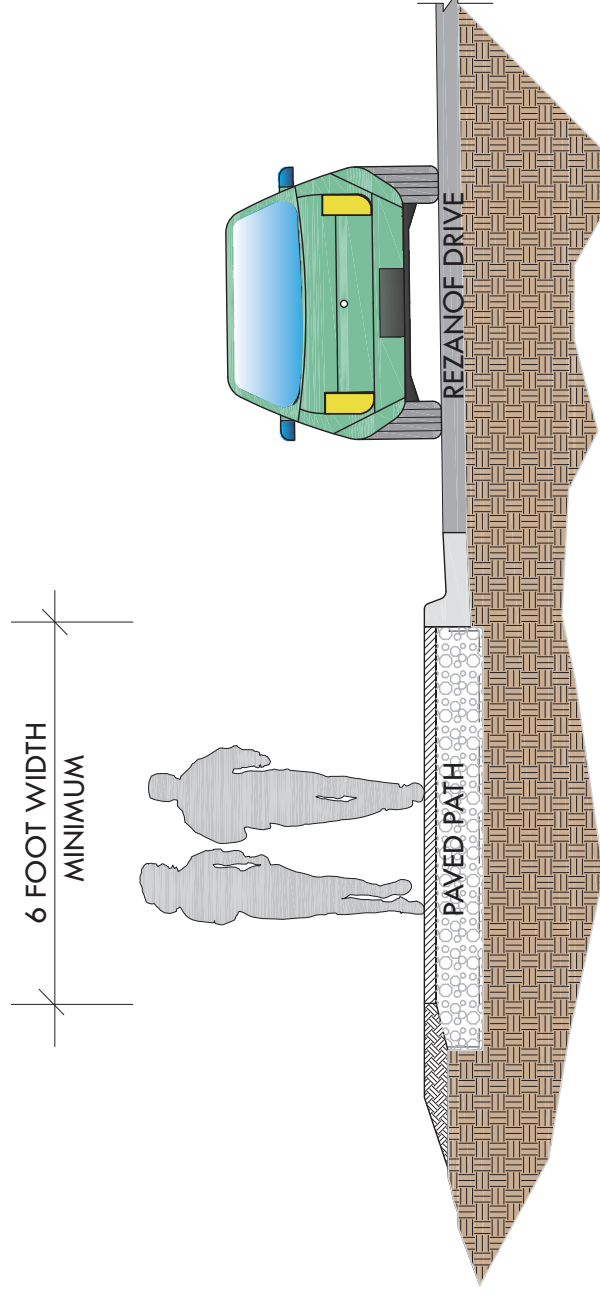


Figure 15: Rezanof Drive sidewalk section.



Image 31: Existing gravel edge along Rezanof Drive. View from the bridge looking west on Rezanof Drive.

Mission Road Connections

Legend

- - - Parcels
- - - Contours (10 foot)
- - - Existing Sidewalks
- - - Potential Connections (within City of Kodiak ROW or property)
- - - Potential Connections (not in City of Kodiak ROW)
- - - Potential Crosswalk (Depends on recommendations of an analysis)

Potential Projects

- M1** **Sidewalk on Mission Road**
 - Widen existing sidewalk and add new paved sidewalk along Mission
- M2** **Connection from Mission to Bridge Connection**
 - Gravel trail and/or stairs and retaining walls as needed
 - Pedestrian lighting recommended



Figure 16: Mission road connections.



Image 32: Cruise ship passengers walking down Mission Road near the bridge to Near Island.



Image 33: Existing conditions on Mission Road.

Project Description

Paved sidewalk along the south-side of Mission Road. Widen the existing sidewalk along Sargent Park, and construct new sidewalk from the Russian Orthodox Church to just under the bridge to Near Island.

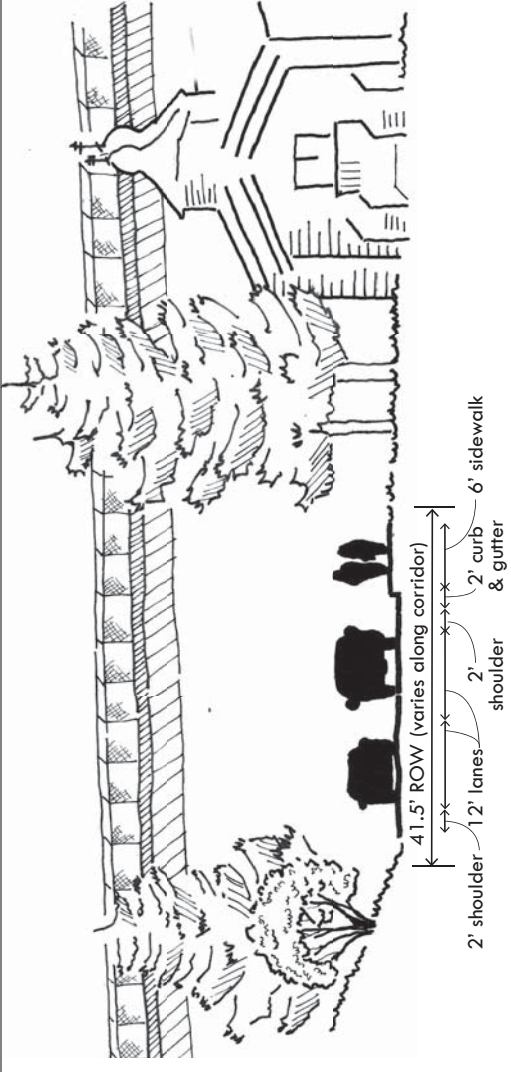
- Identified as an area for pedestrian improvements in previous plans: 2002 Community Design Workshop, 2011 Kodiak Road System Trails Master Plan.
- Social trails along roadway imply frequent pedestrian use.
- Observation of tourists and locals walking the area in the street corridor.
- Property: COK right-of-way, may require some right-of-way acquisition.
- Width: 6 feet minimum.
- Slope: Align running slope with Mission Road, cross slope maximum 2%.
- Surface: Paved with concrete and 6" curb.
- Length: 288 linear feet of widened sidewalk (4.5 feet to 8 feet); 800 linear feet of new sidewalk (6 feet wide).
- Potential short retaining wall (approximately 2-3 feet tall for up to 125 linear feet).
- Will need to accommodate driveways and potentially on-street parking across from Erskine Avenue.

Construction Cost: \$620,000
 Design and Survey: \$100,000
 Total: \$720,000

- Design & Construction Costs**
- Assumptions:**
- Does not include right-of-way acquisition costs.
 - Existing storm drain system to tie into with new curb and gutter.
 - May require some right-of-way acquisition and parking re-configuration at the properties west of Tagura Road.
 - Re-use existing curb and gutter along Mission Road.

Due to the scope of this study this recommendation shows the sidewalk terminating at the bridge; consider extending pedestrian improvements further along Mission Road. The 2011 Kodiak Road System Trails Master Plan includes creating a trail on Mission Road all the way out to Mission Beach.

Table 19: Project M1 Description



Mission Road Section
 In front of St. Herman's Seminary facing the bridge to Near Island

Project Score

Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	High use is expected based on observed pedestrian traffic, and that this sidewalk will pass the St. Herman Catholic Seminary, a destination for locals and tourists.	2	3	6
Design & Construction Costs	This project estimate falls within the high range of costs: >\$400,000.	0	3	0
Level of Effort	Short time frame; the majority of the project is in COK right-of-way and would be along the edge of an existing roadway, therefore relatively short design and construction would be anticipated.	2	2	4
Public Input	This project received two votes at the 2nd Open House and was discussed with several attendees at the 1st Open House.	1	1	1
Maintenance	This sidewalk will be an extension of existing pedestrian facilities but is for a relatively long distance compared to other projects in this plan, therefore moderate additional maintenance is expected.	1	1	1
Total Score				12
Project Rank				5

Table 20: Project M1 Score

Project Description	
Project Description	Pathway connecting Mission Road to the improvements between lower Erskine Avenue and the bridge (see project: Lower Erskine Avenue to Bridge).
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Existing social trails. Provides a route for tourists to walk down Mission Road then directly up to the bridge to connect to Near Island or to the library.
Design Considerations	<ul style="list-style-type: none"> Property: starts on COK right-of-way then continues in DOT&PF right-of-way, and U.S. Fish & Wildlife Service property on which DOT&PF is a permitted use for operation and maintenance of the bridge to Near Island. Width: 6 feet. Slope: Follow accessible trail design recommendations. Surface: Compactable gravel (meeting accessible trail standards), or asphalt. Length: 230 linear feet (based on an average slope of 7%-8%) – this would be the length required to meet the base of a stairway connecting to the bridge. Additional trail and/or stairs would be needed if a sloped sidewalk were developed from Erskine Avenue. May require short segments of retaining wall, potentially 100 linear feet long total to reduce the amount of cut. Recommended material: dry stack boulder wall. (i.e. rockery wall). Consider lighting for improved security. Provide crosswalk from sidewalk improvements on Mission Road if warranted.
Design & Construction Costs	<p>Construction Cost: \$260,000</p> <p>Design & Survey: \$50,000</p> <p>Total: \$310,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Assumed use of class II Rip Rap for slope protection at 1.5 to 1 slope. Stairs, gabion baskets or concrete retaining wall may be a more suitable option. This will require fairly extensive excavation and re-grading of area. This will be difficult to construct if some right-of-way is not acquired from St. Herman’s Seminary, unless stairs are used. Asphalt pavement used in cost estimate for trail surface. Crosswalk on Mission Road is not included in estimate, as an analysis is needed. Cost to obtain easement or right-of-way not included.
Other Comments	<ul style="list-style-type: none"> The portion of improvements that cross U.S. Fish & Wildlife Service property, on which DOT&PF is a permitted use for operation and maintenance of the bridge, will need to be permitted by DOT&PF. This property is currently used as a State of Alaska Fish & Game Bunk House. It will be important to maintain separation between the existing driveway to the bunk house and the trail alignment.

