

City of Kodiak Regular Council Meeting Agenda for April 27, 2017
7:30 p.m., at 710 Mill Bay Road, Assembly Chambers (Room 232)

- I. Call to Order/Roll Call**
Invocation/Pledge of Allegiance

- II. Previous Minutes**
Approval of Minutes of the April 13, 2017, Regular Council Meeting.....1

- III. Persons to Be Heard**
 - a. Presentation of GFOA Award to the Finance Department8
 - b. Public Comments (limited to 3 minutes) (486-3231)

- IV. Unfinished Business**
 - a. None

- V. New Business**
 - a. First Reading, Ordinance No. 1362, Establishing Supplemental Appropriation No. 1 to the Budget for the Fiscal Year Commencing on the First Day of July 2016 and Ending on the Thirtieth Day of June 201712
 - b. Resolution No. 2017–11, Authorizing the Transfer of \$21,000 From the FY2017 General Fund Balance to the General Capital Project Fund – Kodiak Fire Station Deferred Maintenance Project No. 4015.....22
 - c. Resolution No. 2017–12, Supporting Changes to Timing and Location of the U.S. Department of Defense “Northern Edge 19” Training Exercise, and Subsequent Operations, in the Gulf of Alaska28
 - d. Authorization of Pedestrian Pathway Plan34
 - e. Port and Harbors Advisory Board Appointment92

- VI. Staff Reports**
 - a. City Manager
 - b. City Clerk

- VII. Mayor’s Comments**

- VIII. Council Comments**

- IX. Audience Comments (limited to 3 minutes) (486-3231)**

- X. Adjournment**

(This page left intentionally blank.)

<p>DRAFT</p>

**MINUTES OF THE REGULAR COUNCIL MEETING
OF THE CITY OF KODIAK
HELD THURSDAY, APRIL 13, 2017
IN THE BOROUGH ASSEMBLY CHAMBERS**

I. MEETING CALLED TO ORDER/INVOCATION/PLEDGE OF ALLEGIANCE

Deputy Mayor Randall C. Bishop called the meeting to order at 7:30 p.m. Councilmembers Laura B. Arboleda, Gabriel T. Saravia, Richard H. Walker, and John B. Whiddon were present and constituted a quorum. Mayor Pat Branson and Councilmember Charles E. Davidson were absent. City Manager Aimée Kniazowski and Deputy Clerk Michelle Shuravloff-Nelson were also present.

Salvation Army Major Dave Davis gave the invocation and the Pledge of Allegiance was recited.

II. PREVIOUS MINUTES

Councilmember Whiddon MOVED to approve the minutes of the March 23, 2017, regular meeting as presented.

The roll call vote was Councilmembers Arboleda, Bishop, Saravia, Walker, and Whiddon in favor. Councilmember Davidson was absent. The motion passed.

III. PERSONS TO BE HEARD

a. Proclamation: Month of the Military Child

Councilmember Whiddon read the proclamation, which encourages all citizens to show appreciation and understanding to the military children in our communities, and recognize their commitment, courage, sacrifice, and continued resilience.

HS2 Bonnie Baker and her daughter Abigail accepted the proclamation with Dayton Will of the National Guard. Velma Vining, Child Development Resource Specialist for the Coast Guard acknowledged the dedication and services provided by the military families and their children.

b. Proclamation: Week of the Young Child

Councilmember Arboleda read the proclamation, which encourages all citizens to work to make a good investment in early childhood education.

Ms. Vining and Rebecca BJORKE, Occupational Therapist for the Infant Learning Program accepted the proclamation. Ms. BJORKE spoke about the importance of investing in your children and the positive outcomes it has on a community. Ms. Vining announced the Children's Fair and Parent Café on April 22, 2017, from 10 a.m. until 2 p.m. at the Kodiak High School.

c. Public Comments

Katie Baxter, Library Director announced “National Library Week” and shared the theme is “Libraries Transform”. She updated the public on local activities in the community and some of the new services performed by the City’s library staff. She enthusiastically shared that there are over 10,000 library users at this time.

IV. UNFINISHED BUSINESS

- a. None

V. NEW BUSINESS

a. **Resolution No. 2017–08, Authorizing the Issuance of a Permit to the Kodiak Chamber of Commerce for Use of Public Property in Conjunction With the Kodiak Crab Festival**

Deputy Mayor Bishop read Resolution No. 2017–08 by title. The Kodiak Chamber of Commerce is the local organization that manages the annual Kodiak Crab Festival, one of Kodiak’s premiere events. It promotes Kodiak, gives a boost to the local economy, and provides fundraising opportunities for many local organizations and non-profits. This year the festival will run from May 24 through May 30, 2017. As in past years, Council authorizes a permit for the activities associated with Kodiak Crab Festival by adoption of a resolution.

Councilmember Walker MOVED to adopt Resolution No. 2017–08.

The roll call vote was Councilmembers Arboleda, Bishop, Saravia, Walker, and Whiddon in favor. Councilmember Davidson was absent. The motion passed.

b. **Resolution No. 2017–09, Authorizing the Issuance of a Permit to Hope Community Resources for the Use of Public Property for Its Walk and Roll Community Walk Fundraiser**

Deputy Mayor Bishop read Resolution No. 2017–09 by title. Hope Community Resources (Hope) is a non-profit that is funded by grants and community donations. Hope wants to offer its annual walk and roll community walking fundraiser on May 6, 2017, at the Baranof Park track. Hope will set up a tent to offer snacks and drinks to participants and will accept donations.

Councilmember Arboleda MOVED to adopt Resolution No. 2017–09.

The roll call vote was Councilmembers Arboleda, Bishop, Saravia, Walker, and Whiddon in favor. Councilmember Davidson was absent. The motion passed.

c. **Resolution No. 2017–10, Authorizing the Issuance of a Permit to Kodiak High School Softball Team for the Use of Public Property to Sell Concessions During Home Games**

Deputy Mayor Bishop read Resolution No. 2017–10 by title. The high school softball coach, Michelle Stratton, requested to sell concessions as a fundraiser for the high school softball program from April 13 - July 31. The concessions will be sold during home games. The softball team is a high school sport that requires a significant amount of self funding. The team raises money to cover the cost of travel and other costs associated with the sport.

Councilmember Arboleda MOVED to adopt Resolution No. 2017–10.

The roll call vote was Councilmembers Arboleda, Bishop, Saravia, Walker, and Whiddon in favor. Councilmember Davidson was absent. The motion passed.

d. Authorization of Amendment No. 2 to the Fisheries Analyst Contract

The City Council and Borough Assembly have worked with a fisheries analyst since 2011 to help track and respond to fisheries resource management issues important to the Kodiak area. Following the resignation of the first analyst in late 2013, the City and Borough selected and hired McCarty and Associates for a two year contract with an option to renew. The majority of the two governing bodies agreed to a one-year extension of the contract with some modifications at the joint work session on February 24, 2016. Staff worked with the contractor who agreed to Amendment No. 1 to the contract ending on February 28, 2017. The City and Borough would like to extend this agreement at the recommendation of the Kodiak Fisheries Work Group for an additional year, ending on February 28, 2018. Amendment No. 2 has changes that reflect a reduction in contract fee by \$5000 and clearer direction on points of contact, reporting, and meeting attendance. The Borough Assembly adopted contract Amendment No. 2 on March 23, 2017.

Councilmember Whiddon MOVED to authorize Amendment No. 2 to the Fisheries Analyst Contract with McCarty and Associates effective from March 1, 2017, through February 28, 2018, in the amount of \$27,500 per year plus authorized expenses for the City's share of the contract costs, with funds coming from the General Fund Legislative professional services account and authorize the City Manager to execute the documents on behalf of the City.

The roll call vote was Councilmembers Arboleda, Bishop, Saravia, Walker, and Whiddon in favor. Councilmember Davidson was absent. The motion passed.

e. Authorization of Amendment to Lease 222854 With the Kodiak Maritime Museum and Art Center For the Thelma C

The Kodiak Maritime Museum (KMM) developed the idea of restoring and displaying a historic 36' fishing vessel, the Thelma C in the harbor area downtown. KMM representatives came to several City Council meetings since 2013 with their idea, which Council and harbor users supported. The KMM was given formal approval of the original lease site on the harbor spit in January 2016 by Council. KMM requested the location be adjusted to make the installation more affordable and this action was approved in August 2016 by Council. KMM has requested the footprint in the lease that was approved by Ordinance No. 1356 and Lease No. 222854 in August 2016 be modified to reflect the increased location by 720 square feet due to changes in the design and funding constraints. Lease No. 222854 would require an amendment with a substitution of Exhibit A and Exhibit B; all other terms of Lease No. 222854 would remain the same.

Councilmember Walker MOVED to authorize an amendment to Lease No. 222854 by substituting Exhibit A and Exhibit B dated March 23, 2017, for Exhibit A and Exhibit B as authorized by Ordinance No. 1356 and Lease No. 222854 on August 11, 2016.

The roll call vote was Councilmembers Arboleda, Bishop, Saravia, Walker, and Whiddon in favor. Councilmember Davidson was absent. The motion passed.

f. Approval of Second Amendment to Near Island Communications Site Sublease Between Kodiak Island Broadcasting, Inc. and New Cingular Wireless PCS, LLC

Kodiak Island Broadcasting Co., Inc. is requesting an amendment to the sublease held with New Cingular Wireless PCS, LLC. Any communication sublease must be approved by Council per City Code Title 18, Section 18.20.230. This amendment, if approved, will be the second amendment to the sublease between Kodiak Island Broadcasting Co., Inc. and New Cingular Wireless PCS, LLC.

Councilmember Whiddon MOVED to approve the second amendment between Kodiak Island Broadcasting, Inc. and New Cingular Wireless PCS, LLC for Near Island Communications Site Lease 221887.

The roll call vote was Councilmembers Arboleda, Bishop, Saravia, Walker, and Whiddon in favor. Councilmember Davidson was absent. The motion passed.

VI. STAFF REPORTS

a. City Manager

Manager Kniazowski spoke about the budget process, addressing the budget deficit, and shared the budget presentation is scheduled for May 6, 2017. She said her office will meet with the KIB's Manager regarding consolidation next week and discuss the formation of the new committee. She informed the public that Pillar Mountain is closed for safety reasons and commented that the road may need to be gated in the future. She said Fire Chief Mullican reported there were five cars that have went off Pillar Mountain road this winter. She said the community pool will be closed for the required maintenance from May 1 through May 21. She said the Parks and Recreation department will be hosting an Easter egg hunt for children on April 16 at Baranof Park. She said she recently traveled to Juneau for an annual lobbying trip and stated she and Councilmember Walker testified at the House Finance Committee in support of House Bill 115. She spoke to Lobbyist Ray Gillespie and commented the 90-day session will likely exceed the timeframe. She said Finance Director Mayes issued the 2nd quarter financial report to Council and commented that revenues are down by 13 percent and the expenses are well within budget for the departments. She said there will be a supplemental budget appropriation amendment scheduled for a Council meeting in the near future.

b. City Clerk

Deputy Clerk Shuravloff-Nelson informed the public of the next scheduled Council work sessions and regular meeting.

VII. MAYOR'S COMMENTS

Deputy Mayor Bishop said the Marijuana Advisory Committee had their final meeting on April 4, 2017, to finalize their recommendations to the Council. He thanked co-chair Councilmember Arboleda and the committee membership for their time and work. He said the Council work session to discuss the committee's recommendations is scheduled for Tuesday, June 23, 2017, at 7:30 p.m. He said there was a recent Fisheries Work Group meeting with great public participation for salmon fisheries discussion. He thanked Councilmembers Walker and Saravia for volunteering to be on the attorney selection committee. He wished everyone a happy Easter.

VIII. COUNCIL COMMENTS

Councilmember Walker said the trip to Juneau was very interesting and there are many hard decisions to be made during this budgetary time. He said to fix the budgetary issues will involve everyone to give up some things on a statewide level to move Alaska forward. He said he appreciated Governor Walker taking the time to meet with them and stated there are many good people working for Alaska. He wished everyone a happy Easter.

Councilmember Saravia thanked Manager Kniaziowski for her years of service and he indicated she improved the quality of life for the people in Kodiak. He wished everyone a happy Easter and said to enjoy their family.

Councilmember Arboleda said it is a beautiful day in Kodiak and people are getting involved in outdoor activities; she said to enjoy and stay safe. She said she was recently a judge at a youth speech contest and commented that youth activities are very important within a community.

Councilmember Whiddon said he went to the same event as Councilmember Arboleda and commented the youth were very impressive. He thanked Finance Director Mayes for the financial report and he appreciated the extra work that went into preparing the report. He said there was a Fisheries Work Group meeting and the Kodiak Salmon Work group will be presenting an upper Cook Inlet genetic study on salmon next week, which can impact Kodiak. He encouraged citizens to attend the meeting. He said he was recently in Juneau for the Alaska Marine Highway Strategic Planning Committee meeting, and shared they are reviewing different governance models to meet the needs of all Alaskans. He wished everyone a happy Easter.

IX. AUDIENCE COMMENTS

Chris Lynch thanked the Council for approving the amendment to the Thelma C lease. She said as a downtown business owner she has a concern regarding the vagrant population in the downtown area and she requested an increase in foot patrol by KPD. She said she can't contribute to the economy of the community when there are impediments to her customers entering her business.

X. ADJOURNMENT

Councilmember Whiddon MOVED to adjourn the meeting.

The roll call vote was Councilmembers Arboleda, Bishop, Saravia, Walker, and Whiddon in favor. Councilmember Davidson was absent. The motion passed.

The meeting adjourned at 8:20 p.m.

CITY OF KODIAK

MAYOR




ATTEST:

CITY CLERK

Minutes Approved:

PERSONS TO BE HEARD

MEMORANDUM TO COUNCIL

To: Mayor Branson and City Councilmembers
From: Aimée Kniaziowski, City Manager 
Thru: Kelly Mayes, Finance Director  and Mike Tvenge, Deputy City Manager 
Date: April 27, 2017

Agenda Item: III. a. Presentation of GFOA Award to the Finance Department

SUMMARY: The Government Finance Officers Association of the United States and Canada has awarded Finance Director Kelly Mayes and the Finance Department with the Distinguished Budget Presentation Award for the City's Fiscal Year 2016 Operating Budget. This award is the highest form of recognition in governmental accounting and financial reporting.

ATTACHMENTS:

Attachment A: GFOA Distinguished Budget Presentation Award



GOVERNMENT FINANCE OFFICERS ASSOCIATION

*Distinguished
Budget Presentation
Award*

PRESENTED TO

City of Kodiak

Alaska

For the Fiscal Year Beginning

July 1, 2016

Jeffrey R. Emery

Executive Director

(This page left intentionally blank.)

NEW BUSINESS

MEMORANDUM TO COUNCIL

To: Mayor Branson and City Council Members

From: Aimée Kniazowski, City Manager *AK*

Thru: Kelly Mayes, Finance Director *KM* and Mike Tvenge, Deputy City Manager *MT*

Date: April 27, 2017

Agenda Item: V. a. **First Reading Ordinance No. 1362, Establishing Supplemental Appropriation No. 1 to the Budget for the Fiscal Year Commencing on the First Day of July 2016 and Ending On the Thirtieth Day of June 2017**

SUMMARY: The Supplemental Appropriation No. 1 to the budget for the fiscal year commencing on the first day of July 2016 and ending on the thirtieth day of June 2017 is in the amount of \$4,922,454. It is customary for the City Council to approve at least one supplemental budget annually to authorize the adjustments of current revenues and expenses as detailed in the attachments provided. These adjustments are for the operating funds as well as additions to project funds for grant revenues received and additional expenditures needed that were not known at the time the original budget. This is the first supplemental appropriation of FY2017. Staff recommends Council pass Ordinance No. 1362 in the first reading and advance to second reading and public hearing at the next regular or special Council meeting.