Table 21: Project M2 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	Based on social trails this area is used by pedestrians; moderate use is expected since there are other means of access to the bridge.	1	3	3	
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000 - \$400,000.	1	3	3	
Level of Effort	Moderate time frame; this project would require DOT&PF permitting but since the length of trail is short, a shorter design time frame is anticipated.	1	2	2	
Public Input	This project received no votes at the 2nd Open House.	0	1	0	
Maintenance	This trail would require maintenance outside of existing right-of-way.	0	1	0	
Total Score				8	
Project Rank				9	

Table 22: Project M2 Score



Image 34: Existing driveway off of Mission Road. The driveway runs under the bridge to the ADF&G bunk house. The proposed trail would start here and switch back to the left and connect to a connection to the bridge.

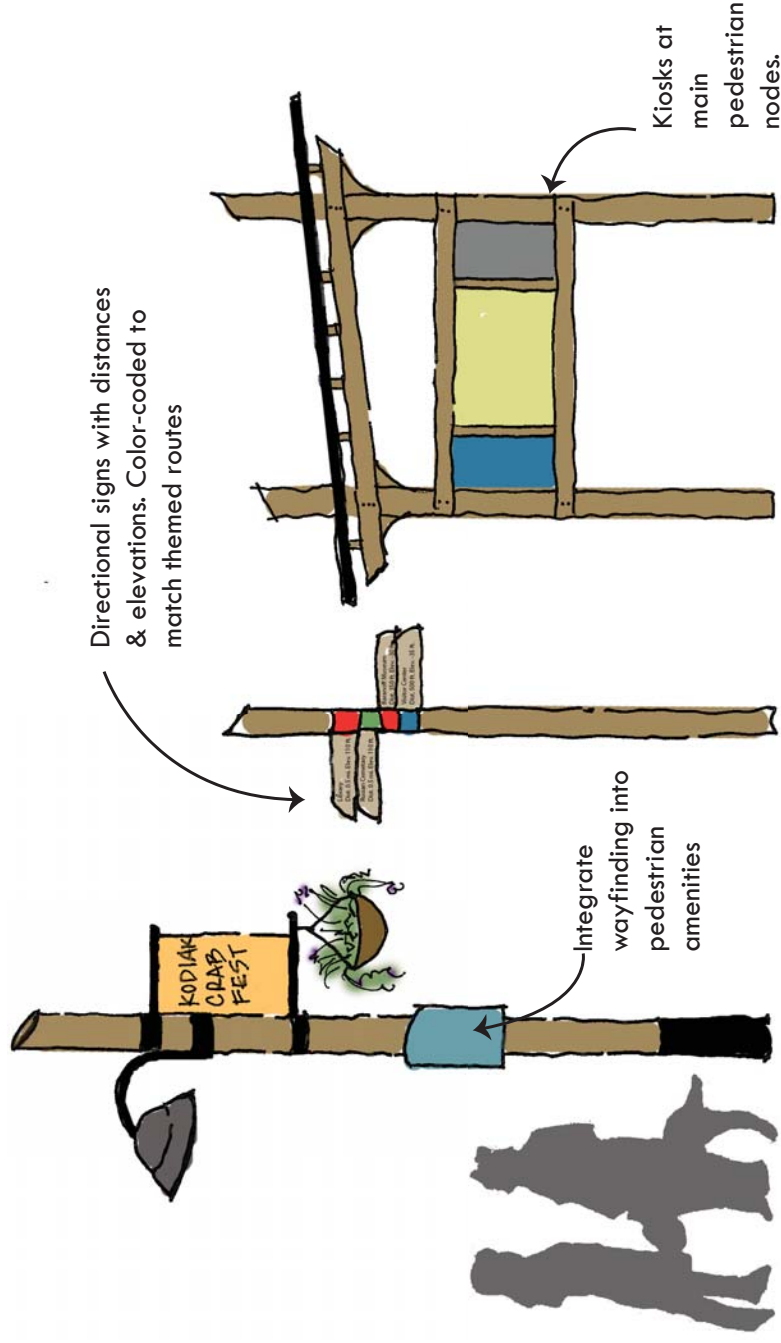


Image Source: left: <http://nutshell.com.au/projects/shire-of-central-goldfields-interpretive-trail/>;
middle: <https://www.smashingmagazine.com/street-and-wayfinding-signs-part-4/> ; right:
susanjweit.com (Monarch Spur Trailhead)

Above Figure 17: Other opportunities - wayfinding.
Above Right Table 23: Project 01 Description
Right Table 24: Project 01 Score

Project Description				
Project Description	Develop a wayfinding plan for the City of Kodiak, specifically for the core areas that tourists walk. A wayfinding system consists of directional signs that help locals and visitors orient themselves. They should be designed to reflect the community's aesthetic and character.			
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> • Navigating the pedestrian routes from downtown to the bridge and library is not intuitive and the use of maps is required, wayfinding would help navigation significantly. • Several comments from the public stressed the need for improved wayfinding signage. • This project received four votes at the 2nd Open House. 			
Design Considerations	<ul style="list-style-type: none"> • Signs that give distances and elevations to destinations. • Are color coded based on themed routes (themed routes adopted from Discover Kodiak Map and updated to reflect projects as they get implemented). • Are subtle in design and use materials and styles reflective of the historic and contemporary character of the COK and the surrounding natural beauty. • Include nodes with larger contextual maps and interpretive signs, and directional arrow signs in-between destinations. • Signs should direct people to main attractions including the library and attractions on Near Island (including the small boat harbor). 			
Plan Costs	Wayfinding Plan: +/- \$100,000 Assumptions: <ul style="list-style-type: none"> • The wayfinding plan would be for the area from Pier 2, through downtown and up to the library. • This estimate is just for the planning portion of developing wayfinding, the costs of detailed design and construction would depend on the outcomes of the plan. 			
Other Comments	Consider doing a city branding plan prior to a wayfinding effort. A branding plan is a way for Kodiak to help identify and brand itself, it can be used for marketing, developing design standards, and developing wayfinding that matches and complements branding themes.			
Project Score				
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	Wayfinding is an amenity that can be used by visitors to navigate to areas of interest. This could be highly used by all types of visitors.	2	3	6
Plan Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6
Level of Effort	Short time frame; developing the wayfinding plan could be done relatively quickly. The actual implementation would be a longer time frame.	2	2	4
Public Input	This project was within the highest group of votes at the 2nd Open House (4 total).	2	1	2
Maintenance	This would add a whole new set of signage that would need to be maintained from vandalism and general weathering; therefore higher additional maintenance is expected.	0	1	0
Total Score		18		
Project Rank		2		

General Considerations

Beyond the ten specific projects outlined in the previous section this section provides additional recommendations that should be considered. These are either:

- outside of the Study Area of this plan, but were included based on public feedback (waterfront connections, and pedestrian nodes);
- programming considerations, such as continued coordination with Discover Kodiak and cruise ships; or
- general considerations that apply to all or most of the ten specified projects, such as pedestrian design recommendations.

WATERFRONT CONNECTIONS

During the public involvement process feedback regarding potential pedestrian connections outside of the Study Area was provided. People were interested in pedestrian routes from:

- the bridge to Marine Way (under the bridge), and
- Center Avenue to Pearson Cove, a small park along the waterfront.

Both of these projects would provide great walking opportunities for visitors.

The connection under the bridge provides a direct pedestrian route from the bridge all the way to waterfront. This connection includes various scenic vantage points and would direct visitors to businesses along the waterfront.

The pedestrian connection along Marine Way would provide an opportunity for visitors to see the working waterfront with an easy, flat walk to Pearson Cove. The cove is a small park offering views of Near Island, boat activity, and marine life. Both improvements fall outside of this plan's Study Area, but should be considered for further study.

Potential Projects

-  - Concept Potential Connections
-  - Parcels

-  **W1**
Sidewalk on Marine Way From Center Avenue to Pearson Cove
 - 6 to 8-foot wide paved sidewalk

-  **W2**
Connection from Mission to Marine Way (Under Bridge)
 - Combination of stairs and trails to provide a pedestrian connection from Mission Road to Marine Way



Image 35: Pearson Cove Park. View of waterfront looking towards Near Island.



Image 36: Marine Way, view looking northeast towards Pearson Cove.

Figure 18: Potential waterfront connections.

CONTINUED COORDINATION BETWEEN DISCOVER KODIAK & CRUISE LINES

Discover Kodiak is a nonprofit 501(c) 6 organization that works to promote tourism within the KIB and works directly with cruise-ship operators to provide visitor information to passengers. Discover Kodiak publishes a map of walking routes through downtown Kodiak. As connections and wayfinding infrastructure are developed, the maps should be updated to reflect changes. Figure 16 is the existing Discover Kodiak map which outlines themed routes, and Figure 17 shows how this map could be updated to reflect new routes that this plan outlines. The updates should also include information on the distance and elevation change of the various walks so that visitors can gauge their time and level of effort of the walk.

Local business owners expressed concern that cruise ship passengers are bussed directly from Pier 2 to destinations, thus missing opportunities to walk through the town and visit local businesses. Although Discover Kodiak passes out walking information at a booth at Pier 2 during docking, by the time the passengers debark many have already determined their plans on shore. To help address this, Discover Kodiak should continue to work with the cruise ship companies to ensure passengers are aware of the walking opportunities that connect to shopping, scenic, and cultural destinations prior to debarking. As shown in Table 23, the 2016 cruise ship schedule, some boats are only docked for seven hours; this may not be sufficient time for activities outside of the city but could be enough to walk from Pier 2 to destinations in and around downtown.

PEDESTRIAN FACILITIES DESIGN RECOMMENDATIONS

Sidewalk and Trail Design Standards

Sidewalks and trails should be designed in a sustainable way that will minimize long term maintenance and degradation. Wherever possible, pedestrian pathways should be designed to accommodate multiple types of users (i.e., for walking, jogging, cycling); this may not be feasible where steep grades exist. In such cases Americans with Disabilities Act (ADA) accessible pathway



Figure 19: Discover Kodiak visitor map.

Port of Kodiak, Cruise Ship Schedule 2016

Vessel Name	PAX	Cruise Line	Length	Arrival Date	Day	Arrival Time	Dep Time	Location
Crystal Serenity	922	Crystal Cruise Lines	781	4/25/2016	Monday	8:00 AM	5:00 PM	P-2
Volendam	1,432	Holland America	781	4/28/2016	Thursday	8:00 PM	5:00 PM	P-2
Silver Shadow	382	Silver Seas	610	5/17/2016	Tuesday	8:00 AM	6:00 PM	P-2
Maasdam	1,898	Holland America	610	6/1/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	6/15/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	6/29/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	7/13/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	7/27/2016	Wednesday	7:00 AM	2:00 PM	P-2
Crystal Serenity	922	Crystal Cruise Lines	610	8/10/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	8/17/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	8/24/2016	Wednesday	7:00 AM	2:00 PM	P-2
Silver Shadow	382	Silver Seas	610	9/20/2016	Tuesday	9:00 AM	3:00 PM	P-2

Table 25: Port of Kodiak, cruise ship schedule, 2016.

and stairway design standards should be followed. This includes providing level stopping points on long sections of steep trail and providing adequate handrails on any steps or stairways.

Sidewalk widths should vary based on expected pedestrian volume and the character of location. For example downtown sidewalks should be wider than sidewalks within residential areas. Typically, sidewalks should provide a five-foot minimum width; this would allow two people moving in opposite directions to pass

each other. Wider sidewalks are desirable in areas where people are expected to walk in pairs or groups. Having wider sidewalks in areas with higher volumes of vehicle traffic provides pedestrians more separation so that they are not right next to traffic. All the sidewalk and pathway improvements recommended in this plan provide a six-foot minimum width for paths on routes that lead to destinations where visitors and locals are expected to walk in larger groups.

Several comments from the public noted issues with ice and snow on sidewalks. Icy sidewalks can create a hazard and cause pedestrians to walk in the streets. The City should maintain sidewalks as snow and ice free as possible. During site visits it was observed that cars parked on sidewalks with rolled curbs. Cars blocks pedestrians from using the sidewalk facilities. The city should enforce parking violations that create conflicts with pedestrian use of sidewalks and paths.

Create Pedestrian Amenity Nodes

Create pedestrian destination nodes for tourists. These could be specific points along pedestrian routes that have amenities such as benches, informative/

interpretive signs, and provide scenic views. A recommendation from Discover Kodiak is to create structures that frame views of Kodiak that visitors can take photos through. This element is adopted from the National Parks Service (NPS) “#FindYourPark” campaign in which frame structures are provided at tourist destinations offering a “framed” photograph opportunity for visitors.

Standardized Pedestrian Lighting

Pedestrian lighting can transform a street from a corridor just for cars to a comfortable route for pedestrians. Lighting at the pedestrian scale is both an aesthetic improvement and safety improvement. It enhances visibility between people and cars. Pedestrian lighting should be considered along pedestrian routes that are not along roadways (i.e., trails or stairways), and on sidewalks where street lights are not sufficient or that pedestrians frequently use. The COK should consider adopting a standard pedestrian scale light fixture and luminaire.

Benefits of having a standard style of lighting include:

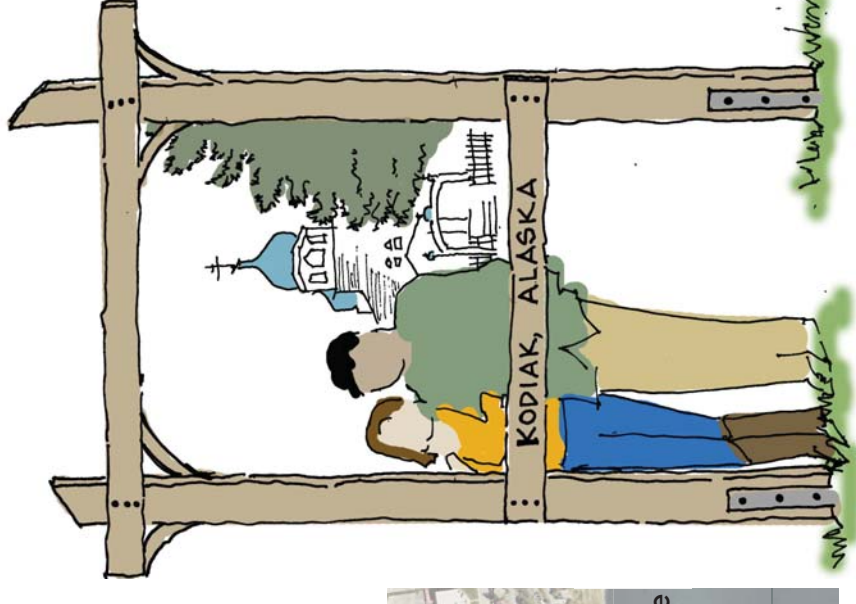
- maintenance personnel can become familiar and

- efficient with one type of fixture;
- the City will only need to keep spare parts for one product;
- the City can make bulk boarders which can reduce costs; and
- one style of lighting will provide a uniform appearance throughout walking areas.

Pedestrian Nodes



Figure 21: Potential pedestrian nodes with frame structures that take advantage of views.



Update Themed Walking Routes On The Discover Kodiak Map

- Cultural
- Shopping
- Nature Hike
- Waterfront



Figure 20: Diagram of how pedestrian routes on the Discover Kodiak Map could be updated once projects are implemented.



Image 37: View of where a pedestrian amenity node could be.

SECTION IV: IMPLEMENTATION

Implementation Order

The ranking that resulted from project scores is a recommended order of project implementation. The project scores, resulted in the following Rank Order:

1. Crosswalk Improvements on Rezanof Drive- L5 (score 20)
2. Wayfinding Plan- O1 (score 18)
3. Sidewalk on Egan Way- L1 (score 17)
4. Pave Sidewalk on Rezanof Drive- B2 (score 16)
5. Sidewalk along Mission Road- M1 (score 12)
6. Trail from Erskine Avenue to Library- L2 (score 12)
7. Trail from Rezanof Drive to Erskine Avenue- L3 (score 9)
8. Connection from Lower Erskine Avenue to Rezanof Drive- B1 (score 8)
9. Trail from Mission Road to Bridge Connection- M2 (score 8)
10. Widen Sidewalk on Rezanof Drive- L4 (score 8)

4 Ranked at number one with an overall score of twenty is providing a crosswalk between the bridge to Near Island and Rezanof Drive's north side. This could be a quick, low cost, and potentially highly used project. This project requires a study to determine if this area warrants a crosswalk and what appropriate signs and lighting would be required by DOT&PF. If a study does show a need, a permit from DOT&PF would be required.

Two projects tied with a score of twelve: the sidewalk along Mission Road and the trail from Erskine Avenue to the Library. The Mission Road sidewalk was ranked higher due to a higher expected use by pedestrians. It would serve a section of road that currently has no sidewalk, whereas, the trail to the library would be an additional alternative connection to the library.

Three projects received a score of eight:

- the connection from lower Erskine Avenue to Rezanof Drive,
- the trail from Mission Road to the bridge connection, and
- widening the sidewalk on Rezanof Drive.

the KIB, COK, DOT&PF, Alaska Department of Fish and Game (ADF&G), United States Fish and Wildlife Service (USFWS), and private property owners.

A majority of the proposed pedestrian facilities will likely be permissible within the various rights-of-way held by KIB, COK, and DOT&PF. There is one parcel where multiple entities may have to agree to trail construction and use, parties include the USFWS and DOT&PF. The remaining areas include a few properties where public use easements will need to be acquired from private property owners.

FUNDING

There are several funding sources available for bicycle and pedestrian projects. "Pedestrian and Bicycle Funding Opportunities U.S. Department of Transportation Transit, Highway, and Safety Funds", is attached to this Plan as an appendix for reference. Given that recent program budgets have been shrinking nationwide, funding partners are seeking to leverage funds¹ from multiple sources to complete transportation projects. The funding sources discussed below are those most commonly involved in funding bike and pedestrian-related projects statewide.

FEDERAL:

U.S. Department of Transportation - Transportation Investment Generating Economic Recovery (TIGER)

The U.S. Department of Transportation (U.S. DOT) has made nearly \$500 million available annually for transportation projects since 2010 through the TIGER grant program. Applications under this program are successful if the project being applied for demonstrates construction readiness (National Environmental Protection Agency (NEPA) and design work is complete), the ability to leverage funds, create jobs, and enhance the economic well-being within a community. The TIGER grant program supports innovative projects,

¹ *Leveraging funding is the process of using multiple funding sources for one project. For example, a grant program may require the grant applicant provide a 20% cost share (match). The grant applicant can use local funds or other funding sources to provide the cost share for the project. Funding agencies are more likely to fund a project if other funds are assigned to the project already.*

including multi-modal and multi-jurisdictional projects; bicycle lanes, parking, transit, bus shelters and benches, crosswalks; sidewalk improvements such as lighting, curb cuts and American with Disabilities Act (ADA) ramps; and paved shoulders for pedestrian and bicyclist use.

The TIGER grant program is focused on capital projects that generate economic development and improve access to reliable, safe and affordable transportation for communities, both urban and rural. TIGER grant program funding opportunities are highly competitive, and are typically announced in late spring.

U.S. Department of Transportation - Federal Transit Administration (FTA)

The FTA provides formula funding to help cities, towns, and rural areas invest in bicycle infrastructure that can improve mobility and help people access public transportation. A local transit provider, such as Kodiak Area Transit System (KATS), may be interested in partnering with a local government to improve sidewalks and bicycle paths so that residents have better access to their transit system. This opportunity is developed through partnerships and leveraging funding. There is no solicitation for this opportunity.