PREVIOUS COUNCIL ACTION:

- June 9, 2016, Council adopted Ordinance No. 1351 for the FY2017 budget in the amount of \$43,415,113 commencing on the first day of July 2016 and ending on the thirtieth day of June 2017.
- July 14, 2016, Council adopted Resolution No. 2016-16 to transfer \$165,000 from the Cargo Fund to the Cargo Capital Projects for the Pier III Construction.
- August 11, 2016, Council authorized the purchase of Skate ramps for Parks and Recreation Improvement Project No. 9013. This authorization included an additional appropriate from the Parks and Recreation Improvement fund balance in the amount of \$20,000.
- August 11, 2016, Council adopted Resolution No. 2016-28 to transfer \$650,000 from the Cargo Fund to the Cargo Capital Projects for the Pier III Construction.
- October 13, 2016, Council adopted Resolution No. 2016-36 to transfer \$20,010 from the General Fund to the General Capital Project No. 4047 for the additional funding required for the ambulance purchase under the Code Blue grant.
- October 13, 2016, Council adopted Resolution No. 2016-39 to appropriate \$30,550 from the Water Improvement fund balance to Project No. 7023, UV Water Treatment Facility to program and upgrade the UV reactors as part of the DEC certificate to operate.

APRIL 27, 2017

Agenda Item V. a. Memo Page 1 of 3

- February 28, 2017, Council adopted Resolution No. 2017-04 to reallocate \$51,250 between general fund departments for additional salaries and benefits and office rent for the HR Manager position.
- April 25, 2017, Council reviewed Ordinance 1362 and attachments at the work session in preparation for first reading at this regular meeting.

DISCUSSION: The adoption of the budget by the City Council puts the budget into effect for the budget year July 1 through June 30. Amendments to the budget can occur anytime during the fiscal year through a supplemental budget ordinance, which is introduced at one Council meeting and typically adopted at the next Council meeting.

All new appropriations are authorized by an ordinance that amends the annual budget ordinance. An ordinance is required to move amounts between funds, departments and projects. An ordinance is required to move funds, to add permanent personnel, or to grant unscheduled salary increases. The Supplemental No. 1 Appropriation is requesting the addition of \$4,922,454 to the adopted budget bringing the total amended budget to \$50,038,127 for FY2017.

The Supplemental No. 1 Appropriation includes adjustments related to health insurance discounts received from the City's health insurance provider, additional funding requests for citywide legal services and citywide telephone and internet services, reinstatement of a Cargo capital project that was closed in error in prior fiscal years, adjustments for federal and state grant awards accepted previously by Council, and adjustments for the transfers of funds for water and sewer loan proceeds received for the Monashka Pump House and the Compost facility.

FINANCIAL IMPLICATIONS: All expense appropriations requested in Supplemental Appropriation No. 1 are funded by increased revenue sources, additional state and federal grant sources, and use of fund balances and transfers. The ordinance makes a number of changes in the FY2017 budget reflecting increased revenues and expenditures, new project and grant funding, and movement of funds to more accurately reflect current City operations. Details of funding sources and expenses have been submitted in the attachments.

LEGAL: The Kodiak City Charter and Kodiak City Code grant Council the authority to make appropriations and adopt and amend budgets as required.

STAFF RECOMMENDATION: Staff recommends Council pass Ordinance No. 1362 in the first reading and advance to second reading and public hearing at the next regular or special Council meeting.

CITY MANAGER'S COMMENTS: The City Council traditionally adopts at least one major budget amendments each fiscal year to make necessary adjustments to cover changes or additions to projects, to account for the receipt of additional revenues, and increased operating expenses. The ordinance and attachments detail the sections of the City's operating and capital budget that require changes. I

recommend Council pass Ordinance No. 1362 in the first reading and advance to second reading and public hearing at the next regular or special Council meeting

ATTACHMENTS:

Attachment A: Ordinance No. 1362

Attachment B: Detail of supplemental budget No. 1

PROPOSED MOTION:

Move to pass Ordinance No. 1362 in the first reading and advance to second reading and public hearing at the next regular or special Council meeting.

**CITY OF KODIAK
ORDINANCE NUMBER 1362**

**AN ORDINANCE OF THE COUNCIL OF THE CITY OF KODIAK ESTABLISHING
SUPPLEMENTAL APPROPRIATION NO. 1 TO THE BUDGET FOR THE FISCAL YEAR
COMMENCING ON THE FIRST DAY OF JULY 2016 AND ENDING ON THE THIRTIETH
DAY OF JUNE 2017**

BE IT ORDAINED by the Council of the City of Kodiak, Alaska, as follows:

Section 1: The following estimated revenues and expenditures are hereby appropriated for the corporate purposes and objects of the City of Kodiak for fiscal year 2017.

GENERAL FUND				
	Adopted Budget	FY 2017 Council Action	Supplemental #1	Amended Budget
Budgeted Revenues				
Property Tax	\$ 953,500	\$ -	\$ -	\$ 953,500
Sales Tax	12,327,500		-	12,327,500
Licenses and Permits	52,300		-	52,300
Intergovernmental	2,813,431		15,000	2,828,431
Charges for Services	1,661,217		-	1,661,217
Fines and Forfeitures	15,000		-	15,000
Interest Income	10,000		-	10,000
Rental Income	230,000		-	230,000
Other Revenues	22,000		169,140	191,140
Interfund Charges	1,565,978		-	1,565,978
Appropriation (to) from Fund Balance	(72,702)	20,010	(54,640)	(107,332)
Transfers In	-			-
Total Budgeted Revenues	\$ 19,578,224	\$ 20,010	\$ 129,500	\$ 19,727,734
Budgeted Expenditures				
Legislative	\$ 288,404	\$ -	\$ -	\$ 288,404
Legal	100,000		100,000	200,000
Executive	704,353	51,250	-	755,603
Emergency Preparedness	38,000		-	38,000
City Clerk	540,296		-	540,296
Finance	1,421,001	(16,683)	29,500	1,433,818
Police	7,139,995	(21,585)	-	7,118,410
Fire	2,410,371		-	2,410,371
Public Works	2,228,043	(20,382)	-	2,207,661
Engineering	280,451		-	280,451
Parks & Recreation	1,513,707		-	1,513,707
Library	988,352		-	988,352
Non-Departmental	721,657	7,400	-	729,057
Transfers	1,203,594	20,010	-	1,223,604
Total Budgeted Expenditures	\$ 19,578,224	\$ 20,010	\$ 129,500	\$ 19,727,734

SPECIAL REVENUE FUNDS

	Adopted Budget	FY 2017 Council Action	Supplemental #1	Amended Budget
Budgeted Revenues				
Tourism Development	\$ 189,500	\$ -	\$ -	\$ 189,500
City Enhancement	-	-	-	-
Total Budgeted Revenues	<u>\$ 189,500</u>	<u>\$ -</u>	<u>\$ -</u>	<u>\$ 189,500</u>
Budgeted Expenditures				
Tourism Development	\$ 189,500	\$ -	\$ -	\$ 189,500
City Enhancement	-	-	-	-
Total Budgeted Expenditures	<u>\$ 189,500</u>	<u>\$ -</u>	<u>\$ -</u>	<u>\$ 189,500</u>

CAPITAL PROJECTS

	Adopted Budget	FY 2017 Council Action	Supplemental #1	Amended Budget
Budgeted Revenues				
300 General Capital Projects	83,790	20,010	50,222	154,022
315 Vehicle Replacement Capital	203,594		-	203,594
301 Street Improvements	1,500,000		-	1,500,000
302 Building Improvements	-		-	-
305 Water Capital Fund	4,469,000	30,550	-	4,499,550
306 Sewer Capital Fund	1,515,000		-	1,515,000
307 Cargo Development Fund	-	815,000	2,400,000	3,215,000
308 Harbor Development	250,000		-	250,000
309 Parks & Recreation Fund	134,000	20,000	-	154,000
Total Budgeted Revenues	<u>8,155,384</u>	<u>885,560</u>	<u>2,450,222</u>	<u>11,491,166</u>
Budgeted Expenditures				
300 General Capital Projects	83,790	20,010	50,222	134,012
315 Vehicle Replacement Capital	203,594		-	203,594
301 Street Improvements	1,500,000		-	1,500,000
302 Building Improvements	-		-	-
305 Water Capital Fund	4,469,000	30,550	-	4,499,550
306 Sewer Capital Fund	1,515,000		-	1,515,000
307 Cargo Development Fund	-	815,000	2,400,000	3,215,000
308 Harbor Development	250,000		-	250,000
309 Parks & Recreation Fund	134,000	20,000	-	154,000
Total Budgeted Expenditures	<u>8,155,384</u>	<u>885,560</u>	<u>2,450,222</u>	<u>11,471,156</u>

ENTERPRISE FUNDS

	Adopted Budget	FY 2017 Council Action	Supplemental #1	Amended Budget
Budgeted Revenues				
Cargo	\$ 1,066,194	\$ 815,000	\$ -	\$ 1,881,194
Harbor	3,791,619		-	3,791,619
Shipyards	1,283,398		-	1,283,398
Electric Utility	637,884		-	637,884
Water Utility	3,365,988		548,128	3,914,116
Sewer Utility	4,483,078		1,794,604	6,277,682
Trident Basin	301,293		-	301,293
E-911 Services	13,611		-	13,611
Total Budgeted Revenues	\$ 14,943,065	\$ 815,000	\$ 2,342,732	\$ 18,100,797
Budgeted Expenditures				
Cargo	\$ 1,066,194	\$ 815,000	\$ -	\$ 1,881,194
Harbor	3,791,619		-	3,791,619
Shipyards	1,283,398		-	1,283,398
Electric Utility	637,884		-	637,884
Water Utility	3,365,988		548,128	3,914,116
Sewer Utility	4,483,078		1,794,604	6,277,682
Trident Basin	301,293		-	301,293
E-911 Services	13,611		-	13,611
Total Budgeted Expenditures	\$ 14,943,065	\$ 815,000	\$ 2,342,732	\$ 18,100,797

INTERNAL SERVICE FUND

	Adopted Budget	FY 2017 Council Action	Supplemental #1	Amended Budget
Budgeted Revenues				
Self Insurance Fund	\$ 548,940	\$ -	\$ -	\$ 548,940
Total Budgeted Revenues	\$ 548,940	\$ -	\$ -	\$ 548,940
Budgeted Expenditures				
Self Insurance Fund	\$ 548,940		\$ -	\$ 548,940
Total Budgeted Expenditures	\$ 548,940	\$ -	\$ -	\$ 548,940
Total Revenues	\$ 43,415,113	\$ 1,720,570	\$ 4,922,454	\$ 50,058,137
Total Expenditures	\$ 43,415,113	\$ 1,720,570	\$ 4,922,454	\$ 50,038,127

Section 2: This ordinance shall be in full force and effect from and after its passage as required by law.

CITY OF KODIAK

MAYOR

ATTEST:

CITY CLERK

First Reading:
Second Reading:
Effective Date:


FY 2017 Supplemental Budget # 1 - Line Item Detail

Fund	GL #	Description	Debit	Credit	Purpose
General Fund	100-001 330.325	Federal Grt - Operating		(15,000.00)	Grant award previously accepted by Council
General Fund	100-001 375.605	Discounts Received		(169,140.00)	Health Insurance discounts received from Aetna
General Fund	100-001 385.100	Appropriation - Fund Bal	54,640.00		Offset for General Fund - will increase fund balance
General Fund	100-100-106 430.110	Professional Services	100,000.00		Increased legal fees due to various litigation and State Hearings regarding unionizator
General Fund	100-110-100 430.110	Professional Services		(5,000.00)	Executive - Administration department - adjust budget line items
General Fund	100-110-100 450.510	Repair & Maintenance	2,400.00		Executive - Administration department - adjust budget line items
General Fund	100-110-100 470.125	Mach&Equip Less \$5000	2,600.00		Executive - Administration department - adjust budget line items
General Fund	100-130-135 450.115	Telephone	29,500.00		Budget increase due to Citywide increased telephone & internet charges
General Fund	100-140-100 470.125	Mach&Equip Less \$5000	41,250.00		KPD - Body Cameras and Batteries - adjust budget line items
General Fund	100-140-146 410.100	Salaries & Wages		(15,000.00)	KPD - Body Cameras and Batteries - adjust budget line items
General Fund	100-140-146 410.140	Overtime		(2,250.00)	KPD - Body Cameras and Batteries - adjust budget line items
General Fund	100-140-146 420.130	Retirement Contributions		(4,000.00)	KPD - Body Cameras and Batteries - adjust budget line items
General Fund	100-140-148 420.110	Health Insurance		(15,000.00)	KPD - Body Cameras and Batteries - adjust budget line items
General Fund	100-140-148 420.130	Retirement Contributions		(5,000.00)	KPD - Body Cameras and Batteries - adjust budget line items
General Fund	100-160-162 450.510	Repair & Maintenance		(16,000.00)	KPD - Body Cameras and Batteries - adjust budget line items
General Fund	100-160-163 450.530	Repair & Maintenance-Bldg	16,000.00		PW - Garage doors - adjust budget line items
General Fund	100-170-100 450.310	Supplies		(8,000.00)	Parks & Rec - vehicle fuel - adjust budget line items for new account
General Fund	100-170-100 450.330	Vehicle Fuel	8,000.00		Parks & Rec - vehicle fuel - adjust budget line items for new account
General Fund	100-170-173 450.130	Dues & Subscriptions	75.00		Parks & Rec - AFLAC admin fees - adjust budget line items for new account
General Fund	100-170-173 450.310	Supplies		(75.00)	Parks & Rec - AFLAC admin fees - adjust budget line items for new account
		Total General Fund	254,465.00	(254,465.00)	
General Capital Projects	300-300-402 430.110	Professional Services		(35,000.00)	City Land Development - Adjust line items to appropriate accounts
General Capital Projects	300-300-402 470.710	Eng & Inspec - Non Caplit	35,000.00		City Land Development - Adjust line items to appropriate accounts
General Capital Projects	300-300-413 410.100	Salaries & Wages		(663.14)	Museum Phase I - Adjust line items to appropriate accounts
General Capital Projects	300-300-413 470.710	Eng & Inspec - Non Caplit		(1,023.59)	Museum Phase I - Adjust line items to appropriate accounts
General Capital Projects	300-300-413 470.725	Construction - Non Caplit	1,686.73		Museum Phase I - Adjust line items to appropriate accounts
General Capital Projects	300-300-415 470.116	Bldg Improv > \$5000		(15,000.00)	KFD Deferred Maintenance - Adjust line items to appropriate accounts
General Capital Projects	300-300-415 470.126	Mach&Equip Greater\$5000	15,000.00		KFD Deferred Maintenance - Adjust line items to appropriate accounts
General Capital Projects	300-300-429 430.110	Professional Services		(1,000.00)	E-911 Upgrade - Adjust line items to appropriate accounts
General Capital Projects	300-300-429 450.135	Training & Travel	1,000.00		E-911 Upgrade - Adjust line items to appropriate accounts
General Capital Projects	300-300-439 470.126	Mach&Equip Greater\$5000	55,000.00		Downtown Revitalization - Adjust line items to appropriate accounts
General Capital Projects	300-300-439 470.725	Construction - Non Caplit		(55,000.00)	Downtown Revitalization - Adjust line items to appropriate accounts
General Capital Projects	300-300-444 410.100	Salaries & Wages	200.00		Storm Damage - Adjust line items to appropriate accounts
General Capital Projects	300-300-444 450.510	Repair & Maintenance		(200.00)	Storm Damage - Adjust line items to appropriate accounts
General Capital Projects	300-300-448 470.125	Mach&Equip Less \$5000	36,862.96		To amend budget for Federal Equipment Grant award accepted by Council
General Capital Projects	300-300-449 470.125	Mach&Equip Less \$5000	13,359.00		To amend budget for Grant award accepted by Council
General Capital Projects	300-300-001-330.320	Federal Grant		(36,862.96)	To amend budget for Federal Equipment Grant award accepted by Council
General Capital Projects	300-300-001-330.300	State Grant		(13,359.00)	To amend budget for Grant award accepted by Council
		Total General Capital Projects	158,108.69	(158,108.69)	
Street Capital Projects	301-320-530 410.100	Salaries & Wages	3,500.00		Snow Dump - Adjust line items to appropriate accounts
Street Capital Projects	301-320-530 430.110	Professional Services	500.00		Snow Dump - Adjust line items to appropriate accounts
Street Capital Projects	301-320-530 470.710	Eng & Inspec - Non Caplit		(264,000.00)	Snow Dump - Adjust line items to appropriate accounts
Street Capital Projects	301-320-530 470.725	Construction - Non Caplit	210,000.00		Snow Dump - Adjust line items to appropriate accounts
Street Capital Projects	301-320-533 470.710	Eng & Inspec - Non Caplit	50,000.00		Snow Dump - Adjust line items to appropriate accounts
Street Capital Projects	301-320-535 410.100	Salaries & Wages	1,300.00		SPCC - Adjust line items to appropriate accounts
Street Capital Projects	301-320-535 430.110	Professional Services		(1,300.00)	SPCC - Adjust line items to appropriate accounts
Street Capital Projects	301-320-536 470.710	Eng & Inspec - Non Caplit	62,000.00		Mill Bay Road Rebuild - Adjust line items to appropriate accounts
Street Capital Projects	301-320-536 470.725	Construction - Non Caplit		(62,000.00)	Mill Bay Road Rebuild - Adjust line items to appropriate accounts
		Total Streets Capital Projects	327,300.00	(327,300.00)	
Building Capital Projects	302-330-612 410.100	Salaries & Wages	50,000.00		New Library Building - Adjust line items to appropriate accounts
Building Capital Projects	302-330-612 430.110	Professional Services	200,000.00		New Library Building - Adjust line items to appropriate accounts
Building Capital Projects	302-330-612 470.126	Mach&Equip Greater\$5000	425,000.00		New Library Building - Adjust line items to appropriate accounts
Building Capital Projects	302-330-612 470.726	Construction - Capitalize		(675,000.00)	New Library Building - Adjust line items to appropriate accounts
Building Capital Projects	302-330-615 410.100	Salaries & Wages	20,000.00		New Fire Station - Adjust line items to appropriate accounts
Building Capital Projects	302-330-615 430.110	Professional Services		(770,000.00)	New Fire Station - Adjust line items to appropriate accounts
Building Capital Projects	302-330-615 470.726	Construction - Capitalize	750,000.00		New Fire Station - Adjust line items to appropriate accounts
		Total Building Capital Projects	1,445,000.00	(1,445,000.00)	

Water Capital Projects	305-340-724 410.100	Salaries & Wages	1,300.00	Utility Rate Study - Adjust line items to appropriate accounts	
Water Capital Projects	305-340-724 430.110	Professional Services		Utility Rate Study - Adjust line items to appropriate accounts	(1,300.00)
Water Capital Projects	305-340-729 410.100	Salaries & Wages	30,000.00	Monashka Pump House - Adjust line items to appropriate accounts	
Water Capital Projects	305-340-729 470.710	Eng & Inspec - Non Capiti	1,500,000.00	Monashka Pump House - Adjust line items to appropriate accounts	
Water Capital Projects	305-340-729 470.725	Construction - Non Capiti		Monashka Pump House - Adjust line items to appropriate accounts	(1,530,000.00)
Water Capital Projects	305-340-737 410.100	Salaries & Wages	4,000.00	Alutian Homes Phase VI - Adjust line items to appropriate accounts	
Water Capital Projects	305-340-737 430.110	Professional Services	40,000.00	Alutian Homes Phase VI - Adjust line items to appropriate accounts	
Water Capital Projects	305-340-737 470.726	Construction - Capitalize		Alutian Homes Phase VI - Adjust line items to appropriate accounts	(44,000.00)
Water Capital Projects	305-340-738 410.100	Salaries & Wages	1,000.00	Monashka Transmission Line - Adjust line items to appropriate accounts	
Water Capital Projects	305-340-738 470.710	Eng & Inspec - Non Capiti	2,000.00	Monashka Transmission Line - Adjust line items to appropriate accounts	(1,000.00)
Water Capital Projects	305-340-739 410.100	Salaries & Wages		Dam Safety Inspections - Adjust line items to appropriate accounts	(2,000.00)
Water Capital Projects	305-340-739 470.710	Eng & Inspec - Non Capiti		Dam Safety Inspections - Adjust line items to appropriate accounts	
Total Water Capital Projects			1,578,300.00		(1,578,300.00)
Sewer Capital Projects	306-350-759 410.100	Salaries & Wages	15,000.00	Upgrade Lift Stations - Adjust line items to appropriate accounts	
Sewer Capital Projects	306-350-759 430.110	Professional Services	60,000.00	Upgrade Lift Stations - Adjust line items to appropriate accounts	
Sewer Capital Projects	306-350-759 470.710	Eng & Inspec - Non Capiti	425,000.00	Upgrade Lift Stations - Adjust line items to appropriate accounts	
Sewer Capital Projects	306-350-759 470.725	Construction - Non Capiti		Upgrade Lift Stations - Adjust line items to appropriate accounts	(600,000.00)
Sewer Capital Projects	306-350-768 410.100	Salaries & Wages	7,500.00	Bio-Solid Management Project - Adjust line items to appropriate accounts	
Sewer Capital Projects	306-350-768 430.110	Professional Services	325,000.00	Bio-Solid Management Project - Adjust line items to appropriate accounts	
Sewer Capital Projects	306-350-768 470.710	Eng & Inspec - Non Capiti	250,000.00	Bio-Solid Management Project - Adjust line items to appropriate accounts	
Sewer Capital Projects	306-350-768 470.726	Construction - Capitalize		Bio-Solid Management Project - Adjust line items to appropriate accounts	(682,500.00)
Sewer Capital Projects	306-350-770 410.100	Salaries & Wages	5,000.00	WWTP Centrifuge Project - Adjust line items to appropriate accounts	
Sewer Capital Projects	306-350-770 470.711	Eng & Inspec - Capitalize	175,000.00	WWTP Centrifuge Project - Adjust line items to appropriate accounts	
Sewer Capital Projects	306-350-770 470.726	Construction - Capitalize		WWTP Centrifuge Project - Adjust line items to appropriate accounts	(180,000.00)
Total Sewer Capital Projects			1,262,500.00		(1,262,500.00)
Cargo Capital Projects	307-001 330.300	State Grt - Capital		Shelkof Street Pedestrian Improvements - To reinstate project closed in error in prior years	(2,300,000.00)
Cargo Capital Projects	307-001 390.100	Transfer from General		Shelkof Street Pedestrian Improvements - To reinstate project closed in error in prior years	(100,000.00)
Cargo Capital Projects	307-360-816 410.100	Salaries & Wages	14,000.00	Shelkof Street Pedestrian Improvements - To reinstate project closed in error in prior years	
Cargo Capital Projects	307-360-816 430.110	Professional Services	106,000.00	Shelkof Street Pedestrian Improvements - To reinstate project closed in error in prior years	
Cargo Capital Projects	307-360-816 470.710	Eng & Inspec - Non Capiti	280,000.00	Shelkof Street Pedestrian Improvements - To reinstate project closed in error in prior years	
Cargo Capital Projects	307-360-816 470.725	Construction - Non Capiti		Shelkof Street Pedestrian Improvements - To reinstate project closed in error in prior years	
Cargo Capital Projects	307-360-823 410.100	Salaries & Wages	2,000.00	Pedestrian Pathways - Adjust line items to appropriate accounts	
Cargo Capital Projects	307-360-823 430.110	Professional Services	13,000.00	Pedestrian Pathways - Adjust line items to appropriate accounts	
Cargo Capital Projects	307-360-823 470.710	Eng & Inspec - Non Capiti	369,000.00	Pedestrian Pathways - Adjust line items to appropriate accounts	
Cargo Capital Projects	307-360-823 470.725	Construction - Non Capiti		Pedestrian Pathways - Adjust line items to appropriate accounts	(384,000.00)
Total Cargo Capital Projects			2,784,000.00		(2,784,000.00)
Harbor Capital Projects	308-370-850 410.100	Salaries & Wages	1,000.00	SHH Repairs - Adjust line items to appropriate accounts	
Harbor Capital Projects	308-370-850 430.110	Professional Services	1,000.00	SHH Repairs - Adjust line items to appropriate accounts	
Harbor Capital Projects	308-370-850 470.725	Construction - Non Capiti		SHH Repairs - Adjust line items to appropriate accounts	(2,000.00)
Harbor Capital Projects	308-370-855 410.100	Salaries & Wages	7,500.00	Channel Transient Float Replacement - Adjust line items to appropriate accounts	
Harbor Capital Projects	308-370-855 430.110	Professional Services	1,000.00	Channel Transient Float Replacement - Adjust line items to appropriate accounts	
Harbor Capital Projects	308-370-855 470.711	Eng & Inspec - Capitalize		Channel Transient Float Replacement - Adjust line items to appropriate accounts	(508,500.00)
Harbor Capital Projects	308-370-855 470.726	Construction - Capitalize		Channel Transient Float Replacement - Adjust line items to appropriate accounts	
Total Harbor Capital Projects			510,500.00		(510,500.00)
Cargo Fund	500-001 375.605	Discounts Received		Health Insurance discounts received from Aetna	(6,570.00)
Cargo Fund	500-001 385.100	Appropriation - Fund Bal	6,570.00	Offset for Cargo fund - will increase fund balance	
Total Cargo Fund			6,570.00		(6,570.00)
Harbor Fund	510-001 375.605	Discounts Received		Health Insurance discounts received from Aetna	(22,000.00)
Harbor Fund	510-001 385.100	Appropriation - Fund Bal	22,000.00	Offset for Harbor Fund - will increase fund balance	
Total Harbor Fund			22,000.00		(22,000.00)
Shipyard Fund	512-001 375.605	Discounts Received		Health Insurance discounts received from Aetna	(2,925.00)
Shipyard Fund	512-001 385.100	Appropriation - Fund Bal	2,925.00	Offset for Shipyard Fund - will increase fund balance	
Total Shipyard Fund			2,925.00		(2,925.00)

Water Utility Fund	550-001 375.605	Discounts Received	(12,750.00)	Health Insurance discounts received from Aetna
Water Utility Fund	550-001 385.100	Appropriation - Fund Bal	(565,378.15)	Offset for Water Utility fund
Water Utility Fund	550-001 390.309	Transfer from P&R	30,000.00	Adjustment needed to appropriate reflect split purchase of equipment between two funds:
Water Utility Fund	550-560-198 490.305	Trans-Water Cap Project	548,128.15	To adjust budget for transfer of water loan proceeds already reflected in the projec
		Total Water Utility Fund	578,128.15	
Sewer Utility Fund	570-001 375.605	Discounts Received	(20,660.00)	Health Insurance discounts received from Aetna
Sewer Utility Fund	570-001 385.100	Appropriation - Fund Bal	(1,773,943.95)	Offset for Sewer Utility Fund
Sewer Utility Fund	570-580-198 490.306	Trans-Sewer Cap Project	1,794,603.95	To adjust budget for transfer of sewer loan proceeds already reflected in the projec
		Total Sewer Utility Fund	1,794,603.95	
Trident Basin Airport Fund	580-590-100 450.170	Security Patrols	3,000.00	Adjust line items to appropriate accounts due to new account number
Trident Basin Airport Fund	580-590-100 460.225	Public Works Services	(3,000.00)	Adjust line items to appropriate accounts due to new account number
		Total Trident Basin Airport Fund	3,000.00	
E-911 Services	585-001 375.605	Discounts Received	(165.00)	Health Insurance discounts received from Aetna
E-911 Services	585-001 385.100	Appropriation - Fund Bal	165.00	Offset for E911 Fund - will increase fund balance
		Total E911 Services Fund	165.00	
		All Funds - Total Revenues	(4,922,454.06)	
		All Funds - Total Expenditures	4,922,454.06	

MEMORANDUM TO COUNCIL

To: Mayor Branson and City Council members
From: Aimée Kniazowski, City Manager 
Thru: James R. Mullican Jr., Fire Chief
Date: April 27, 2017

Agenda Item: V. b. **Resolution No. 2017–11, Authorizing the Transfer of \$21,000 From the FY2017 General Fund Balance to the General Capital Project Fund – Kodiak Fire Station Deferred Maintenance Project No. 4015**

SUMMARY: The existing 1000 gallon fuel tank at the City Fire Station is in need of immediate replacement. This single walled tank has been in service for over twenty years supplying fuel for both Fire Departments boiler/hot water and emergency generator. Recent external inspections have revealed several areas showing significant sign of impending failure. John's Heating submitted a quote to complete required fuel tank and fuel line replacement in the amount of \$20,943.85. Currently, the Fire Department Deferred Maintenance account does not have enough funds to complete this project. Staff recommends Council authorize the movement of funds from the General Fund to General Capital Project Fund – Kodiak Fire Station Deferred Maintenance Project No. 4015 account in the amount of \$20,943.85 in order to complete this project.

PREVIOUS COUNCIL ACTION: Council approves movement of funds from the General Fund to projects requiring funding.

DISCUSSION: The City Fire Department provides 24 hour emergency Fire and Medical response to the citizen's and guests of the City and Medical services to the Kodiak road system. The Fire Station houses this service. In 2007 the City upgraded the existing boiler and heating system throughout the station, however, the existing 1000 gallon fuel tank and delivery system was not upgraded. Recent inspections have revealed several areas of impending failure on the tank. Failure of the tank would release up to 1000 gallons of fuel oil into the environment as well as shut down the Fire Stations heat, hot water, and emergency generator.

ALTERNATIVES:

- 1) Adopt Resolution No. 2017–11, which is the recommendation of staff because it will allow the Fire Department's fuel tank to be replaced.
- 2) Do not adopt Resolution No. 2017–11, which is not recommended because it would require the current fuel tank to remain in service significantly increasing the City's liability for potential fuel release.

FINANCIAL IMPLICATIONS: The cost of this repair requires movement of funds from the General Fund to the General Capital Project Fund – Kodiak Fire Station Deferred Maintenance Project No. 4015 account.

LEGAL: The Kodiak City Code, Section 3.12.070 (d), Exceptions to bidding requirements, allows the City Manager to determine a sole source for award of a bid or contract that serves the best interest of the City. This award recommendation falls into the sole source category.

STAFF RECOMMENDATION: Staff recommends Council adopt Resolution No. 2017–11 to approve transfer of funds to allow for the replacement of the Fire Station’s fuel tank.

CITY MANAGER’S COMMENTS: This is another needed repair that falls into the deferred maintenance category for the fire station. I support this transfer as a tank failure would have serious consequences. Therefore, I recommend Council adopt Resolution No. 2017–11.