U.S. Economic Development Administration

The Economic Development Administration (EDA) solicits applications from applicants in rural and urban areas to provide investments that support construction, non-construction, technical assistance, and revolving loan fund projects under EDA's Public Works and other programs. Grants and cooperative agreements made under these programs are designed to leverage existing regional assets and support the implementation of economic development strategies that advance new ideas and creative approaches to advance economic prosperity in distressed communities. EDA provides strategic investments on a competitive- merit-basis to support economic development, foster job creation, and attract private investment in economically distressed areas of the United States. This opportunity is open year round. A grant applicant can meet with the local EDA Program Manager out of Anchorage to determine eligibility.

Federal Highway Administration

The Federal Highway Administration (FHWA) administers what is often referred to as Chapter 1 funds authorized under current transportation legislation. (Chapter 1 of 23 USC is where the regulations governing these funds are located.) This agency works with each state to expend the state's share of these federal transportation dollars for programs such as the Surface Transportation Program, Transportation Alternatives Program, Tribal Transportation Program, and Tribal Transportation Safety programs, and all transit programs. In Alaska, FHWA's partner agency is the Alaska Department of Transportation & Public Facilities (DOT&PF). Funding is administered through DOT&PF.

Western Federal Lands Highway Division

Western Federal Lands Highway Division (WFLHD) of the Office of Federal Lands Highway (FLH) jointly administers the Tribal Transportation Program with the Bureau of Indian Affairs (BIA). Each federally recognized tribe is eligible for this funding and is allocated an annual dollar amount based on a codified formula that takes into account tribal population, road mileage, and average tribal shares. These funds are often referred to as Chapter 2 funds and can be used by tribes as local match funds on projects funded with Chapter 1 funding.

WFLHD also administers Alaska's Federal Lands Access (FLAP) Program; a program for surface transportation facilities providing access to, or within, federally-owned lands. This program is designed to encourage cooperation and coordination among federal land management agencies, state agencies, and local and tribal governments. Funding is administered through DOT&PF.

Bureau of Indian Affairs

The Tribal Transportation Program (TTP) provides tribes with a percentage of funding allocated based on highway legislation to plan, design, construct, and maintain their local transportation system. TTP funds can be used for safety, transit, administration, maintenance, bike and pedestrian facilities, and planning. These funds can be leveraged and used as non-federal match. This opportunity is developed through partnerships and leveraging funding. There is no solicitation for this opportunity.

STATE:

Alaska Surface Transportation Block Grant (STBG) Set-Aside Program

The Fixing America's Surface Transportation (FAST) Act of 2015 includes the Surface Transportation Block Grant (STBG) Program, which sets aside funds for use for a broad range of projects, including walking and bicycling projects. Funding levels in the STBG Set-Aside Program are set at \$835 million for financial years 2016 and 2017, rising to \$850 million in financial years 2018-2020. This funding is distributed to the state and local level on a formula-set apportionment basis. A local match is required for all projects. Funding opportunities are typically made available in late spring.

Legislature

Each year the Alaska Legislature develops both capital and operating budgets for the state. In years when the state's fiscal situation allows, transportation projects for areas across the state are often included as line items in the capital budget. Additionally, the legislature periodically drafts bond bills that are then voted on by state residents during general elections. Unlike capital budget line items, items identified in an approved bond bill are funded through the sale of general obligation bonds, which are repaid at a later date using specified state revenues.

Department of Transportation & Public Facilities

As FHWA's partner agency for the State of Alaska, DOT&PF is responsible for the planning and programming of funding under the purview of FHWA. Several types of funding DOT&PF administers allow tribal governments, municipal governments, and other similar entities to nominate projects for inclusion in the Statewide Transportation Improvement Program (STIP) or compete for grant-like funding to complete projects. The State of Alaska also has a State Highway Safety Improvement Program (HSIP). The HSIP is a core Federal-aid program with the purpose of achieving a significant reduction in fatalities and serious injuries on all public roads. Funding is administered through DOT&PF.

Department of Commerce, Community, and Economic Development

Alaska's Department of Commerce, Community, and Economic Development (DCCED) administers several programs of interest in terms of developing and maintaining transportation infrastructure vital to a community's success. Most notably, it administers the Community Development Block Grant (CDBG) program, funded by the U.S. Department of Housing and Urban Development (HUD). Once each year, municipal governments are able to apply for CDBG funding for an array of project types, which include transportation improvements such as bicycle and pedestrian facilities. In addition to capital projects, HUD also allows CDBG funding to be used for planning efforts. Funding is administered through the State of Alaska.

DESIGN & CONSTRUCTION

The level of design and construction for each project varies. Some projects are technically complex, requiring retaining walls or footings in the case of elevated stairs. Others have relatively minimal design and construction efforts, such as short sections of gravel trail. In either case the design will need to be reviewed and permitted by the appropriate authority such as the COK Building Safety Department or DOT&PF.

Depending on the complexity of the project, the construction work will likely need to be contracted out to professional contractors. In cases where construction is minimal, there may be opportunities for volunteers to assist with construction. This may be possible for short sections of gravel trail that have minimal grading restrictions, or sign installation.

Conclusion

As a coastal town the COK has a unique, authentic working industrial waterfront with ocean and mountainous views. By improving pedestrian access to the waterfront, downtown, the Study Area in this plan and beyond, tourists and locals will have opportunities to walk to local businesses, cultural sites and community events. A pedestrian network improves alternative transportation, provides health benefits, contributes to the local economy, and can contribute to an overall sense of place.

Appendix A: Public Comment

The following table outlines the public comment received over the duration on the project. The comments are categorized into two categories:

- Specific Route Comments
- General Comments

Within those two categories the comments are further divided into topics, this includes:

- Specific Route Comments:
 - Crosswalks
 - Connection at the Bridge
 - Connections Under the Bridge (to Marine Way)
 - Mission Road
 - Marine Way/Park at Pearson Cove
 - Connection to the Library
 - Other Route Considerations
- General Comments:
 - Wayfinding/Signage
 - Visitor Circulation & Cruise Ship Passenger Drop-Off
 - Miscellaneous