ATTACHMENTS:

Attachment A: Resolution No. 2017–11

Attachment B: Request for transfer of funds from Fire Chief Mullican

Attachment C: John’s Heating quote #2744 dated 4/11/2017

PROPOSED MOTION:

Move to adopt Resolution No. 2017–11.

**CITY OF KODIAK
RESOLUTION NUMBER 2017-11**

**A RESOLUTION OF THE COUNCIL OF THE CITY OF KODIAK
AUTHORIZING THE TRANSFER OF \$21,000 FROM THE FY2017 GENERAL
FUND BALANCE TO THE GENERAL CAPITAL PROJECT FUND – KODIAK
FIRE STATION DEFERRED MAINTENANCE PROJECT NO. 4015**

WHEREAS, the City currently has the Kodiak Fire Station Deferred Maintenance Project No. 4015 for repairs and maintenance to the Kodiak Fire Station; and

WHEREAS, the fuel tank at the Kodiak Fire Station requires immediate removal and replacement of the existing fuel tank; and

WHEREAS, the cost of the Kodiak Fire Station fuel tank removal and replacement would cost \$20,934, which exceeds the current available balance in Project No. 4015; and

WHEREAS, funds are available in the FY2017 General Fund fund balance to transfer the \$21,000 required; and

WHEREAS, Article V of the City of Kodiak Charter provides that an appropriation of funds separate from the budget document may be made by resolution.

NOW, THEREFORE, BE IT RESOLVED by the Council of the City of Kodiak, Alaska:

Section 1. The Fiscal Year 2017 Operating Budget is amended by appropriating and transferring \$21,000 from the General Fund fund balance to the following account to the General Capital Project:

Appropriation / Transfer From:

<u>Account Description</u>	<u>Amount</u>
General Fund fund balance	\$21,000

Transfer to:

<u>Account Description</u>	<u>Amount</u>
General Capital Project Fund - Kodiak Fire Station Deferred Maintenance Project No. 4015	\$21,000

Section 2. This resolution shall become effective upon adoption.

CITY OF KODIAK

MAYOR

ATTEST:

CITY CLERK

Adopted:

Office of the Fire Chief

219 Lower Mill Bay Road Kodiak, Alaska 99615



Memorandum

To: Aimee Kniaziowski, City Manager
From: James R. Mullican Jr, Fire Chief
Date: April 14, 2017
Re: Supplemental Budget Request

Aimee:

The existing 1000 gallon fuel tank currently in service at the Fire Station is in need of immediate replacement. As you know I originally budgeted this replacement for next fiscal year, however, after a recent visual inspection my concern for a catastrophic failure has greatly increased. The existing tank is over 20 year's old, single walled, as is showing significant signs of impending failure.

I am requesting supplemental monies be approved to purchase and install a new double walled 1000 gallon fuel tank. This installation would also bring our existing fuel storage and piping system up to code. I have contacted John's Heating, at the recommendation of Mark Kozak and received the enclosed quote for a total of \$20,943.85. The total request is as follows:

Table with 2 columns: Item and Amount. Items include Fuel Tank, Shipping, Labor, Fork lift Rental, Parking Billiards and associated metal, Concrete Pad, Fuel Line, Existing system Disposal, Miscellaneous materials, Discount, and Total.

I have included the quote from John's Heating with this memo. If approved, I request a transfer of funds from the General Fund to General Capital Projects #4015 KFD Deferred Maintenance Account and approval to go forward with the project utilizing John's Heating.

Thank you.



"IN HEAT 24 HOURS A DAY"
(907) 486-3706

John's Heating Service, Inc.

1311 Mill Bay Road
Kodiak , Alaska 99615

Phone # (907) 486-3706

Fax # (907) 486-2497

Estimate

Date	Estimate #
4/11/2017	2744

Name / Address
City of Kodiak PO Box 1397 Kodiak AK 99615-1397

			Project
			Fire Department - 21...
Description	Qty	Rate	Total
Fuel Tank	1	16,713.25	16,713.25
Cost for shipping	1	1,800.70	1,800.70
Cost of Labor	1	1,650.00	1,650.00
Cost of Helper's Labor	1	880.00	880.00
Forklift	1	330.00	330.00
Billards, and steel c channel for removal of tank and stand if forklift is needed.	1	220.00	220.00
Concrete Pad	1	1,002.00	1,002.00
3/8" OD Copper Fuel Line (underground plastic coated)	100	3.15	315.00
Disposal Fee for system	1	200.00	200.00
Miscellaneous costs and materials throughout the installation.	1	160.00	160.00
Estimate to install new 1000gal double walled tank with oil sensors and spill protection - painted inside and out. Set in new tank on concrete pad w/ forklift slots for ease of removal. Also install billards into the concrete pads for collision protection. Run new oil lines to building. Pump fuel from old tank and fire up boiler. Get plumbing works to restart sensor. Dispose of old tank and lines. Clean up area.			
Invoice Subtotal			23,270.95
10% Discount		-10.00%	-2,327.10
Estimate is good for 30 days from today's date. Please sign & return if you agree to this estimate.			Subtotal
			\$20,943.85
			Sales Tax (0.0%)
			\$0.00
			Total
			\$20,943.85

MEMORANDUM TO COUNCIL

To: Mayor Branson and City Councilmembers

From: Aimée Kniazowski, City Manager and Mike Tvenge, Deputy City Manager

Date: April 27, 2017

Agenda Item: V. c. **Resolution No. 2017-12, Supporting Changes to Timing and Location of the U.S. Department of Defense "Northern Edge 2019" Training Exercise, and Subsequent Operations, in the Gulf of Alaska**

SUMMARY: Northern Edge is a U.S. Pacific Command exercise, hosted by the Alaskan Command. The exercise is designed to sharpen combat skills, improve command, control and communication relationships, and mature interoperable plans and programs. This resolution requests the Navy and Alaska Command consider the location and timing of the U.S. Navy's Northern Edge 2019 training activities in the Gulf of Alaska. City Council recommends a training date after the month of September, when marine mammal, fish and migratory bird abundances are lower and utilize a training location to avoid the northwestern area of the Temporary Marine Activities Area. Resolution No. 2017-12, (Attachment A) outlines these requests for the Council to consider adopting.

PREVIOUS COUNCIL ACTION:

- On December 6, 2016, Council had a discussion of the military exercises in Alaska and Upcoming Northern Edge Exercise with a representative from the Alaskan Command.

DISCUSSION: In December 2016, a presentation was provided to the City Council regarding Northern Edge 2017. Recently, the Kodiak Island Borough adopted a resolution on April 6, 2017, requesting the time and location change of the Northern Edge training exercise. Council has received public comment from Tom Lance, Natural Resource Director for Sun'aq, encouraging the Council to consider adopting a resolution or writing a letter to Senator Murkowski to impact the change in timing and location of these exercises. A letter is attached (Attachment B) from Senator Murkowski, addressed to Admiral Harris Jr. USN Commander and Admiral Swift USN Commander outlining her support for Kodiak and the Gulf of Alaska, regarding these exercises and the potential impact to the fishing communities in Alaska.

ALTERNATIVES:

- 1) Adopt Resolution No. 2017-12.
- 2) Do not adopt the resolution.

CITY MANAGER'S COMMENTS: The City of Kodiak respects the need for training exercises by our military forces, specifically our Navy. While it is important and necessary, the community also recognizes this training exercise competes with our local fishing season. The timing of fishing cannot be changed however it may be possible to adjust the Northern Edge 2019 training exercise to a compatible

time with our food harvest. I support the efforts Senator Murkowski has shown by requesting open dialogue by the Alaska Command and the Gulf of Alaska communities to work together to provide benefit for all. Northern Edge 2017 Exercise is scheduled; however there is time to affect the next training exercise, Northern Edge 2019. I support Resolution No. 2017-12.

ATTACHMENTS:

Attachment A: Resolution No. 2017-12

Attachment B: Letter from Senator Murkowski re: Northern Edge

PROPOSED MOTION:

Move to adopt Resolution No. 2017-12.

**CITY OF KODIAK
RESOLUTION NUMBER 2017-12**

A RESOLUTION OF THE COUNCIL OF THE CITY OF KODIAK IN SUPPORT OF CHANGES TO TIMING AND LOCATION OF THE US DEPARTMENT OF DEFENSE “NORTHERN EDGE 19” TRAINING EXERCISE, AND SUBSEQUENT OPERATIONS, IN THE GULF OF ALASKA

WHEREAS, all of the communities in the Kodiak Island Borough are economically and culturally dependent upon strong fisheries; and

WHEREAS, the economies of the City of Kodiak as well as the road system and remote communities of the Kodiak Island Borough are already struggling due to downturns in the national and international pollock markets and weak pink salmon return; and

WHEREAS, Kodiak has an important history of being a Navy town, and the current military presence of the US Naval Special Warfare Command and US Coast Guard are essential elements of our economy, community, and safety of our residents; and

WHEREAS, the efforts of the Department of Defense to communicate and consult with local organizations regarding past and planned Northern Edge exercises are greatly appreciated; and

WHEREAS, it is essential for the military to be able to conduct meaningful exercises for the sake of national security, which encompasses the security of Kodiak, the security of Alaska, and the security of the United States as a whole; and

WHEREAS, food security and economic security are also essential elements of national security, and it is critical for the communities of Kodiak to have harvestable fisheries resources to sustain our families, as well as to provide seafood for markets across the United States and the entire world; and

WHEREAS, previous military exercises have occurred when populations of migratory fish species are locally low or absent, and/or been conducted in areas of lower habitat importance for feeding and rearing of economically and culturally essential fisheries resources; and

WHEREAS, a military presence in the Gulf of Alaska can be conducted to minimize negative impacts on the existing maritime framework of subsistence and commercial economies and cultures reliant on fisheries in Kodiak; and

WHEREAS, flexibility in changing the spatial and temporal locations of maritime exercises has demonstrated that strong fisheries and naval activities are not necessarily mutually exclusive.

NOW, THEREFORE, BE IT RESOLVED by the Council of the City of Kodiak, Alaska, hereby requests the US Navy to continue its communication and consultation with the communities of Kodiak and their tribal and municipal governments to assist us in protecting our fisheries; to cooperate with our researchers to enhance knowledge of these fisheries re-

sources; and to determine equitable methods to conduct their naval exercises without negatively impacting our Archipelago's economies, such as through the moratorium on expending live ordnance on the Portlock Bank.

BE IT FURTHER RESOLVED that the Council hereby requests the US Navy to conduct their activities in the Temporary Marine Activities Area after September when overall marine mammal, fish, and migratory bird abundances are lower; and to avoid the northwestern area of the TMAA that includes the State Marine Protected Area, National Oceanic and Atmospheric Administration Fisheries Essential Salmon Habitat Protected Area, and the Gulf of Alaska Seamount Protected areas.

CITY OF KODIAK

MAYOR

ATTEST:

CITY CLERK

Adopted:

COMMITTEES:
ENERGY AND NATURAL RESOURCES
CHAIRMAN
APPROPRIATIONS
SUBCOMMITTEE ON INTERIOR,
ENVIRONMENT, AND RELATED AGENCIES
CHAIRMAN
HEALTH, EDUCATION, LABOR,
AND PENSIONS
INDIAN AFFAIRS

United States Senate

WASHINGTON, DC 20510-0203
(202) 224-6665
(202) 224-5301 FAX

April 7, 2017

Admiral Harry B. Harris, Jr. USN
Commander
US Pacific Command
Box 64031
Camp H.M. Smith, Hawaii 96861-4031

Admiral Scott H. Swift, USN
Commander
US Pacific Fleet
250 Makalapa Drive
Pearl Harbor, Hawaii 96860-3131

Gentlemen:

I write with respect to the Northern Edge 2017 exercise which is scheduled to take place in Alaska from May 1-12, 2017. This biennial exercise is conducted under the auspices of the US Pacific Command and hosted by the Alaskan Command. While much of the exercise occurs on the ground ranges and airspace of the Joint Pacific Alaska Range Complex, a portion is conducted in the Gulf of Alaska. It is this latter aspect of the exercise, conducted by the Navy, which has generated concern among Alaska communities.

In September 2016, I wrote Secretary Mabus to request that the Navy conduct a robust outreach to stakeholders in the course of planning Northern Edge 2017 to ensure that concerns were received and addressed. The Navy, with support from Alaskan Command, has participated in an extensive series of stakeholder meetings. During these meetings, the Navy discussed a mitigation package for Northern Edge 2017, which included avoidance of sensitive areas and restrictions on the scope of the exercise. The outreach team briefed the Alaska congressional delegation on two occasions.

In spite of the Navy's improved outreach there remains dissatisfaction with respect to the timing of the exercise: specifically, its proximity to the fishing season in the Gulf of Alaska. Some stakeholders argue that scientific knowledge is insufficient to assure that the Navy's activities during this sensitive time are fully compatible with the region's commercial fishing economy. Last evening, the legislative body of the Kodiak Island Borough unanimously passed a resolution acknowledging these concerns.

The purpose of this letter is to request that US Pacific Command give serious thought to conducting the Gulf of Alaska component of the Northern Edge 2019 exercise in the fall. I have raised this issue with the outreach team and understand that this alternative is under consideration. It is important that your consideration of this alternative be transparent to the affected communities.

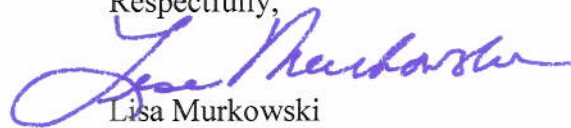
While the communities are appreciative that the outreach team came to visit, one member of the Kodiak Island Borough Assembly related the frustration at not being able to meet with the senior leader who ultimately makes the decision on exercise scheduling. Another related that a member of the outreach team stated that the concerns expressed by the Assembly Member were “above his pay grade.”

In addition to the scheduling issues, it has also been suggested to me that the Navy review the base of scientific knowledge relating to the compatibility of naval exercises with high value fisheries and commit to fund research gaps. Some of the stakeholders insist that the Navy submit to an independent review of the impact of offshore exercise activities in the Pacific on the fisheries by the National Academies. I find that the National Academies’ rigorous evidence based analysis is useful to constructive resolution of resource conflicts. I would appreciate your views on these suggestions.

Finally, I encourage the outreach team to continue its work with communities and stakeholders before, during and after the Northern Edge 2017 exercise. If adverse environmental impacts are identified in the course of the exercise, it is important that they be immediately addressed. An after-action review with communities and stakeholders following the conclusion of the exercise, to include the satisfaction of stakeholders with the quality of the outreach, would also be in order.

I expect to address these issues with senior leaders when the Navy appears before the Defense Appropriations Subcommittee next month. Thank you for considering my views on this matter.

Respectfully,




Lisa Murkowski
United States Senator

cc: Lt. Gen. Kenneth S. Wilsbach, USAF,
Commander, Alaskan Command

MEMORANDUM TO COUNCIL

To: Mayor Branson and City Councilmembers

From: Aimée Kniazowski, City Manager 

Thru: Mike Tvenge, Deputy City Manager 

Date: April 27, 2017

Agenda Item: V. d. Authorization of Pedestrian Pathway Plan

SUMMARY: During the Council work session on April 11, 2017, DOWL gave a presentation on the final draft of the Kodiak Pedestrian Pathway Plan, which was supported by Council. DOWL has held several open houses for public comment since the planning and design phases and the plan was posted for public comment from March 16 through April 10, 2017. The City Council received a final Kodiak Pedestrian Pathway Plan from DOWL dated April 2017 to authorize by motion during a regular meeting (Attachment A).

PREVIOUS COUNCIL ACTION:

- January 13, 2011, Council adopted Resolution No. 2011-02, requesting state cruise ship funds for pedestrian improvements on Shelikof from Pier II to downtown (#4 priority) and requesting cruise ship funds for pathway design from Pier II to Deadman's Curve (#5 priority)
- November 17, 2011, Council adopted Resolution No. 2011-37, accepting the legislative grant award for pathway design from Pier II to City Boundary (Deadman's Curve)
- March 2015, Council and staff talked with Alaska Department of Commerce, Community, and Economic Development staff and Kodiak's legislative delegation on problems using the grant award for the paved pathway from Pier II to Deadman's Curve due to excessive costs and the need to meet state Department of Transportation requirements
- July 22, 2015, Council received a presentation by State grants administrator on the need to decide on a project to use the grant funds on, as soon as possible, but no later than the fall
- August 11, 2015, Council discussed three options for pathway planning and design using the grant funds
- August 13, 2015, Council moved to authorize the 2012 legislative grant funds to be applied to developing a pedestrian pathway project from Pier II to Near Island and authorize the City Manager to request a planning and design proposal to initiate use of those funds
- October 22, 2015, Council authorized a professional services contract with DOWL for the Pedestrian Facilities Plan and Design in the amount of \$333,376 with funds coming from the 2012 Pedestrian Pathway Planning and Design Grant Project No. 8023 and authorized the City Manager to execute the documents on behalf of the City.
- August 9, 2016, Council received a presentation from DOWL on the pedestrian pathway plan during a work session.

APRIL 27, 2017

Agenda Item V. d. Memo Page 1 of 3

- March 21, 2017, Council received an update from DOWL on the draft pedestrian pathway plan during a work session.
- April 11 and 24, 2017, Council received the last presentation from DOWL on the final draft pedestrian pathway plan and reviewed it at these work sessions.

DISCUSSION: As indicated above, the City received a cruise ship excise tax grant in the amount of \$333,376 in FY2012 for the City’s portion of development of a bike path from Pier II to Deadman’s Curve. The project which was identified and funded in the statewide STIP program became too expensive to build due to the special design needs of the area.

The Council discussed possible legitimate ways to use the grant funds, but did not come to a consensus. They received a presentation in July of 2015 from a grants administrator who helped review the grant requirements. The administrator explained that the \$333,376 was awarded in FY2012 for the planning and design of a pathway project and that the funds would be lost if a decision was not made on which project to direct the money to. She also explained that the City could return the funds if they had no project ready for planning and design. She explained that the legislative grant funds came from the cruise ship excise tax and required a project that enhanced or benefitted cruise ship passengers, provided for their safety, and/or enhanced their experience in Kodiak as well as a project that would benefit Kodiak residents.

Following the meeting with the state grants administrator, the Council decided to authorize the grant funds be applied to developing a pedestrian pathway project from Pier II to Near Island and authorized the City Manager to request a planning and design proposal to initiate use of those funds

DOWL made a proposal to the City and was awarded work to develop a pedestrian facilities plan that completed existing connections from Pier II to the Near Island Bridge. DOWL held community meetings and presented the proposed plan to the City Council over the course of about 18 months. Michelle Ritter with DOWL made the last presentation of the draft plan and user comments to Council at the April 11, 2017, Council work session. The plan allows the City to build on previous plans for improved pedestrian access and to support additional tourism in the described area. The final step in the process is for Council to approve the plan by motion, which is staff’s recommendation.

ALTERNATIVES:

- 1) Authorize the Pedestrian Pathway Plan, which is staff’s recommendation and reflects Council’s direction.
- 2) Do not authorize the Pedestrian Pathway Plan, which is not recommended by staff because many months of work went into this plan which is usable and can be implemented as funds become available to make Kodiak a more pedestrian-friendly community.

FINANCIAL IMPLICATIONS: This will complete the project and the state grant funds allocated will be expended.

LEGAL: N/A

STAFF RECOMMENDATION: Staff recommends Council authorize the Pedestrian Pathway Plan.

CITY MANAGER'S COMMENTS: Council determined that this pathway plan would provide a basis for making Kodiak a better and safer place to walk and sightsee for tourists as well as local residents. I support the work and recommend Council make a formal acceptance of the plan by motion.

ATTACHMENT:

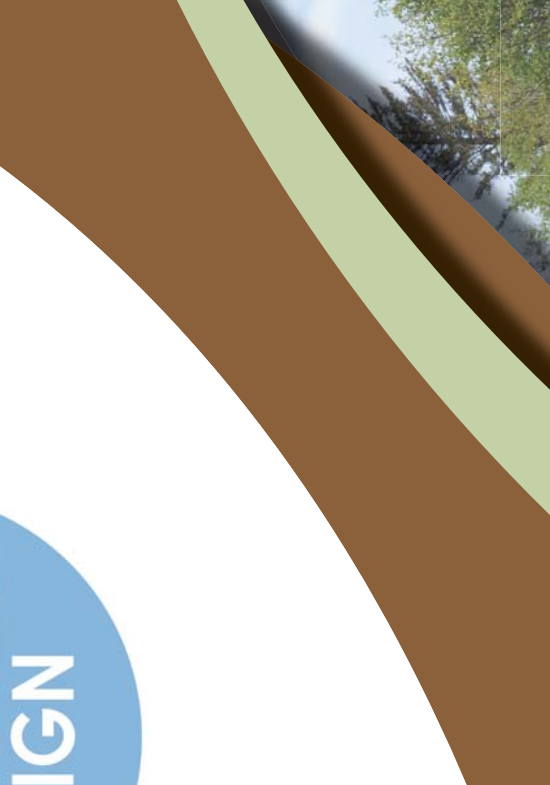
Attachment A: Final Pedestrian Pathway Plan from DOWL dated April 2017

PROPOSED MOTION:

Move to authorize the Kodiak Final Pedestrian Pathway Plan dated April 2017.

Kodiak Pedestrian Pathway

PLANNING AND DESIGN



Attachment A

Final Pedestrian
Pathways Plan
April 2017



Prepared For: City of Kodiak
Prepared By: DOWL

Kodiak Pedestrian Pathway

PLANNING AND DESIGN



Prepared For:
City of Kodiak
Public Works Department
2410 Mill Bay Road
Kodiak, Alaska 99315



Prepared By:
DOWL
4041 B Street
Anchorage, Alaska 99503
(907)562-2000
W.O. 1124.62085.01

Contents

Executive Summary	1
Section I: Plan Background	3
Introduction	3
Plan Description	3
Planning Context.....	3
Section II: Plan Development	7
Plan Process	7
Existing Conditions.....	7
Public Involvement	10
Summary of Recommendations	15
Section III: Recommendations	15
Project Ranking Criteria	17
Project Description & Scoring Tables	17
General Considerations	32
Section IV: Implementation	35
Implementation Order.....	35
Implementation Steps	35
Conclusion	36
Section V: Appendices	37
Appendix A: Public Comment	38
Appendix B: Federal Pedestrian and Bicycle Funding Opportunities	45

List of Images

Image 1: View of the distant mountains and waterfront from the bridge to Near Island.	3
Image 2: Sidewalk constructed along Shelikof Street from Pier 2 to Jack Hinkle Way.	6
Image 3: Mission Road looking southwest from the Alutiiq Museum.	7
Image 4: The intersection of Erskine Avenue and Mission Road southwest of the bridge to Near Island.	7
Image 5: Erskine Avenue near the intersection with Mill Bay Road.	7
Image 6: Gravel shoulder along Rezanof Drive that is used as a walkway by pedestrians.	7
Image 7: Vantage Point #1 - View from Mission Road of the Holy Resurrection Russian Orthodox Cathedral.	8
Image 8: Vantage Point #2 – View of the waterfront from Mill Bay Road and Kasheveroff Avenue looking towards the water.	8
Image 9: Vantage Point #3 – View from Mission Road looking northeast under the bridge.	8
Image 10: Vantage Point #4 – View of downtown Kodiak and the waterfront from the bridge to Near Island looking south.	8
Image 11: Vantage Point #5 – View of the water and surrounding mountains from the library social trail.	8
Image 12: Residential area along Mill Bay Road.	9
Image 13: Mission Road looking west from under the bridge to Near Island.	9
Image 14: Social trail on the embankment along Rezanof Drive near the bridge to Near Island.	9
Image 15: Social trail from the bend in Erskine Avenue to the Library.	9
Image 16: Intersection of Mission Road and Tagura Road.	9
Image 17: Signage on Mission Road to the Alutiiq Museum and to the Senior Center.	10
Image 18: Project presentation at the second open house.	10
Image 19: Egan Way looking northwest from the library driveway. Proposed sidewalk would be on the left side of the road in this image.	19
Image 20: Library entry sign. Proposed Sidewalk would wrap right in front of sign.	19

Image 21: Egan Way looking southeast from the Mill Bay Road. Proposed sidewalk would be on the right side of the road in this image.	19
Image 22: West side of library looking south. The existing social trail can be seen in the foreground. The proposed trail would follow the social trail south towards Erskine Avenue.	20
Image 23: Intersection of Rezanof Drive and the bridge to Near Island. The proposed trail from Rezanof (L3) would start on the left side of the image on the west side of Rezanof Drive.	21
Image 24: West side of Rezanof Drive looking north.	22
Image 25: West side of Rezanof Drive looking south.	22
Image 26: Pedestrians crossing Rezanof Drive to access the bridge to Near Island. This is where one of the proposed crosswalks would be located.	23
Image 27: As noted in the “Other Comments” row there is a pedestrian sign for crossing Rezanof Drive at the intersection of Mill Bay Road. But as seen in this image the crest in the hill above this crossing makes it hard see oncoming traffic.	23
Image 28: West side of the bridge abutment.	26
Image 29: Approach to the west side of the bridge.	26
Image 30: Existing social trail to the bridge.	26
Image 31: Existing gravel edge along Rezanof Drive. View from the bridge looking west on Rezanof Drive.	27
Image 32: Cruise ship passengers walking down Mission Road near the bridge to Near Island.	29
Image 33: Existing conditions on Mission Road.	29
Image 34: Existing driveway off of Mission Road. The driveway runs under the bridge to the ADF&G bunk house. The proposed trail would start here and switch back to the left and connect to a connection to the bridge.	30
Image 35: Pearson Cove Park. View of waterfront looking towards Near Island.	32
Image 36: Marine Way, view looking northeast towards Pearson Cove.	32
Image 37: View of where a pedestrian amenity node could be.	34

List of Figures

Figure 1: Isometric sketch of the Study Area and recommended projects.	1
Figure 2: Overall schematic plan.	2
Figure 3: Location map and plan boundary.	4
Figure 4: Previous and current plans and projects adjacent to the current Study Area.	5
Figure 5: Site inventory figure of existing conditions.	8
Figure 6: Consolidated map graphic from the first open house.	11
Figure 7: Concept routes and votes from the first open house.	12
Figure 8: Concept projects and votes from the second open house.	13
Figure 9: Consolidated graphic comments from the second open house.	14
Figure 10: Isometric sketch of the Study Area and recommended projects.	15
Figure 11: Overall schematic plan.	16
Figure 12: Library connections.	18
Figure 13: Bridge connections.	24
Figure 14: Sections of potential stair types including concrete, metal, and wood.	26
Figure 15: Rezanof Drive sidewalk section.	27
Figure 16: Mission road connections.	28
Figure 17: Other opportunities - wayfinding.	31
Figure 18: Potential waterfront connections.	32
Figure 19: Discover Kodiak visitor map.	33
Figure 20: Diagram of how pedestrian routes on the Discover Kodiak Map could be updated once projects are implemented.	34
Figure 21: Potential pedestrian nodes with frame structures that take advantage of views.	34

List of Tables

Table 1: Broad plans reviewed for pertinent information.	6
Table 2: Plans specific to pedestrian infrastructure adjacent to the study area.	6
Table 3: Construction and design that resulted from previous pedestrian plans.	6
Table 4: How ranking criteria is scored and weighted.	17

Table 5: Project L1 Description.	19
Table 6: Project L1 Score.	19
Table 7: Project L2 Description.	20
Table 8: Project L2 Score.	20
Table 9: Project L3 Description.	21
Table 10: Project L3 Score.	21
Table 11: Project L4 Description.	22
Table 12: Project L4 Score.	22
Table 13: Project L5 Description.	23
Table 14: Project L5 Score.	23
Table 15: Project B1 Description.	25
Table 16: Project B1 Score.	26
Table 17: Project B2 Description.	27
Table 18: Project B2 Score.	27
Table 19: Project M1 Description.	29
Table 20: Project M1 Score.	29
Table 21: Project M2 Description.	30
Table 22: Project M2 Score.	30
Table 23: Project O1 Description.	31
Table 24: Project O1 Score.	31
Table 25: Port of Kodiak, cruise ship schedule, 2016.	33

List of Acronyms

ADF&G	Alaska Department of Fish and Game
BIA	Bureau of Indian Affairs
COK	City of Kodiak
DOT&PF	Department of Transportation and Public Facilities (State of Alaska)
FLH	Office of Federal Lands Highway
FHWA	Federal Highway Administration
FTA	Federal Transit Administration
ITN	Island Trails Network
KIB	Kodiak Island Borough
NEPA	National Environmental Protection Agency
ROW	Right-of-way
TIGER	Transportation Investment Generating Economic Recovery
US DOT	United States Department of Transportation
USFWS	United States Fish and Wildlife Service
WFLHD	Western Federal Lands Highway Division

EXECUTIVE SUMMARY

A waterfront town tucked along the coast of Kodiak Island, the City of Kodiak (COK) is a destination for cruise ships that tour Alaska's waters. The city receives an annual influx of tourists during the cruise-ship season, April through September. Over this six-month period in 2016, the city welcomed 19,224 cruise ship passengers, according to the 2016 cruise ship schedule. In order to accommodate these visitors the State of Alaska distributes the Commercial Passenger Vessel Excise Tax, a cruise-ship "head tax", to coastal cities hosting cruise ships. As a recipient, the COK has completed plans and projects aimed at improving the pedestrian infrastructure starting at Pier 2, where cruise ships dock.

This Pedestrian Pathways Plan builds on previous efforts by focusing on pedestrian improvements between downtown and periphery tourist attractions; specifically a Study Area from Center Avenue, to the Fred Zharoff Memorial Bridge (referenced as the bridge to Near Island), and to the Kodiak Public Library (referenced as the library).

The steps to develop this plan included: researching previous plans in and around the area, walking the site to inventory existing conditions, analyzing opportunities and constraints, collecting feedback through a public involvement process, developing a list of potential projects, and analyzing that list to determine an implementation strategy.

The public involvement process included two open houses, collecting feedback through an online comment form and project email, and discussions with stakeholders. Based on research and feedback, recommendations for proposed projects within the Study Area were developed within four categories: library connections, bridge connections, Mission Road connections, and other opportunities.

Ten specific projects were identified:

- L** Library Connections
 - Sidewalk on Egan Way (L1)
 - Trail from Erskine Avenue to the Kodiak Public Library (L2)



Figure 1: Isometric sketch of the Study Area and recommended projects.

- Trail from Rezanof Drive to Erskine Avenue (L3)
 - Widen the Sidewalk on Rezanof Drive (L4)
 - Crosswalk Improvements on Rezanof Drive (L5)
- B** Bridge Connections
- Connection from Lower Erskine Avenue to Rezanof Drive (B1)
 - Pave the Sidewalk on Rezanof Drive (B2)

- M** Mission Road Connections
- Sidewalk on Mission Road (M1)
 - Trail from Mission Road to the Bridge Connection (M2)
- O** Other Opportunities
- Wayfinding (O1)

The ten projects were then scored based on these criteria:

- potential use intensity,
- estimated costs,
- level of effort,
- public input, and
- potential maintenance.