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Specific Route Improvements				
Crosswalk				
1	Crosswalks on Rezanof should have light up crossing signs to alert motorists on this blind corner.	Web	1	A crosswalk is proposed across Rezanof to the bridge (project L5). Whether or not a crosswalk is feasible and what features are appropriate will need to be determined with appropriate analysis as noted in the L5 project description table.
2	Near the intersection of Rezanof Drive and the bridge, an inlaid crosswalk is needed. Inlaid crosswalks may cost more upfront, but they last much longer so the cost is recovered.	Comment Form	1	See response to comment 1.
3	Flashing sign for crosswalk. (Arrow pointing to crossing on Upper Mill Bay Road near the intersection with Wilson Ave.)	Map Comments (Open House #2)	1	Since this plan is for improving the pedestrian connections for tourist destinations this was not included as a potential pedestrian improvement because it does not appear to connect to a tourist destination or route.
4	I worry about elderly people crossing Rezanof. Is there some way to connect route "G" directly to the Bridge and route "C" to downtown? At least this is a major intersection with excellent visibility both up and down Rezanof.	Web	1	A series of projects are proposed to create this connection between route "C" and "G"; this would include projects: L2, L3, L4, L5, and B1 (A or B). See the overall schematic for how each project is linked.
5	"Route B" is my preferred route to Near Island so I would suggest a pedestrian walkway on the south side of Rezanof."	Web	1	Project B2 is to pave a sidewalk along this section of Rezanof.
6	Seepage from natural spring down the hill from the library can make the sidewalk slick.	Map Exercise (Open House #1)	1	This has been noted on the public comment map, and noted as a design consideration in the L4 sidewalk project.
7	Dirt path along Rezanof prevents ice.	Map Exercise (Open House #1)	1	Project B2 is to pave a sidewalk along this section of Rezanof. Currently the surface is dirt and gravel which is less accessible than a paved sidewalk. Although this section of sidewalk seems less slick than the north sidewalk it may be because there appears to be drainage seeping from the hillside at that location as noted by other members of the public.
8	Easy and helpful improvement would be a crosswalk on Rezanof at Upper Mill Bay. People walk to and from the Borough buildings, an high school/middle school, main elementary to the downtown area and that is where they cross.	General Comments (Open House #2)	1	Project L5 recommends a crosswalk at the bridge, but the best location for a crosswalk will need to be determined with proper analysis. The crosswalk option at Mill Bay Road has been noted in the L5 project table under other comments.
9	Crosswalk school side. (Intersection of Upper Mill Bay Road and Rezanof Drive.)	Map Comments (Open House #2)	1	See response to comment 8.
10	Crosswalk to walkway side of bridge. (Across Rezanof)	Map Comments (Open House #2)	1	A crosswalk is proposed across Rezanof to the bridge (project L5).
11	Heavy pedestrian traffic, including school students on lunch break, yet it has no caution signs for motorists. At least one senior citizen pedestrian fatality happened here. Note proximity of this nasty intersection to the Senior Center. (X2)	Map Comments (Open House #2)	1	A cross-walk is proposed across Rezanof to the bridge (project L5). Whether or not a cross-walk is feasible and what features are appropriate will need to be determined with full analysis.
12	Sidewalk! (Arrow pointing to intersection of Upper Mill Bay Road and Rezanof Drive.)	Map Comments (Open House #2)	1	This has been noted on the public comment map.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Connection at Bridge				
13	We should avoid doing a stair set if at all possible to maintain access to bicycles.	Web	1	As an alternative to a stairway Project B1-A, a sloped trail, has been included in the potential projects for this area.
14	Route C- it's used already but muddy, steep, unsafe.	Comment Form	1	Project B1 is proposed to improve these conditions; this would either be a sloped sidewalk or stairway.
15	There is a social trail that cuts up the hill to Rezanof from the end of Erskine.	"Map Exercise (Open House #1)"	1	See response to comment 14.
16	Walk up from Senior Center parking to the bridge can feel unsafe due to brush and poor visibility.	"Map Exercise (Open House #1)"	1	See response to comment 14.
17	Stairs at bridge.	"Map Exercise (Open House #1)"	1	See response to comment 14.
18	Stairs with lighting (at bridge).	"Map Exercise (Open House #1)"	1	See response to comment 14.
19	Information, signage, views (at bridge stairs).	"Map Exercise (Open House #1)"	1	Noted in the project description of B1.
20	Possible connection from end of lower Erskine up the hill to Rezanof.	"Map Exercise (Open House #1)"	1	See response to comment 19.
21	New Mortuary along lower Erskine.	"Map Exercise (Open House #1)"	1	This has been noted on the public comment map.
22	Route pedestrians around parking lot at Senior Center.	"Map Exercise (Open House #1)"	1	Project B1 can address this by tying a new sidewalk into the existing one at the end of Erskine Avenue.
23	Difficult to cross parking lot near Senior Center.	"Map Exercise (Open House #1)"	1	See response to comment 22.
24	Busy intersection: boat traffic, gravel trucks, etc. (intersection at bridge to Near Island)	Map Comments (Open House #2)	1	This has been noted on the public comment map.
25	(Arrow pointing out metal stairs, galvanized steel treads and landings.)	Map Comments (Open House #2)	1	This is included in the design considerations for project B1-B.
26	Many people go up and down this bank (slope along Rezanof Dr. north of the Senior Center). Need steel stairs a la Best Western to Shellikof.	Map Comments (Open House #2)	1	Included as a recommendation in project B1-B.
27	Routes from Erskine to Rezanof could be potential emergency route for residents of housing near Senior Center.	Map Comments (Open House #2)	1	This has been noted on the public comment map.
28	Steel grate type of stairs might nag certain types of ice cleats, causing falls. Sign at top and bottom of these (new) stairs asking pedestrians to remove them (cleats) might help.	Map Comments (Open House #2)	1	Noted in the project description of B1.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Connections Under Bridge (to Marine Way)				
29	What about access from Marine Way, down by F&A and Power house?	Comment Form	1	This is outside of the Study Area of this plan but has been noted in the General Consideration section for further exploration.
30	It would be nice to have a pathway that connects Marine Way (under the bridge) to the bridge.	Comment Form	1	See response to comment 29.
31	KEA easement? (noted under bridge near Marine Way).	Map Exercise (Open House #1)	1	See response to comment 29.
32	Link from Mission under bridge to Powerhouse drawn on map.	Map Exercise (Open House #1)	2	See response to comment 29.
33	Route from Marine Way to bridge drawn.	Map Exercise (Open House #1)	4	See response to comment 29.
34	Good idea (trail under the bridge connecting Mission Road to Marine Way).	Map Comments (Open House #2)	1	See response to comment 29.
Mission Road				
35	Old sidewalk under dirt/gravel along Mission Road past the church.	Map Exercise (Open House #1)	1	This has been noted on the public comment map.
36	Poor or no lighting along Mission.	Map Exercise (Open House #1)	1	Under the General Considerations section there are recommendations to enhance pedestrian lighting where existing lighting is nonexistent or insufficient.
37	Mission Road "not pedestrian friendly".	Map Exercise (Open House #1)	1	Project M1 is included to improve pedestrian connections along Mission Road.
38	Ideal route because already sidewalks (along Mission near Center).	Map Exercise (Open House #1)	1	The Mission Road projects include M1 & M2. M1 is a sidewalk along the southeast side of the road and M2 is a trail that connects up to the bridge pedestrian connection.
39	Two houses along Mission near Erskine are condemned.	Map Exercise (Open House #1)	2	This has been noted on the public comment map.
40	Potential hoop house as a part of St. Herman's Chapel on Mission.	Map Exercise (Open House #1)	1	See response to comment 39.
41	Properties along Mission belong to Seminary.	Map Exercise (Open House #1)	1	See response to comment 39.
42	House between two condemned buildings along Mission belongs to Seminary.	Map Exercise (Open House #1)	1	See response to comment 39.
43	Main Seminary parking lot along Mission.	Map Exercise (Open House #1)	2	Because of the Seminary location the sidewalk along Mission Road is recommended to be on the southeast side of the road to improve pedestrian access to the Seminary (project M1).
44	Sidewalk on Mission after Tagura would require retaining wall.	Map Exercise (Open House #1)	1	This has been considered for the preliminary cost estimate for project M1.
45	High pedestrian traffic along Mission from Seminary.	Map Exercise (Open House #1)	1	Noted, this is one of the reasons that the sidewalk (project M1) was recommended to be on the Seminary side of Mission Road.
46	Existing driveway below seminary potential- pedestrian path.	Map Exercise (Open House #1)	1	This is outside of the study area but has been noted on the public comment map.
47	Safety concern- sidewalk on Mission.	General Comments (Open House #2)	1	A sidewalk (project M1) has been proposed to improve safety.

	Comment	Comment Source	Number of Times Comment Appeared	Response
Marine Way/Park at Pearson Cove				
48	Marine Way drawn as a route on comment map notes; "Marine Way is a great walk".	Map Exercise (Open House #1)	1	This is outside of the Study Area of this plan but has been noted in the General Consideration section for further exploration.
49	Pearson Cove Park drawn on map.	Map Exercise (Open House #1)	1	See response to comment 48.
50	Route drawn along Marine Way.	Map Exercise (Open House #1)	1	See response to comment 48.
51	Connection to park at Pearson Cove drawn.	Map Exercise (Open House #1)	1	See response to comment 48.
52	Route drawn from Pearson Cove drawn to Steller Way.	Map Exercise (Open House #1)	1	See response to comment 48.
Connection to Library				
53	Coming down "A" or "B" there is very little to see except a graveyard and possibly a view of Kodiak with mountains in the background, which I imagine you can get from "G".	Web	1	Route "G" developed into projects L2 & L3 which provide scenic views of the water and surrounding mountains.
54	Route from Mill Bay Road to Library noted as "Trail?"	Map Exercise (Open House #1)	1	We did not see a trail at this location but did see one coming south from the library which was proposed as project L2.
55	Route drawn from Rezanof to Library via trail.	Map Exercise (Open House #1)	1	This social trail was located and has been included as project L2.
56	Route drawn from curve in Erskine to Library.	Map Exercise (Open House #1)	6	See response to comment 55.
57	Free wifi, tourist/cruise destination (Library)	Map Comments (Open House #2)	1	This has been noted on the public comment map.
58	Fix drainage (existing seepage). (Sidewalk on Upper Mill Bay Road near intersection with Egan Way)	Map Comments (Open House #2)	1	This has been noted on the public comment map, and noted as a design consideration in the L1 sidewalk project.
59	Pubic Radio KMXT noted on plan next to Library.	Map Comments (Open House #2)	1	See response to comment 58.
60	Already a foot path along fence that allows loop around Library. (Along the east side of the U.S. Fish & Wildlife Service property and Alascom property).	Map Comments (Open House #2)	1	Noted, projects L2 & L3 would formalize a trail through this area.
Other Route Considerations				
61	Route drawn from Upper Mill Bay Road going northwest to Post Office.	Map Exercise (Open House #1)	1	This is outside of the Study Area but has been noted on the public comment map.
62	Students from High School go down Mill bay to Subway almost every day.	Map Exercise (Open House #1)	1	This has been noted on the public comment map.
63	Route on Tagura Road drawn.	Map Exercise (Open House #1)	2	See response to comment 61.
64	Trident workers walk from housing down Center Avenue and Mission Road to Trident facility.	Map Exercise (Open House #1)	2	See response to comment 62.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
General Considerations				
Wayfinding/Signage				
65	How long they (tourists) spend in Kodiak to see the sights is key, thus every attraction should be marked with how long it takes to walk there and perhaps the stress involved e.g. uphill.	Web	1	This has been included in the Wayfinding project O1.
66	The weather will play a big part in what visitors get to see and do, so you need to divide attractions and the routes to them by indoors/outdoors.	Web	1	This has been noted on the public comment map.
67	Rather than putting all this information on signs, it would be best to produce a "walking map/brochure" that the cruise ships can give to their passengers before they dock. This will give passengers time to plan.	Web	1	Discover Kodiak publishes a map that is distributed to visitors. Part of the recommendations of this plan is to update that map with new routes as they are developed.
68	Plan on incorporating interpretive signs into all pathways.	Comment Form	1	The design considerations for project O1 (wayfinding) include a recommendation to have interpretive signs.
69	Easy signs directing people to the library on existing sidewalk on upper Mill Bay to Egan.	General Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to have signage to the library.
70	Inexpensive to upgrade existing walking paths with signage and lighting.	General Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to develop signage. The General Recommendations section recommends pedestrian lighting improvements.
71	Small Boat Harbor wayfinding. (at Rezanof intersection with bridge)	Map Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to have signage to attractions on Near Island.
72	Sign for Library (at intersection of upper Erskine Ave. with Rezanof Dr.).	Map Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to have signage to the library.
73	Library wayfinding sign needed in area. (Area near Mill Bay Road north of Rezanof Drive.	Map Comments (Open House #2)	1	See response to comment 72.
74	Sign "Near Island Trails" & "Dog Bay Small Boat Harbor". Sign @ Y/Alutiiq directing people toward Small Boat Harbor.	Map Comments (Open House #2)	1	See response to comment 72.
Visitor Circulation Considerations & Cruise Ship Passenger Drop-off				
75	If visitors are dropped off at the Alutiiq Museum there is a lot of uphill to reach the library or the bridge. Maybe taxis and vans could drop visitors at the library and then its all downhill from there.	Web	1	This has been noted on the public comment map.
76	Many of the visitors I have seen appear to be elderly, so unless you plan on building a tunnel or overpass, I think that the safest route to cross Rezanof would be coming downhill.	Web	1	See response to comment 75.
77	From the visitors perspective, presumably most will come from cruise ships.	Web	1	See response to comment 75.
78	Cruise ship passenger bus stop passenger load/unload, 60-80 people, in front of Alutiiq Museum.	Map Exercise (Open House #1)	1	See response to comment 75.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Miscellaneous				
79	The City should enforce its ordinance prohibiting parking on sidewalks.	Comment Form	1	This has been noted on the public comment map.
80	Love the bulkhead parking along Shelikof and I would love even more to incorporate the Following: interpretive signs, covered bike shelter, public art.	Comment Form	1	See response to comment 79.
81	There aren't sidewalks in places that are actually useful for pedestrians, especially downtown.	Comment Form	1	The goal of this plan is to highlight potential projects that will improve the network of pedestrian connections. If implemented these projects will add more useful sidewalks and trails.
82	There aren't useful pathways that connect Rezanof/Mission/Mill Bay, in fact, there are only (maybe) 3 roads that directly connect what are the major arteries in Kodiak. How can we create paths that will do what roads can't? I think we can.	Comment Form	1	The projects proposed in this plan will increase pedestrian options in the Study Area. See the overall schematic for the proposed network of paths.
83	"Old Dorms" at corner with Library.	Map Exercise (Open House #1)	1	This has been noted on the public comment map.
84	Future bronze bear statue at the Kodiak National Wildlife Refuge Visitor Center summer 2016.	Map Exercise (Open House #1)	1	See response to comment 83.
85	Bicyclists and skateboarders almost run over pedestrians when the former don't use bike paths like they're supposed to.	Map Comments (Open House #2)	1	See response to comment 83.
86	Snow, ice removal from sidewalks traditionally get last priority, I've noticed in the 40 years I've lived here. This forces pedestrians off the sidewalk and into the road shoulder. I'm amazed that nobody has been killed yet!	Map Comments (Open House #2)	1	Maintaining the sidewalks free of snow and ice was included in the General Recommendations section.
87	Flashing light @ crosswalk @ McDonald's	Map Comments (Open House #2)	1	This is outside of the Study Area of this plan but has been noted on the public comment map.
88	Bridge needs tall barrier on both sides, as a teenage suicide almost happened here. Companions with would be victim grabbed her before she jumped.	Map Comments (Open House #2)	1	See response to comment 87.
89	Noted AK Fish & Game Bunkhouse on building near bridge.	Map Comments (Open House #2)	1	This has been noted on the public comment map.