The ranking criteria resulted in the following order:

1. Crosswalk Improvements on Rezanof Drive (L5)
2. Wayfinding (O1)
3. Sidewalk on Egan Way (L1)
4. Pave the Sidewalk on Rezanof Drive (B2)
5. Sidewalk on Mission Road (M1)
6. Trail from Erskine Avenue to the Kodiak Public Library (L2)
7. Trail from Rezanof Drive to Erskine Avenue (L3)

8. Connection from Lower Erskine Avenue to Rezanof Drive (B1)
9. Trail from Mission Road to the Bridge Connection (M2)
10. Widen the Sidewalk on Rezanof Drive (L4)

Beyond the 10 (ten) specific projects these general recommendations were also outlined: connections outside of the Study Area (specifically waterfront connections), coordination with Discover Kodiak, and pedestrian design recommendations.

This Pedestrian Pathways Plan provides recommendations which, if implemented, can improve safety and the pedestrian experience for tourists and locals walking from downtown, to the bridge to Near Island, to the library, and to the surrounding amenities.

Overall Schematic

L Library Connections

- L1 - Sidewalk on Egan Way
- L2 - Trail from Erskine Ave. to the Kodiak Public Library
- L3 - Trail from Rezanof Dr. to Erskine Ave.
- L4 - Widen the Sidewalk on Rezanof Dr.
- L5 - Crosswalk Improvements on Rezanof Dr.

B Bridge Connections

- B1 - Connection from Lower Erskine Ave. to Rezanof Dr. (Options A & B)
- B2 - Pave the Sidewalk on Rezanof Dr.

M Mission Road Connections

- M1 - Sidewalk on Mission Rd.
- M2 - Trail from Mission Rd. to the Bridge Connection

O Other Recommendations

- O1 - Wayfinding

- Study Area
- Potential Connections
- Existing Sidewalks or Connections

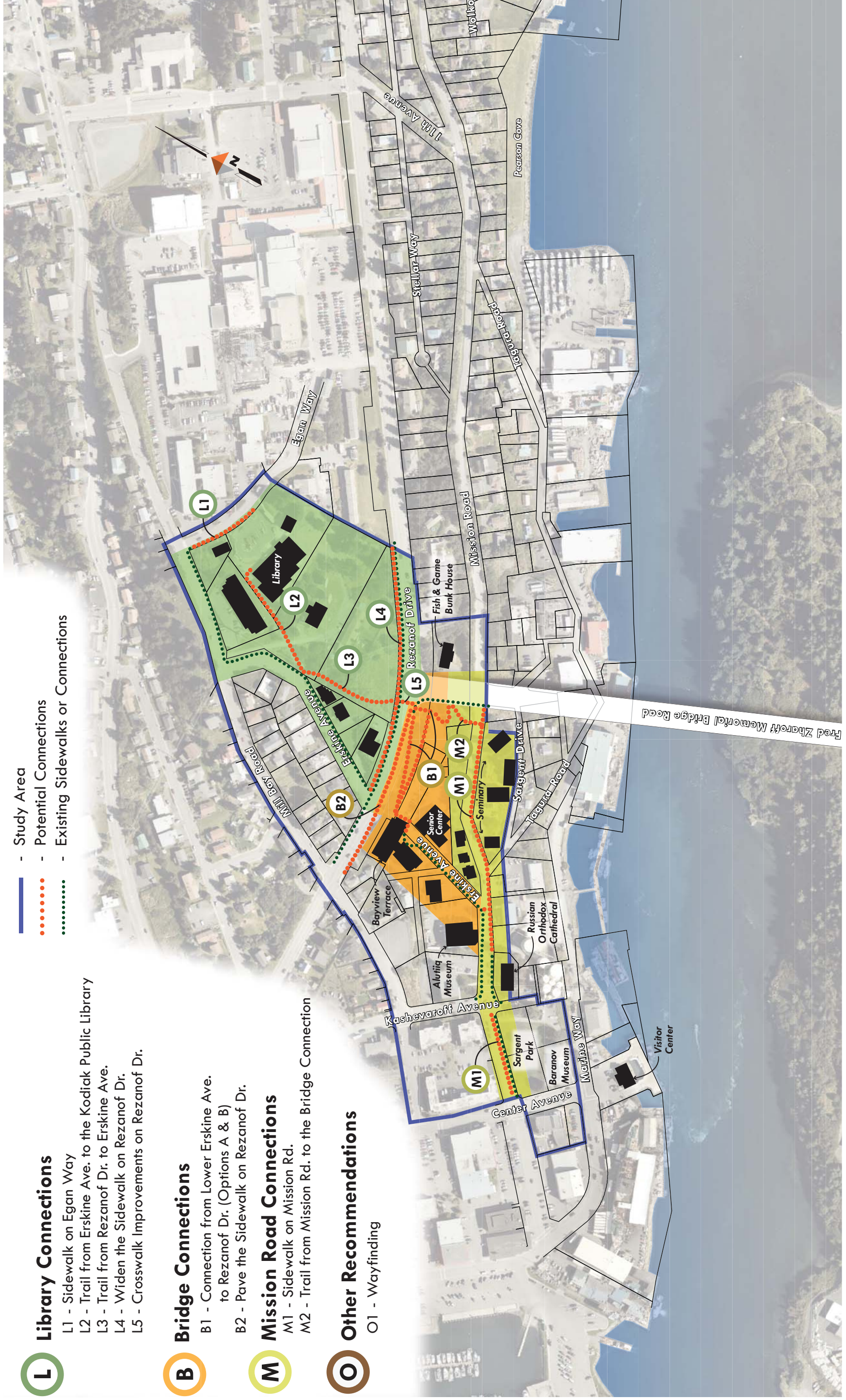


Figure 2: Overall schematic plan.

SECTION I: PLAN BACKGROUND

Introduction

The City of Kodiak (COK) is a coastal town located on the northeast shoreline of Kodiak Island. The city is the most populated area of the island, with a population of 6,253¹. As such it is the island's main transportation hub, with a nearby airport and extensive dock infrastructure including Piers 1 and 2, which serve visitors. Pier 1 serves the Alaska Marine Highway vessels, which are used by residents and visitors, many of which are pedestrians, while Pier 2 is the docking point for visiting cruise ships.

In 2016, the COK was scheduled to receive 19,224 cruise ship passengers over a six-month period, from April through September. The 2016 schedule accounted for thirteen vessels, operated by three different cruise lines, with passenger totals ranging from 382 to 1,898 per ship. In order to assist in accommodating this influx of visitors, the State of Alaska collects and distributes the Commercial Passenger Vessel Excise Tax, a cruise-ship "head tax", to towns like the COK. This excise tax is intended to be used for improvements to cruise ship passenger related infrastructure. Past projects in coastal Alaskan towns include improvements to docks and harbors, sidewalks and sea walks (waterfront pedestrian facilities), and the development of waterfront master plans among others².

As a recipient the COK has used the tax to fund plans and projects aimed at improving pedestrian connections for these visitors. Also funded by the cruise ship tax, this Pedestrian Pathways Plan continues this effort to improve facilities by analyzing and making recommendations regarding pedestrian connections to visitor destinations within walking distance of the COK's downtown.

¹ United States Census. *Quick Facts Population Estimate as of July 1, 2015*. Retrieved from: <http://www.census.gov/quickfacts/table/P5T045215/0240950>

² Alaska Department of Commerce, Community, and Economic Development. "Commercial Passenger Vessel Excise Tax: Community Needs, Priorities, Shared Revenue, and Expenditures Fiscal Years 2007 – 2014". January 2014.

Plan Description

The purpose of the Pedestrian Pathways Plan is to evaluate options and to make recommendations for improved pedestrian links from Center Avenue to the Fred Zharoff Memorial Bridge (referenced as the bridge to Near Island), and to the Kodiak Public Library (library). The Study Area, as outlined in Figure 1, is bordered by Center Avenue to the southwest, Mill Bay Road to the west, Egan Way to the north, and Mission Road to the east and south. Adjacent to the downtown area and waterfront, this Study Area includes several of the COK's main attractions: the Alutiiq Museum, the library, and the pedestrian link to Near Island. Because of these attractions, tourists and locals alike circulate this area on foot. Many of the streets within the area have sidewalks. Some walks are narrow and some streets lack sidewalks.

Planning Context

PREVIOUS PLANS & PROJECTS

To better understand the context of the Study Area we reviewed previous plans and projects relevant to this plan.

These plans and projects fall into two categories:

- Plans that are broad in scope, covering regional and city-wide areas and topics.
- Previous plans and projects specific to pedestrian infrastructure near the current Study Area. These plans focused on specific pedestrian improvements from Pier 2, where cruise ship passengers disembark, through downtown.

The plans specific to pedestrian improvements resulted in projects including:

- The construction of a sidewalk from Pier 2 to Jack Hinkle Way.
- Additional bulk head parking off of Shelikof Street, along St. Paul Harbor, is currently in the design process.



Image 1: View of the distant mountains and waterfront from the bridge to Near Island.

Figure 2 outlines the areas that previous plans adjacent to the Study Area covered. Tables 1, 2 and 3 outline the plans and projects that were reviewed for relevant information.

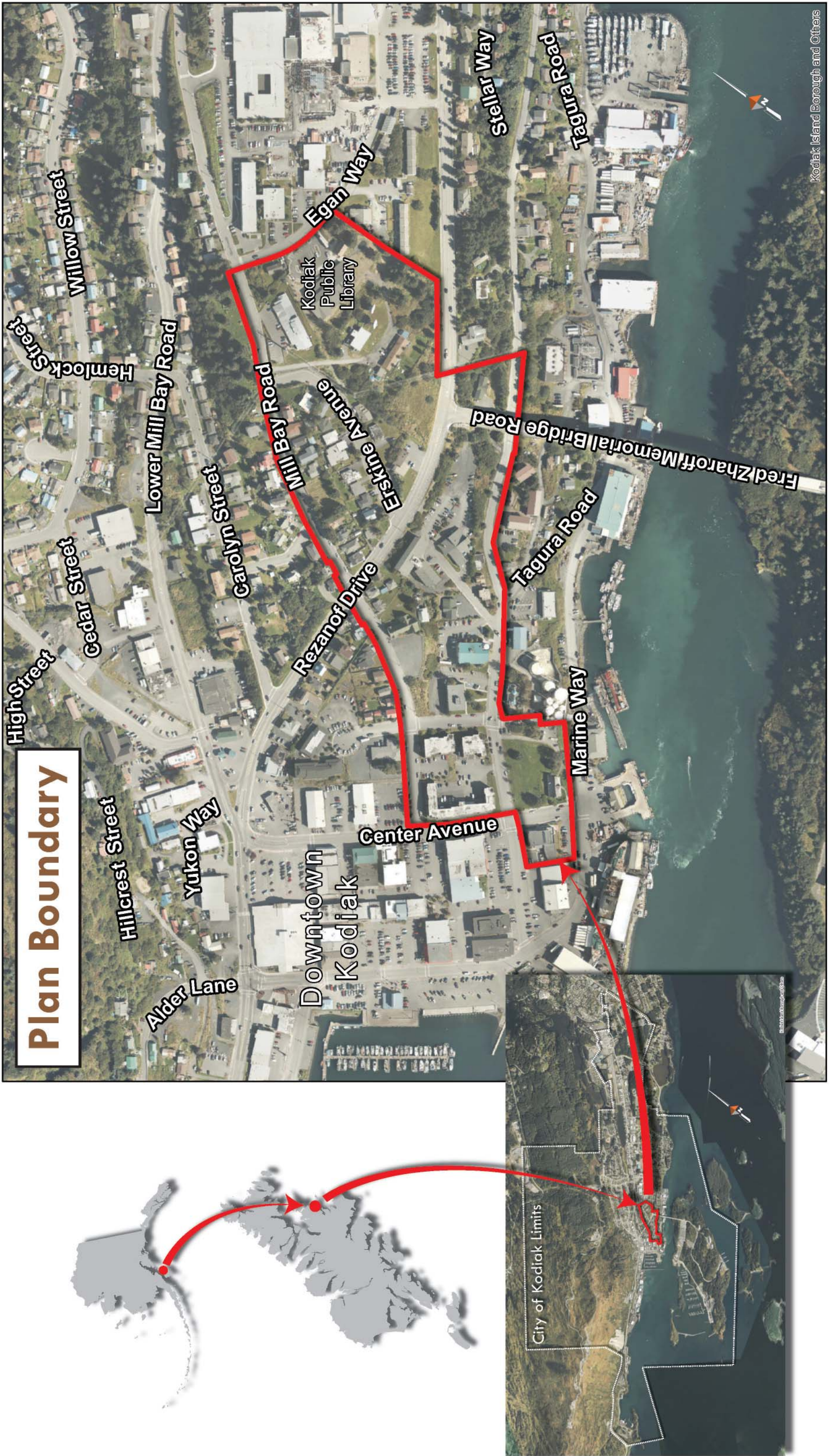
- Title 15 – Buildings and Construction
- Title 16 – Subdivision
- Title 17 – Zoning Code.

and design are in the Kodiak Island Borough Code (KIBC). Sections of code that are relevant to this plan include:

REGULATION & CODE

The City of Kodiak (COK) is an incorporated home-rule city in the Kodiak Island Borough (KIB). The KIB is classified as a Second Class Borough with area-wide powers that include planning, platting and land use regulation. Therefore, ordinances pertinent to planning

For this plan to be adopted as an official Guiding Document it will need to be approved by the Planning and Zoning Commission of KIB.



Kodiak Island Borough and Others

Figure 3: Location map and plan boundary.

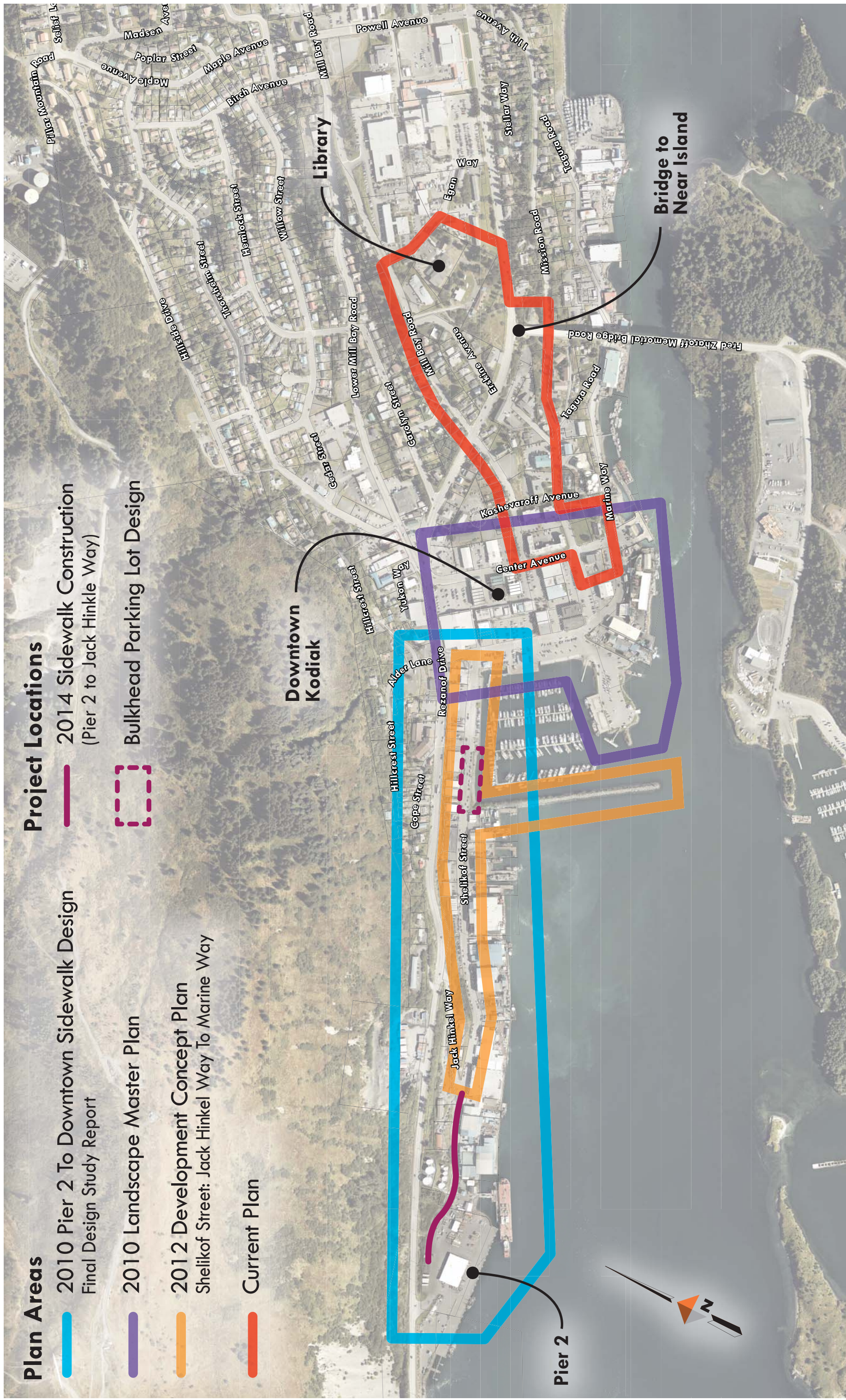


Figure 4: Previous and current plans and projects adjacent to the current Study Area.

Broad Plans Reviewed for Pertinent Information

<p>2002 Community Design Workshop Final Report</p>	<ul style="list-style-type: none"> Design charrette focused on developments in downtown Kodiak and Near Island; included recommendations specific to pedestrian enhancements: <ul style="list-style-type: none"> Crosswalk and sidewalk improvements within the Study Area. Sidewalk along Mission Road from Erskine Avenue to under the bridge to Near Island. Trail from the new Mission sidewalk under the bridge up to bridge entrance (stairway improvement right along either side of bridge abutment). Sidewalk on south-side of Rezanof Drive from Mill Bay Road to bridge. Widen sidewalks on north-side of Rezanof Drive from downtown past this Study Area. Improve pedestrian safety by narrowing roadways, providing curb extensions, and more striped crosswalks.
<p>2008 Kodiak Island Borough Comprehensive Plan</p>	<ul style="list-style-type: none"> Comprehensive plan describing the cultural, political, economic and environmental aspects of the Island, adopted by the Borough to guide planning decisions. Broad goals specific to pedestrian facilities such as: “incorporate creation of bicycle lanes or paths into road construction or improvement projects, where feasible, and incorporate creation of bicycle and pedestrian pathways in residential and other development projects”.
<p>2010 COK Waterfront Master Plan</p>	<ul style="list-style-type: none"> Overview of waterfront facilities’ existing conditions and desired improvements. Recommendations for open space development including amenity improvements to Pearson Cove.
<p>2011 Kodiak Road System Trails Master Plan</p>	<ul style="list-style-type: none"> Trails and pedestrian circulation inside and outside the urban context throughout Kodiak Island. Specific Project Recommendations (From Map 11 Proposed Improvements – Urban System, Proposed Trail Classification): <ul style="list-style-type: none"> Trail between Mission Road and Marine/Tagura under the bridge (Class 3b trail¹). Pedestrian Trail along Mission Road to Mission Beach (Class 5 trail²). <p>¹Trail classes are defined by many factors, please reference the Kodiak Road System Trails Master Plan for a complete description. In general the classes referenced here are:</p> <ol style="list-style-type: none"> Class 3b Trail: Developed/improved trail, typically constructed of native materials, trail experience is natural/semi-primitive. Class 5 Trail: Fully developed trail, rural to urban, constructed of asphalt or hardened surface.
<p>2015 Downtown Kodiak Revitalization Action Initiatives</p>	<ul style="list-style-type: none"> Key projects noted by the Downtown Kodiak Revitalization Special Committee, including the following within the Study Area: <ul style="list-style-type: none"> Install stairs up from Senior Citizens of Kodiak parking lot to bridge and crosswalk from bridge across Rezanof. Make a lot at Kashevaroff and Mill Bay into a small children’s park. Increase crosswalks as designed in 2002 Community design workshop report. Convert block of Marine Way between Sargent Park and Refuge into pedestrian historic core area.

Table 1: Broad plans reviewed for pertinent information

Plans Specific to Pedestrian Infrastructure Adjacent to the Study Area

<p>2010 Pier 2 to Downtown Sidewalk Design Final Design Study Report</p>	<ul style="list-style-type: none"> Analyzed the potential sidewalk improvements to allow cruise ship passengers to walk the waterfront to downtown. Prioritized a series of projects within the area. Catalyst for sidewalk construction and bulkhead parking improvements.
<p>2010 COK Landscape Master Plan for the Downtown Water, Sewer and Storm Drain Master Plan</p>	<ul style="list-style-type: none"> Master plan of pedestrian facilities in the downtown core of Kodiak. Identifies pedestrian and bicycle routes that link to this Study Area.
<p>2012 Development Concept Plan Shellikof Street: Jack Hinkle Way to Marine Way</p>	<ul style="list-style-type: none"> Developed design specific to pedestrian and parking improvements in area of focus. Concept design for bulkhead parking lot.

Table 2: Plans specific to pedestrian infrastructure adjacent to the study area.

Construction & Design That Resulted From Previous Pedestrian Plans

<p>2014 Sidewalk Construction</p>	<ul style="list-style-type: none"> Construction of the sidewalk from Pier 2 to Jack Hinkle Way (see the image below).
<p>2016 Bulkhead Parking Lot Design</p>	<ul style="list-style-type: none"> Current design for the construction of bulkhead parking off Shellikof Drive near downtown.

Table 3: Construction and design that resulted from previous pedestrian plans.



Image 2: Sidewalk constructed along Shellikof Street from Pier 2 to Jack Hinkle Way.

SECTION II: PLAN DEVELOPMENT

Plan Process

The development of this plan included research of previous plans, on-site visits, public meetings and analysis of the area to determine specific projects that will improve pedestrian circulation. The planning process started at the end of 2015 and will run through beginning of 2017. The steps to develop this plan have included:

- Research of the Study Area
 - Reviewing Previous Plans
 - Site Walks
 - Inventory of Existing Conditions
- Collecting feedback on existing conditions and preferred areas of focus. This included feedback from the first public open house on March 8, 2016 and online feedback.
- Developing draft recommendations.
- Presenting the draft recommendations and collecting feedback. Starting with the second open house on May 23, 2016, and continuing through the end of September 2016.
- Presenting the draft recommendations to the City Council on August 9, 2016.
- Developing the Draft Kodiak Pedestrian Pathways Plan and posting for public review in March and April of 2017.
- Reviewing public comments to the Draft Plan at the City Council work session in April of 2017.
- Finalizing the document (current plan).

Existing Conditions

To understand the opportunities and constraints related to pedestrian infrastructure, information on existing conditions was collected from public data, from site visits, and from public feedback.

Figure 3 and the subsections below outline and depict the existing conditions information cataloged in six categories:

- streets,
- terrain and vantage points,

- pedestrian circulation,
- zoning and land use,
- utilities, and
- points of interest.

STREETS

All streets within the Study Area are owned and maintained by the COK except for Rezanof Drive and the bridge to Near Island. These are owned by the DOT&PF. (Information derived from the 2013 Kodiak Urban Area Road Service Map provided by KIB.)

The Study Area's streets range from narrow, quiet residential streets to busy corridors resulting in a variety of pedestrian experiences. All streets in the study area have two-lane, two-way traffic and most have designated on-street parking (Figure 3 shows where on-street parking was either designated with signage or observed during site visits).

Street character was used as an evaluation category because it can help determine the best route for tourists. Street character is a subjective description of how a road

might feel as a pedestrian and can be the result of many things: how wide a road is; whether or not there are pedestrian amenities; or the existence of street trees. For this study two street attributes were considered significant character contributors: adjacent land use and traffic.

Based on these criteria streets in the Study Area fall into three groups:

1. High volume/fast traffic surrounded by mostly commercial land uses
 - Rezanof Drive
2. Low volume/slow traffic surrounded by mostly commercial and institutional land uses
 - Mission Road
 - Egan Way
 - Center Avenue
 - Erskine Avenue (south of Rezanof Drive)
3. Low volume/slow traffic surrounded by mostly residential land uses
 - Erskine Avenue (north of Rezanof Drive)
 - Mill Bay Road
 - Kasheveroff Avenue

Low volume/slow traffic provide a context for a safe and comfortable walk. While residential streets might not

warrant additional tourist foot-traffic. The preferable routes for tourists will most likely be streets with low volume/slow traffic and with adjacent commercial and institutional land uses.

TERRAIN & VANTAGE POINTS

The Study Area is a south-facing slope between the COK's waterfront and the northeast base of Pillar Mountain. There is an overall elevation change of approximately 150 feet. The elevation starts at 20 feet (adjacent to the Visitor Center), and rises to above 170 feet (at the library). The bridge to Near Island, halfway between these high and low points, sits at approximately 115 feet. This elevation change provides scenic vantage points, but steep terrain can make pedestrian connections challenging.



Top Left:
Image 3: Mission Road looking southwest from the Alutiq Museum.



Top Right:
Image 4: The intersection of Erskine Avenue and Mission Road southwest of the bridge to Near Island.



Bottom Left:
Image 5: Erskine Avenue near the intersection with Mill Bay Road.



Bottom Right:
Image 6: Gravel shoulder along Rezanof Drive that is used as a walkway by pedestrians.

Vantage Points

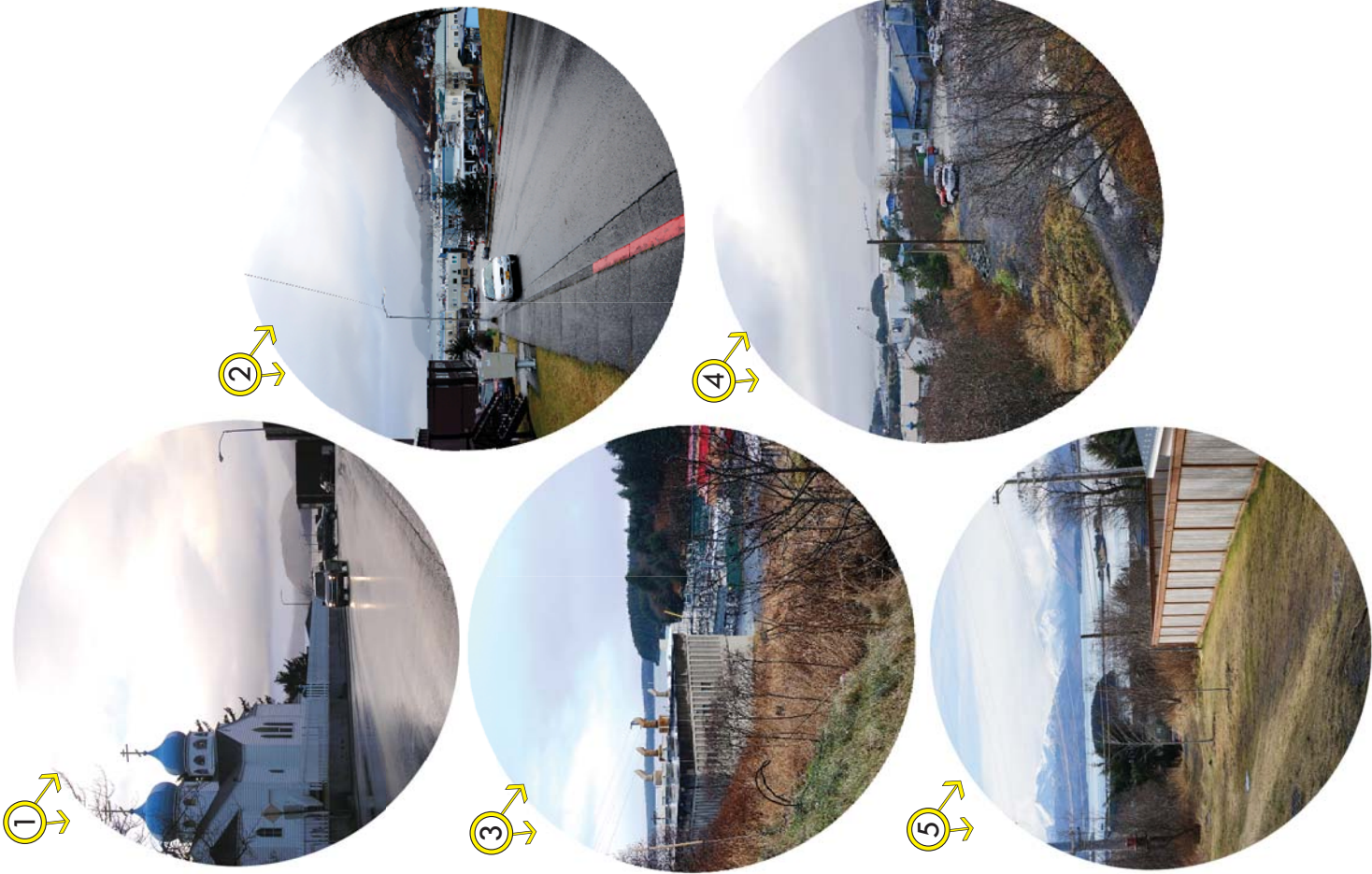


Image 7: Vantage Point #1- View from Mission Road of the Holy Resurrection Russian Orthodox Cathedral.

Image 8: Vantage Point #2 – View of the waterfront from Mill Bay Road and Kasheveroff Avenue looking towards the water.

Image 9: Vantage Point #3 – View from Mission Road looking northeast under the bridge.

Image 10: Vantage Point #4 – View of downtown Kodiak and the waterfront from the bridge to Near Island looking south.

Image 11: Vantage Point #5 – View of the water and surrounding mountains from the library social trail.

Existing Conditions



Figure 5: Site inventory figure of existing conditions.

PEDESTRIAN CIRCULATION

Tourists and residents circulate through the Study Area on foot to access attractions within it and beyond. Pedestrian routes are either along established paved sidewalks or on undeveloped paths through right-of-way, public land, or private properties. Figure 3 shows existing paved sidewalks, pedestrian related signage, shoulders along roads that are used by pedestrians, and social trails. Social trails are informal dirt trails that were not constructed or legally designated, but became distinct overtime by repeated use. The locations of social trails indicate connections that people are making where no established trail or sidewalk exists. The Study Area has several of these social trails as depicted by the dotted orange lines in Figure 3. The social trails indicate where a new sidewalk or trail improvements would formalize connectivity.

ZONING, LAND USE AND OWNERSHIP

Zoning, land use, and ownership information comes from the Kodiak Peninsula Borough Geographic Information System data. The five zoning districts found in the Study Area include the Retail Business (RB) District, Public Use Lands (PL) District, Conservation (C) District, Two-Family Residential (R-2) District, and Multi-Family (R-3) District. The zoning districts are generally established to guide land use and development decisions.