Appendix B: Federal Pedestrian and Bicycle Funding Opportunities

Pedestrian and Bicycle Funding Opportunities U.S. Department of Transportation Transit, Highway, and Safety Funds

Revised August 12, 2016

This table indicates potential eligibility for pedestrian and bicycle projects under U.S. Department of Transportation surface transportation funding programs. Additional restrictions may apply. See notes and basic program requirements below, and see program guidance for detailed requirements. Project sponsors should fully integrate nonmotorized accommodation into surface transportation projects. Section 1404 of the Fixing America's Surface Transportation (FAST) Act modified 23 U.S.C. 109 to require federally-funded projects on the National Highway System to consider access for other modes of transportation, and provides greater design flexibility to do so.

Activity or Project Type	U.S. Department of Transportation Transit, Highway, and Safety Funds														
	TIGER	TIFIA	FTA	ATI	CMAQ	HSIP	NHPP	STBG	TA	RTP	SRTS	PLAN	NHTSA 402	NHTSA 405	FLTP
Access enhancements to public transportation (includes benches, bus pads)	\$	\$	\$	\$	\$		\$	\$	\$						\$
ADA/504 Self Evaluation / Transition Plan								\$	\$			\$			\$
Bicycle plans			\$					\$	\$		\$	\$			\$
Bicycle helmets (project or training related)								\$	\$SRTS		\$		\$*		
Bicycle helmets (safety promotion)								\$	\$SRTS		\$				
Bicycle lanes on road	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Bicycle parking	~\$	~\$	\$	\$	\$		\$	\$	\$		\$				\$
Bike racks on transit	\$	\$	\$	\$	\$			\$	\$		\$				\$
Bicycle share (capital and equipment; not operations)	\$	\$	\$	\$	\$		\$	\$	\$		\$				\$
Bicycle storage or service centers at transit hubs	~\$	~\$	\$	\$	\$			\$	\$		\$				\$
Bridges / overcrossings for pedestrians and/or bicyclists	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Bus shelters and benches	\$	\$	\$	\$	\$		\$	\$	\$		\$				\$
Coordinator positions (State or local)					\$ 1 per State			\$	\$SRTS		\$				
Crosswalks (new or retrofit)	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Curb cuts and ramps	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$
Counting equipment			\$	\$		\$	\$	\$	\$	\$	\$	\$*			\$
Data collection and monitoring for pedestrians and/or bicyclists			\$	\$		\$	\$	\$	\$	\$	\$	\$*			\$
Historic preservation (pedestrian and bicycle and transit facilities)	\$	\$	\$	\$				\$	\$						\$
Landscaping, streetscaping (pedestrian and/or bicycle route; transit access); related amenities (benches, water fountains); generally as part of a larger project	~\$	~\$	\$	\$			\$	\$	\$						\$
Lighting (pedestrian and bicyclist scale associated with pedestrian/bicyclist project)	\$	\$	\$	\$		\$	\$	\$	\$	\$	\$				\$
Maps (for pedestrians and/or bicyclists)			\$	\$	\$			\$	\$		\$	\$*			
Paved shoulders for pedestrian and/or bicyclist use	\$	\$			\$*	\$	\$	\$	\$	\$	\$				\$

Key: \$ = Funds may be used for this activity (restrictions may apply). \$* = See program-specific notes for restrictions. ~\$ = Eligible, but not competitive unless part of a larger project.

Pedestrian and Bicycle Funding Opportunities															
U.S. Department of Transportation Transit, Highway, and Safety Funds															
Activity or Project Type	TIGER	TIFIA	FTA	ATI	CMAQ	HSIP	NHPP	STBG	TA	RTP	SRTS	PLAN	NHTSA 402	NHTSA 405	FLTP
Pedestrian plans			\$						\$		\$	\$			\$
Recreational trails	~\$	~\$							\$		\$				\$
Road Diets (pedestrian and bicycle portions)	\$	\$				\$			\$						\$
Road Safety Assessment for pedestrians and bicyclists						\$			\$			\$			\$
Safety education and awareness activities and programs to inform pedestrians, bicyclists, and motorists on ped/bike safety									\$SRTS		\$	\$*	\$*	\$*	
Safety education positions									\$SRTS		\$		\$*		
Safety enforcement (including police patrols)									\$SRTS		\$		\$*	\$*	
Safety program technical assessment (for peds/bicyclists)									\$SRTS		\$	\$*	\$		
Separated bicycle lanes	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Shared use paths / transportation trails	\$	\$	\$	\$	\$*	\$	\$	\$	\$		\$				\$
Sidewalks (new or retrofit)	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Signs / signals / signal improvements	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Signed pedestrian or bicycle routes	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Spot improvement programs	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Stormwater impacts related to pedestrian and bicycle projects	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Traffic calming	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Trail bridges	\$	\$	\$	\$	\$*	\$	\$	\$	\$		\$				\$
Trail construction and maintenance equipment									\$RTP	\$RTP	\$				
Trail/highway intersections	\$	\$	\$	\$	\$*	\$	\$	\$	\$		\$				\$
Trailside and trailhead facilities (includes restrooms and water, but not general park amenities; see guidance)	~\$*	~\$*							\$*	\$*	\$				\$
Training					\$	\$	\$	\$	\$		\$	\$*	\$*		
Training for law enforcement on ped/bicyclist safety laws									\$SRTS	\$SRTS	\$			\$*	
Tunnels / undercrossings for pedestrians and/or bicyclists	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$				\$

Abbreviations

ADA/504: Americans with Disabilities Act of 1990 / Section 504 of the Rehabilitation Act of 1973
TIGER: Transportation Investment Generating Economic Recovery Discretionary Grant program
TIFIA: Transportation Infrastructure Finance and Innovation Act (loans)
FTA: Federal Transit Administration Capital Funds
ATI: Associated Transit Improvement (1% set-aside of FTA)
CMAQ: Congestion Mitigation and Air Quality Improvement Program
HSIP: Highway Safety Improvement Program
NHPP: National Highway Performance Program
STBG: Surface Transportation Block Grant Program
TA: Transportation Alternatives Set-Aside (formerly Transportation Alternatives Program)
RTP: Recreational Trails Program
SRTS: Safe Routes to School Program / Activities
PLAN: Statewide Planning and Research (SPR) or Metropolitan Planning funds
NHTSA 402: State and Community Highway Safety Grant Program
NHTSA 405: National Priority Safety Programs (Nonmotorized safety)
FLTP: Federal Lands and Tribal Transportation Programs (Federal Lands Access Program, Federal Lands Transportation Program, Tribal Transportation Program, Nationally Significant Federal Lands and Tribal Projects)

Program-specific notes

Federal-aid funding programs have specific requirements that projects must meet, and eligibility must be determined on a case-by-case basis. For example:

- TIGER: Subject to annual appropriations.
- TIFIA: Program offers assistance only in the form of secured loans, loan guarantees, or standby lines of credit, but can be combined with other grant sources, subject to total Federal assistance limitations.
- FTA/ATI: Project funded with FTA transit funds must provide access to transit. See [Bikes and Transit](#) and the FTA Final Policy Statement on the [Eligibility of Pedestrian and Bicycle Improvements under Federal Transit Law](#).
 - Bicycle infrastructure plans and projects funded with FTA funds must be within a 3 mile radius of a transit stop or station, or if further than 3 miles, must be within the distance that people could be expected to safely and conveniently bike to use the particular stop or station.
 - Pedestrian infrastructure plans and projects funded with FTA funds must be within a ½ mile radius of a transit stop or station, or if further than ½ mile, must be within the distance that people could be expected to safely and conveniently walk to use the particular stop or station.
 - FTA funds cannot be used to purchase bicycles for bike share systems.
 - FTA encourages grantees to use FHWA funds as a primary source for public right-of-way projects.
- CMAQ projects must demonstrate emissions reduction and benefit air quality. See the CMAQ guidance at www.fhwa.dot.gov/environment/air_quality/cmaq/ for a list of projects that may be eligible for CMAQ funds. Several activities may be eligible for CMAQ funds as part of a bicycle and pedestrian-related project, but not as a highway project. CMAQ funds may be used for shared use paths, but may not be used for trails that are primarily for recreational use.
- HSIP projects must be consistent with a State’s [Strategic Highway Safety Plan](#) and either (1) correct or improve a hazardous road location or feature, or (2) address a highway safety problem.
- NHPP projects must benefit National Highway System (NHS) corridors.
- STBG and TA Set-Aside: Activities marked “\$SRTS” means eligible only as an SRTS project benefiting schools for kindergarten through 8th grade. Bicycle transportation nonconstruction projects related to safe bicycle use are eligible under STBG, but not under TA (23 U.S.C. 217(a)).
- RTP must benefit recreational trails, but for any recreational trail use. RTP projects are eligible under TA and STBG, but States may require a transportation purpose.
- SRTS: FY 2012 was the last year for SRTS funds, but SRTS funds are available until expended.
- Planning funds must be used for planning purposes, for example:
 - Maps: System maps and GIS;
 - Safety education and awareness: for transportation safety planning;
 - Safety program technical assessment: for transportation safety planning;
 - Training: bicycle and pedestrian system planning training.
- Federal Lands and Tribal Transportation Programs (FLTTP) projects must provide access to or within Federal or tribal lands:
 - Federal Lands Access Program (FLAP): Open to State and local entities for projects that provide access to or within Federal or tribal lands.
 - Federal Lands Transportation Program: For Federal agencies for projects that provide access within Federal lands.
 - Tribal Transportation Program: available for federally-recognized tribal governments for projects within tribal boundaries and public roads that access tribal lands.
- NHTSA 402 project activity must be included in the State’s Highway Safety Plan. Contact the State Highway Safety Office for details: <http://www.ghsa.org/html/about/shsos.html>
- NHTSA 405 funds are subject to State eligibility, application, and award. Project activity must be included in the State’s Highway Safety Plan. Contact the State Highway Safety Office for details: <http://www.ghsa.org/html/about/shsos.html>

Cross-cutting notes

- FHWA Bicycle and Pedestrian Guidance: http://www.fhwa.dot.gov/environment/bicycle_pedestrian/
- **Applicability of 23 U.S.C. 217(i) for Bicycle Projects:** 23 U.S.C. 217(i) requires that bicycle facilities “be principally for transportation, rather than recreation, purposes”. However, sections 133(b)(6) and 133(h) list “recreational trails projects” as eligible activities under STBG. Therefore, the requirement in 23 U.S.C. 217(i) does not apply to recreational trails projects (including for bicycle use) using STBG funds. Section 217(i) continues to apply to bicycle facilities other than trail-related projects, and section 217(i) continues to apply to bicycle facilities using other Federal-aid Highway Program funds (NHPP, HSIP, CMAQ). The transportation requirement under section 217(i) is applicable only to bicycle projects; it does not apply to any other trail use or transportation mode.
- There may be occasional DOT or agency incentive grants for specific research or technical assistance purposes.
- Aspects of many DOT initiatives may be eligible as individual projects. For example, activities above may benefit Ladders of Opportunity; safe, comfortable, interconnected networks; environmental justice; equity; etc.

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**CITY OF KODIAK
RESOLUTION NUMBER 2015-16**

A RESOLUTION OF THE COUNCIL OF THE CITY OF KODIAK RESCINDING RESOLUTION NO. 2014-19 AND ESTABLISHING FUNDING CRITERIA FOR NONPROFIT GRANTS

WHEREAS, the City Council recognizes and supports local nonprofit organizations and has historically made funding available to these organizations on an annual basis; and

WHEREAS, it has been determined that the appropriate total amount of City funds to grant to nonprofit organizations is a maximum of one percent of budgeted general fund revenues, exclusive of any fund balance appropriation; and

WHEREAS, City funds have been provided to nonprofit organizations that supplement and compliment the services provided to residents by the City; and

WHEREAS, it is the intent of the City Council to update this policy statement.

NOW, THEREFORE, BE IT RESOLVED that the Council of the City of Kodiak, Alaska hereby establishes the following additional funding criteria for nonprofit grants provided by the City:

1. Organizations receiving funds must be legally recognized by the Internal Revenue Service.
2. Funding will be granted only for the following kinds of programs/activities and up to the maximum identified funding amount per organization and program type:

Youth Recreation Programs	\$2,500
Adult Recreation Programs	\$5,000
Public Safety Support Programs (Shelter/Food)	\$10,000
Emergency Response Support Programs	\$10,000
3. Subject to available funding, the Council may authorize a special one-time funding increase for a special project.



ATTEST:

CITY OF KODIAK


MAYOR


DEPUTY CITY CLERK

Adopted: May 28, 2015

CITY OF KODIAK
Nonprofit Grant Applications
Fiscal Year 2018
Calendar

- April 11 Council review of City's nonprofit application and award process
- Week of May 8 Mail applications, FY17 financial reports *(if no change to criteria)*
- June 16 Applications and FY17 financial reports due to the City Manager's Office
- July 11 or 25 City Council evaluates applications
- July 13 or 27 Nonprofit funding resolution on agenda for Council approval
- July 27 Check requests prepared and forwarded to finance
- July 13 or 27 FY16 award notification letters and agreements mailed
- Aug 4 Checks returned to City Manager's Office
- August 4 Grant checks available upon execution of grant agreements and verification of FY17 reports filed

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REQUEST FOR PROPOSALS No. 2017-001
For
General Counsel Legal Services
City of Kodiak, Alaska

Proposals to provide general counsel legal services for the City of Kodiak will be received at the Office of the City Clerk, City Hall, City of Kodiak, 710 Mill Bay Road, Room 219, Kodiak, Alaska 99615 until Tuesday, May 2, 2017, at 4 p.m. Proposals will be date stamped, and the City Clerk will record the time received. Proposals received after the time fixed for the receipt of the bids shall not be considered.

It is the intention of the City of Kodiak to enter into a contract with a general counsel that will serve the Kodiak City Council, the City Manager, City Clerk, and other staff designated by the City Manager. The City Attorney is hired by and serves at the pleasure of the City Council. Proposals are invited from any qualified State of Alaska licensed attorney at law or law firm. Attorney/Firms (proposers) must be qualified to represent the City in all State of Alaska Courts and the U.S. District Court for the District of Alaska.

The RFP is available on the City's website at:

<http://www.city.kodiak.ak.us/citymanager/page/rfp-municipal-attorney-services>

For proposal specifications contact:

Debra Marlar, City Clerk
710 Mill Bay Road, Room 219
Kodiak, Alaska 99615
907 486-8636

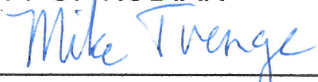
For additional questions regarding this project contact:

Mike Tvenge, Acting City Manager
City of Kodiak
710 Mill Bay Road, Room 114
Kodiak, Alaska 99615
907 486-8640

The City of Kodiak reserves the right to accept or reject any or all proposals, to waive irregularities or informalities in the proposals, and to award the contract to the respondent that best meets the selection criteria. The City shall not accept electronically submitted proposals.

Dated this 23 day of March 2017.

CITY OF KODIAK



Mike Tvenge, Acting City Manager

CITY OF KODIAK

REQUEST FOR PROPOSALS No. 2017-001

**The City of Kodiak is soliciting proposals for
General Counsel Legal Services**

Release Date: March 23, 2017

DEADLINE FOR SUBMISSION: May 2, 2017 by 4 PM

Use this page as the first page of the proposal.

Submitted By:

Attorney/Firm Name _____ Contact Person _____

Address _____

Telephone _____ Fax _____

Email _____ Website URL _____

I have read, understand, and agree to all terms and conditions herein:

Signed _____ Date _____

Printed Name and Title _____

(must be signed by individual with authority to bind the proposer)

TABLE OF CONTENTS

Section	Page No.
I. Overview of Project	3
II. Scope of Services and Minimum Qualifications	3
III. Response to RFP	5
IV. Proposal Evaluation and Award Process	6
V. Submittal Instructions	7-8
VI. General Terms, Conditions, and Exceptions	9

I. OVERVIEW OF REQUESTED SERVICES

The City of Kodiak was incorporated in 1940. In 1965 Kodiak City voters adopted a City Charter. The City of Kodiak operates under the Council-Manager form of government. The population of the City of Kodiak is approximately 6,124. The greater Kodiak area contains approximately 14,000 residents. In addition to City administration (Manager and Clerk), the City is comprised of eight departments. These include engineering; finance; fire/ambulance (EMT); library; parks and recreation; law enforcement (dispatch and jail); port and harbors; and public works, (including water, sewer, and roads). The City operates a water utility and a sanitary sewer system with a wastewater treatment plant. The City is a hub for the maritime industry of Southwest Alaska and home to a large and varied fishing fleet. The Port operates a container terminal and the two small boat harbors accommodate a fleet of over 1,200 fishing and general purpose recreational vessels. The City also operates a large vessel lift and boat yard.

The City is soliciting proposals for general counsel legal services. It is the intention of the City through this solicitation to enter into a contract with a general counsel that will serve the Kodiak City Council, City Manager, City Clerk, and other staff as appropriate. The City attorney is hired by and serves at the pleasure of the City Council. The City reserves the right to enter into contracts with attorneys other than the general counsel for specialized legal services should the City so choose. Proposals are invited from any qualified State of Alaska licensed attorney at law or law firm. Attorney/Firms (**proposers**) must be qualified to represent the City in all State of Alaska Courts and the U.S. District Court for the District of Alaska.

II. SCOPE OF SERVICES, TERM, AND QUALIFICATIONS

The City prefers to enter into an agreement under which a qualified proposer will provide general counsel legal services on behalf of the City on a per hour basis. This fee shall include all general counsel work and attendance at at least one Council meeting per year. The anticipated workload is approximately 20-30 hours per month. Other proposed agreements that address the scope of services outlined below will be considered.

- A. **SCOPE OF SERVICES.** The Proposer is expected to provide the City with general counsel legal services to include the following:
1. Serve as the legal advisor of and be responsible to the City Council, advise the City Manager and City Clerk concerning matters affecting the City, and perform other duties prescribed by the City Council.
 2. Prepare legal documents including, but not limited to ordinances, resolutions, contracts, collective bargaining agreements, conveyances, leases, easements, and legal opinions as needed.
 3. Be readily available for consultation by the City Council, City Manager, and City Clerk.
 4. Provide representation in court and manage the activities of other attorneys who may represent the City in Court.
 5. Assist the City Manager in negotiations on the City's behalf and handle other legal matters as they may arise.
 6. Draft opinion letters regarding, among other things, the interpretation of the City Code, state and federal laws, and policies.
 7. Perform other such duties as may be prescribed for the City Attorney by ordinance or by direction of the City Council and/or City Manager.
 8. Work effectively with the City Council, City Manager, City Clerk, and, when directed, with other public agencies and other entities with which the City has a legal relationship.
 9. Attend at least one meeting annually in person and others telephonically, or in person, as requested.
- B. **TERM.** The term of the professional services agreement shall be one year initially. A longer term may be negotiated by the parties after the initial year. The agreement may be terminated at the convenience of the City at any time with or without cause.
- C. **QUALIFICATIONS.**
1. The proposer (or primary/lead attorney for the City in a firm) must be a member in good standing of the State of Alaska Bar Association and be qualified to represent the City in all State of Alaska Courts and the U.S. District Court for the District of Alaska.
 2. The proposer (or primary/lead attorney) should have at least five years of professional experience as legal counsel for an entity such as an Alaska city, municipality, or borough.
 3. The proposer (or primary attorney) should be experienced and proficient in legal matters affecting the City, to include without limitation, Alaska municipal law including Title 29, the Kodiak Charter and Municipal Code, and other applicable State of Alaska and Federal laws.

III. RESPONSE TORFP

To achieve a uniform review process and obtain a reasonable degree of comparability, a proposer should submit a response to the request for proposals that is consistent with and follows the format presented below. The proposal should not exceed fifteen (15) pages in length, double spaced, including Page 2 of this RFP and the Letter of Transmittal. On Page 2 of this RFP, provide the name, address, phone number, fax number, e-mail address, together with the name of the person who the City Manager may contact regarding the proposal. Provide detail in the proposal for each of the following points:

- A. Letter of Transmittal (limited to one page)
 - 1. Briefly state the proposer's understanding of the services to be performed and make a positive commitment to provide the services as specified.
 - 2. State the long-term availability of the proposer to the City of Kodiak.
 - 3. Give the name(s) of the person(s) who are authorized to make representations for the proposer, their titles and telephone numbers.
 - 4. **The letter must be signed by an individual who has the authority to bind the proposer.**

- B. Statement of Qualifications
 - 1. Provide a statement of qualifications of the attorney who will be the primary or lead attorney providing general counsel and who will attend Council meetings.
 - 2. Provide a statement of qualifications of other attorneys, if any, who will support the primary attorney. These attorneys may be in the same firm or available under some other arrangement approved by the City Council.
 - 3. Provide a statement of qualifications of other attorneys (if any) in the firm who may have specialized experience and expertise that may be of value to the City.
 - 4. Describe the municipal experience, other applicable legal experience, and education of the attorneys listed in B1 through B3 above.
 - 5. List at least five municipal law cases handled by the primary attorney and/or supporting attorneys.
 - 6. Identify and describe the experience over the past five years and the level of expertise in the following areas:
 - a. Ordinance and resolution drafting
 - b. Ordinance enforcement
 - c. Public records law
 - d. Zoning and land use regulation
 - e. Public employee labor law
 - f. Municipal sales and property tax law
 - g. Municipal procurement and contract law

- h. Laws related to police, fire, emergency medical operations
 - i. Acquisition and disposition of public property, land management
 - j. Other municipal issues
- C. Conflicts. List all matters and/or cases where the proposer currently represents an individual or entity with interests potentially adverse to the City. Describe the scope of the representation and the nature of the conflict.
- D. Availability. Specify how available the primary attorney will be to the City. State whether the City will be the primary attorney's main client or one of several or many. State the current workload of the primary attorney and how that attorney intends to accommodate the City's legal work requirements. State the location of the office from which the primary attorney will serve the City.
- E. Disclosure. Disclose any alleged significant prior or ongoing contract failures, contract breaches, any civil or criminal litigation or Bar association investigation which involve the proposer as a party or in which the proposer has been judged guilty or liable or sanctioned. This is a mandatory disclosure.
- F. Cost. Proposals must state the following cost information:
 - 1. The hourly rate for the lead attorney, supporting attorneys, and other attorneys for hours worked.
 - 2. Travel costs
- G. Municipal Experience. List all municipalities for which the proposer has performed legal services in the past five years. Provide a short description of the services, the date performed, and the municipal contact person.
- H. References. Provide a minimum of five references from similar clients, to include local government, state, and/or private clients, for whom the proposer has performed legal services within the last three years. The City may contact any and all references for validation of information submitted and other information relative to the proposal.

IV. PROPOSAL EVALUATION AND AWARD PROCESS

- A. Proposals will be evaluated and scored by the Kodiak City Council on a 100/125 point scale based upon the following criteria.
- B.
 - Qualifications of primary attorney—35, Criteria III B, better qualifications—higher score

TASK	DATE/TIME
Deadline for submission of proposals	May 2, 2017 by 4 p.m.
Initial evaluation period	May 3-19, 2017
Selection / invitation for interviews	May 22, 2017
Selection of highest scoring attorney	May 30, 2017
Contract approval by Council	Next Available Meeting

NOTE: These dates represent a tentative schedule of events. The City reserves the right to modify these dates at any time, with appropriate notice to applicable proposers.

- B. In lieu of a pre-proposal conference, the City will accept questions and/or comments in writing, received either by mail or facsimile. Questions must reference the identifying RFP number and be addressed to City of Kodiak, City Clerk, 710 Mill Bay Road, Room 219, Kodiak, AK 99615 or faxed to 907 486-8633. The deadline for submitting questions is April 18, 2017, at 4 p.m. Alaska Standard Time. All questions will be addressed in writing on or about April 24, 2017. Responses to questions will be mailed, faxed, or emailed to prospective firms known by the City at the response deadline of April 18, 2017. The City shall not be responsible for questions submitted but not received by the deadline.
- C. Proposers shall submit one (1) original proposal marked "MASTER" and ten (10) identical copies to: Debra Marlar, City Clerk, City of Kodiak, 710 Mill Bay Road, Room 219, Kodiak Alaska, 99615.
- D. Proposals shall be clearly labeled in a sealed envelope or box as follows:

REQUEST FOR PROPOSALS No. 2017-001 Legal Counsel Services
- E. Proposals must be received at the above referenced address no later than 4 p.m. on May 2, 2017. Proposals that do not arrive by that time and date WILL NOT BE ACCEPTED. Proposers may submit their proposal at any time prior to the above stated deadline. Facsimile, e-mail, or telephone proposals will NOT be accepted. The City of Kodiak is not responsible for proposals received after May 2, 2017, at 4 p.m.
- F. **For ease of evaluation, the proposal should be presented in a format that corresponds to and references sections outlined in this RFP, and should be presented in the same order. Responses to each**

section and subsection should be labeled so as to indicate which term is being addressed.

- G. Proposals should be prepared in such a way as to provide a straightforward, concise delineation of capabilities to satisfy the requirements of this RFP. Expensive bindings, colored displays, promotional materials, etc. are not necessary or desired. Emphasis should concentrate on conformance to the RFP instructions, responsiveness to the RFP requirements, and on complete and clear content.
- H. The proposal must be signed by the individual legally authorized to bind the proposer.

VII. GENERAL TERMS, CONDITIONS AND EXCEPTIONS

- A. Performance of the proposer may be rated by the City Council semi-annually for the first year following contract award and then annually for the term of the contract, if mutually extended.
- B. The City reserves the right to alter, amend, or modify any provisions of this RFP, or to withdraw this RFP, at any time prior to the award of a contract, if it is in the best interest of the City to do so.
- C. The City reserves the right to waive informalities and minor irregularities in proposals received.
- D. The City reserves the right to reject any or all proposals received prior to contract award.
- E. The City shall not be obligated to accept the lowest priced proposal, but may make an award in the overall best interest of the City after all factors have been evaluated.
- F. Prices offered by proposers in their proposals are an irrevocable offer for the term of the contract. The awarded proposer agrees to provide the services at the costs, rates, and fees as set forth in the proposal.
- G. The City will not be liable for any costs incurred by a proposer to prepare its proposal. Costs to develop the proposals and any other such expenses incurred by the proposer in responding to the RFP are entirely the responsibility of the proposer and shall not be reimbursed in any manner by the City.
- H. All proposals submitted become the property of the City. The master copy of each proposal shall be retained for official files and will become public record after the award of the contract.

- I. The City will not be liable for federal, state, or local excise taxes.
- J. Proposer's response to the RFP shall constitute an agreement to all terms and conditions specified in the RFP, including, without limitation, except such terms and conditions that the proposer expressly excludes, which must be written in **bold, italicized and underscored font**. Exceptions will be taken into consideration as part of the evaluation process.
- K. The City reserves the right to negotiate final contract terms with the proposer selected. The RFP and the awarded proposer's proposal shall be incorporated therein.
- L. Proposer understands and acknowledges that the representations above are material and important, and will be relied on by the City in evaluation of the proposal. Any proposer misrepresentation may be treated as fraudulent concealment from the City of the true facts relating to the proposal.
- M. The City reserves the right to conduct a background inquiry of each proposer that may include collection of appropriate criminal history information, contractual and business associations and practices, Bar sanctions and reprimands, employment histories, and reputation in the legal and business communities. By submitting a proposal to the City, the proposer consents to such an inquiry, prior to a contract offer.
- N. The City is an equal opportunity employer and complies with Title I of the American with Disabilities Act. Proposers who need accommodation to respond to the RFP may call the City Clerk at 907 486-8636.

END OF RFP