The RB District is established for the purpose of providing for a wide range of retail and service businesses for the consumer population of large segments of the community. Because of the potential for heavy traffic and the appearance and performance of these uses, this district is located on the periphery of residential areas and at the intersections of arterial and major collector streets and roads.

Within the Study Area, the RB District is generally located south of Rezanof Drive and includes a mix of land uses, including multi-family residential, institution, church, business residential, and business.

The PL District is established as a land use district for publicly owned land containing recreational, educational and institutional uses. There are several areas zoned PL District within the Study Area: the Baranov Museum and surrounding parcels and the Kodiak Public Library, owned by the COK, are examples of institution land use.

The Kodiak Island Housing Authority owns a multi-family residential building adjacent to the library. The parcel on the corner of Egan Way and Mill Bay Road is owned by the KIB and used for institutional land use. Alascom Incorporated owns a parcel south of the library, which is designated institution use. The Federal Government owns a parcel north of Rezanof Drive. Additionally, the Federal Bureau of Land Management owns a vacant parcel on Mill Bay Road.

The C District is established for the purpose of maintaining open space areas while providing for single-family residential, and limited commercial land uses. Within the Study Area, the C District includes a parcel below the bridge to Near Island owned by the U.S. Fish and Wildlife Service. This parcel is currently permitted to the State of Alaska Department of Transportation and Public Facilities (DOT&PF) for the use and maintenance of the bridge. The bunk house on this parcel is used to house State of Alaska Fish and Game employees.

The R-3 District is established as a land use district for one-, two-, and multi-family dwellings and limited office uses where public water and sewer services are available. The purpose of the zoning district is to encourage residential land use, prohibit commercial and industrial uses, and discourage uses that are incompatible with residential uses.

Within the Study Area, the R-3 District is generally located southeast of the intersection of Mill Bay Road and Rezanof Drive. Several parcels are privately owned and developed with single-family residences. Two parcels are owned by the Community Baptist Church and developed with a church and parking lot.

The R-2 District is established as a land use district for single-family and two-family residential dwellings and limited office uses where public water and sewer services are available. The purpose of the zoning district is to encourage low density residential land use, prohibit commercial and industrial uses, and discourage uses that are incompatible with residential use.

Within the Study Area, the R-2 District is located northeast of the intersection of Mill Bay Road and Rezanof Drive and along Erskine Avenue. The parcels in the Study Area are privately owned single-family residences.



Image 12: Residential area along Mill Bay Road.



Image 13: Mission Road looking west from under the bridge to Near Island.

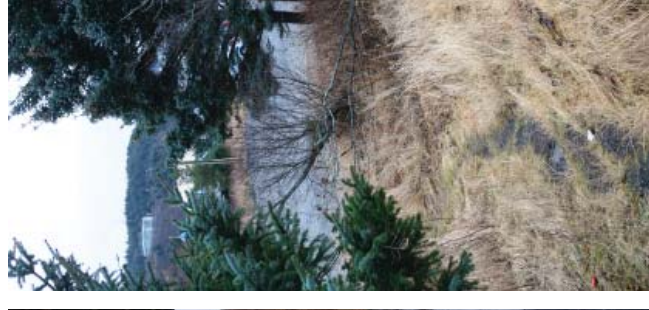


Image 14: Social trail on the embankment along Rezanof Drive near the bridge to Near Island.



Image 15: Social trail from the bend in Erskine Avenue to the Library.



Image 16: Intersection of Mission Road and Tagura Road.

UTILITIES

Street lighting exists in several locations within the Study Area, such as along Mill Bay Road and Rezanof Drive. The section of Mission Road, between Center Avenue and Kashevaroff Avenue, has street lights. Center Avenue between Mission Road and Marine Way also has street lighting. Erskine and Mission Road, north of Kashevaroff Avenue, do not have street lighting. There is no pedestrian-scale lighting developed within the Study Area.

In general, the Study Area is served with basic utilities. Water and sewer service is provided by the COK, Public Works Department. The Kodiak Electric Association provides power to the COK. Overhead power-lines run through the Study Area from the transmission station located northwest of the Study Area. Telecommunication services are provided by General Communications, Incorporated.

POINTS OF INTEREST

The Study Area includes some of the most notable attractions in the COK. Center Street is a main street through downtown. At its southeast end it passes the Kodiak National Wildlife Refuge Visitor Center, the Baranov Museum and dead-ends at the Kodiak Chamber of Commerce Visitor Center. Mission Road, which runs perpendicular to Center Avenue, provides access to Sargent Park, the Holy Resurrection Russian Orthodox Cathedral, that Alutiiq Museum, and St. Herman's Chapel. Buses drop cruise-ship visitors along this section of Mission Road due to the density and variety of attractions (as indicated by the 'Points of Interest' on Figure 3). The north end of the Study Area includes the library, the Russian Cemetery, and government offices which draw tourists and locals alike. This area also plays a role in access to nearby festivals and markets, including:

- Kodiak Brown Bear Festival, in March, hosted by the Kodiak Wildlife Refuge.
- Kodiak Whale Festival, from April through May, observed in a variety of venues including the Kodiak Senior Center, the Baranov Museum, the Kodiak Wildlife Refuge.
- Kodiak Crab Festival, May (Memorial Day Weekend), at venues located throughout downtown Kodiak.
- 1st Friday Art Crawls, observed monthly, in coffee shops and galleries.
- Pilgrimage to St. Herman, on to Spruce Island. Special services are held at the Holy Resurrection Orthodox Cathedral.
- Kodiak Bear Town Art & Crafts Market, operates periodically throughout the year, at the Su'naq Tribe bingo Hall.
- Ongoing Senior Center events, including the Arts & Crafts Fair in November.



Image 17: Signage on Mission Road to the Alutiiq Museum and to the Senior Center.

EXISTING CONDITIONS SUMMARY

The information on existing conditions provided a baseline on what pedestrian infrastructure exists and what is lacking. For example, although several streets have sidewalks, there are areas where people walk that lack a formal connection (i.e. social trails that cut through vegetated areas outside of right-of-way or streets that lack sidewalks). The existing conditions information was shared at the first public open house. The public provided feedback and added their own observations on the area. This information also served as baseline for determining areas of need and in which areas tourists might prefer to walk. Each proposed project in the Recommendations section has a description of the existing conditions that factored into the project being included in the list.

Public Involvement

The public involvement process included two open houses, collecting comments from an online project comment form, and from a project email address, as well as from discussions with stakeholders. These forums were used to:

- inform the public about the plan,
- listen to local perspectives on the area,
- define needed improvements, and
- collect feedback on the plan.

OPEN HOUSE #1

The first open house took place on March 8, 2016, from 6-9pm at the downtown Kodiak Harbor Convention Center. The meeting was attended by 24 residents as well as project representatives from DOWL and the COK. The goal of this meeting was to introduce the project, and collect feedback on existing conditions, determine the preferred routes for upgrades, and understand what types of enhancements are needed.

Two feedback exercises were conducted during the meeting: collecting comments on maps, and a 'Concept Route' voting exercise. For the mapping exercise attendees wrote and drew what they saw as issues and opportunities on aerials of the study area. These comments are consolidated in Figure 4.



Image 18: Project presentation at the second open house.

For the 'Concept Route' voting exercise, a map was provided showing various routes, some existing and some proposed. Attendees were asked to vote for their two preferred routes for pedestrian infrastructure improvements. Attendees were encouraged to provide additional routes if their preferred route was not shown. Vote tallies revealed the top three preferred routes for improvements were:

- 'Route C': From Center Street, to Mission Road, to Erskine Avenue, to the Near Island bridge
- 'Route D': From Center Street, to Mission Road, to the Near Island bridge
- 'Route G': Crossing Rezanof Drive, then to the library via an alternative trail

Comments and information collected during this first phase of feedback coalesced into pedestrian project priorities, including:

- Specific Location Improvements:
 - Route C; improvements from Mission Road, up Erskine Ave, up to the bridge

- Route D; improvements along Mission Road, then up under the bridge
- Route G; alternative trail from Rezanof Drive to the library.
- Improve crossing safety at Rezanof Drive from the bridge.
- Route to Pearson Cove

- General Improvement Considerations:
 - Improve wayfinding
 - General concern for pedestrian safety
 - Cruise-ship passenger drop-off should be considered and programmed accordingly

Based on the comments from this open house and online we developed a list of potential projects which were presented at the second open house.

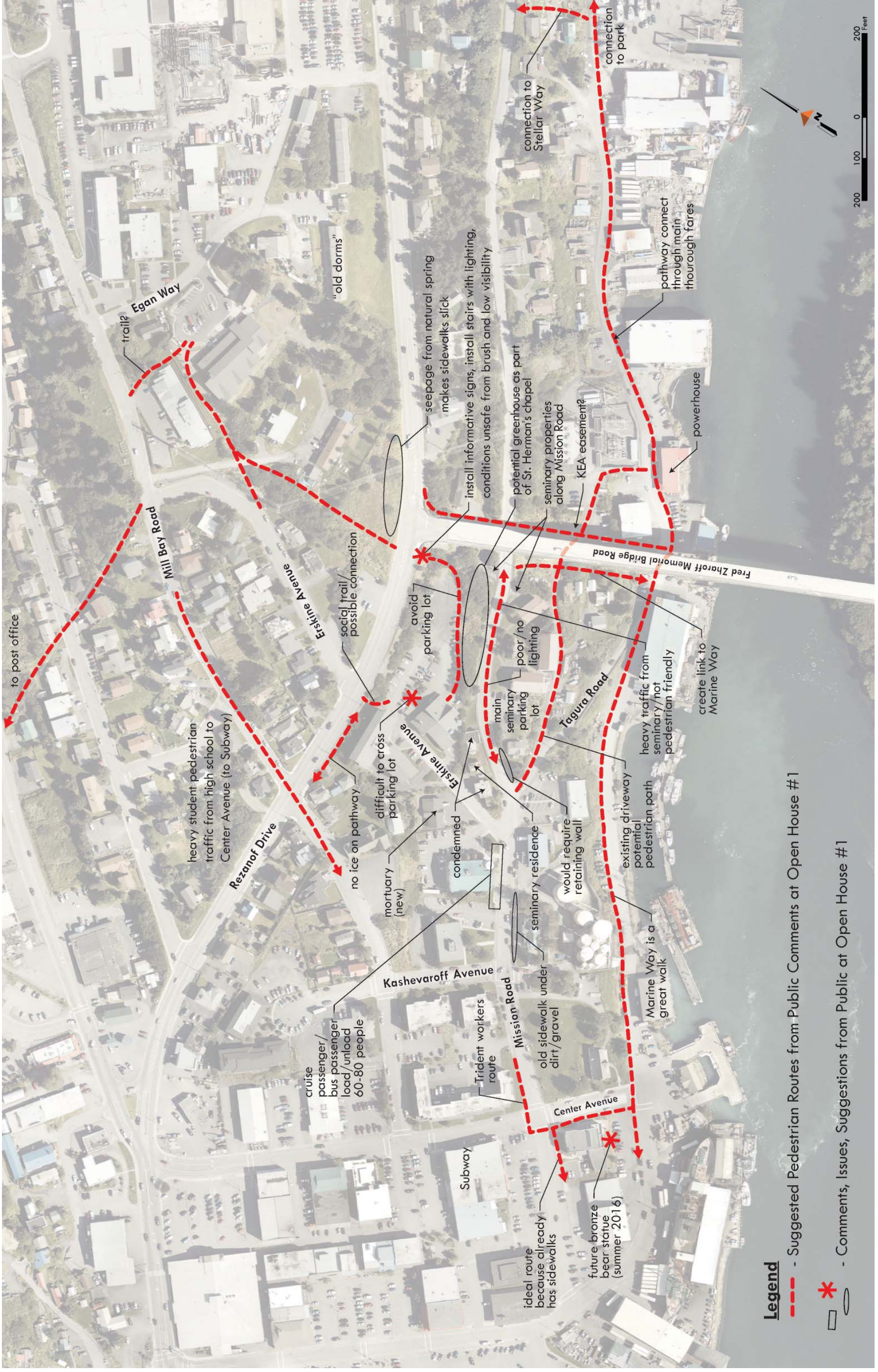


Figure 6: Consolidated map graphic from the first open house.

OPEN HOUSE #2

The second open house was held on May 23rd, again from 6-9pm at the downtown Kodiak Harbor Convention Center. There were 12 attendees, some had been to the first open house, but the majority were participating for the first time. Drafts of potential pedestrian improvement projects were presented. The proposed pedestrian improvements included adding sidewalks and trails, and were developed based on open house and online feedback.

Using the same format as the first meeting, two interactive exercises were held as detailed in the following paragraphs. During the first exercise, attendees participated in a voting exercise. Each attendee was provided with three sticker dots and asked to place the dots next to the projects they felt should be highest priority. Prior to the exercise, it was explained to participants that this would be one of several ranking criteria that would be included in the scoring matrix. The top priority projects identified through the voting exercise were:

1. Crosswalk on Rezanof Drive (7 votes)
2. Connection from Lower Erskine Avenue to Rezanof Drive (4 votes)
3. Sidewalk on Egan Way (4 votes)
4. Wayfinding (4 votes)

For the second exercise attendees were asked to write down any issues or concerns regarding the draft Overall Schematic Map depicting all the potential projects, as shown on Figure 7. General feedback included:

- Safety concerns regarding crossing Rezanof Drive
- Limited right-of-way along Marine Way might make it hard to provide a pedestrian facility (route outside of Study Area)
- Safety concerns for pedestrians and bike riders along Mission Road
- Better wayfinding needed, especially to get tourists to the library

SUMMARY OF PLAN DEVELOPMENT

The Pedestrian Pathways Plan was developed using background research, inventory of existing conditions and the public involvement processes.

Steps in developing the Plan included:

1. Site Inventory: provided a baseline understanding of the opportunity and constraints within the Study Area.
2. Site Walk: site observations and seeing socials trails indicated areas of need. Those observations were shared at the first open house.
3. First Open House: the public provided feedback on areas to for the planners focus on and what specific projects should be considered.
4. Feedback: feedback was used to establish a list of potential projects. The list was presented at the second public open house.
5. Second Open House: feedback from the second open house was used to develop the final list of recommendations.

Each step contributed to the final list of recommendations.




  	
<h1>Vote for Your Top Projects</h1> <p>Place dot next to projects you feel should be considered highest priority</p>	
Library Connections	
L1	Sidewalk on Egan Way ★★★★★
L2	Trail from Erskine Ave. to Library ★★
L3	Trail from Rezanof Dr. to Erskine Ave.
L4	Widen Sidewalk on Rezanof Dr.
L5	Rezanof Crosswalk Improvements ★★★★★★
Bridge Connections	
B1	Connection from Lower Erskine Ave. to Rezanof Dr. ★★★★★★
B2	Pave Sidewalk on Rezanof ★★
Mission Connections	
M1	Sidewalk on Mission Road ★★
M2	Connection from Mission to Bridge Connection
Waterfront Connections	
W1	Connection from Mission to Marine Way (Under Bridge) ★★★★★
W2	Sidewalk on Marine Way From Center Ave. to Pearson Cove
Other Opportunities	
O1	Branding
O2	Wayfinding ★★★★★
O3	Photo Opportunities
O4	Park at Mill Bay & Kashevaroff
O5	Historic Core Area
Other Projects Not Shown <small>Write note next to dot.</small>	

Figure 8: Concept projects and votes from the second open house.

Public Comments From Open House #2

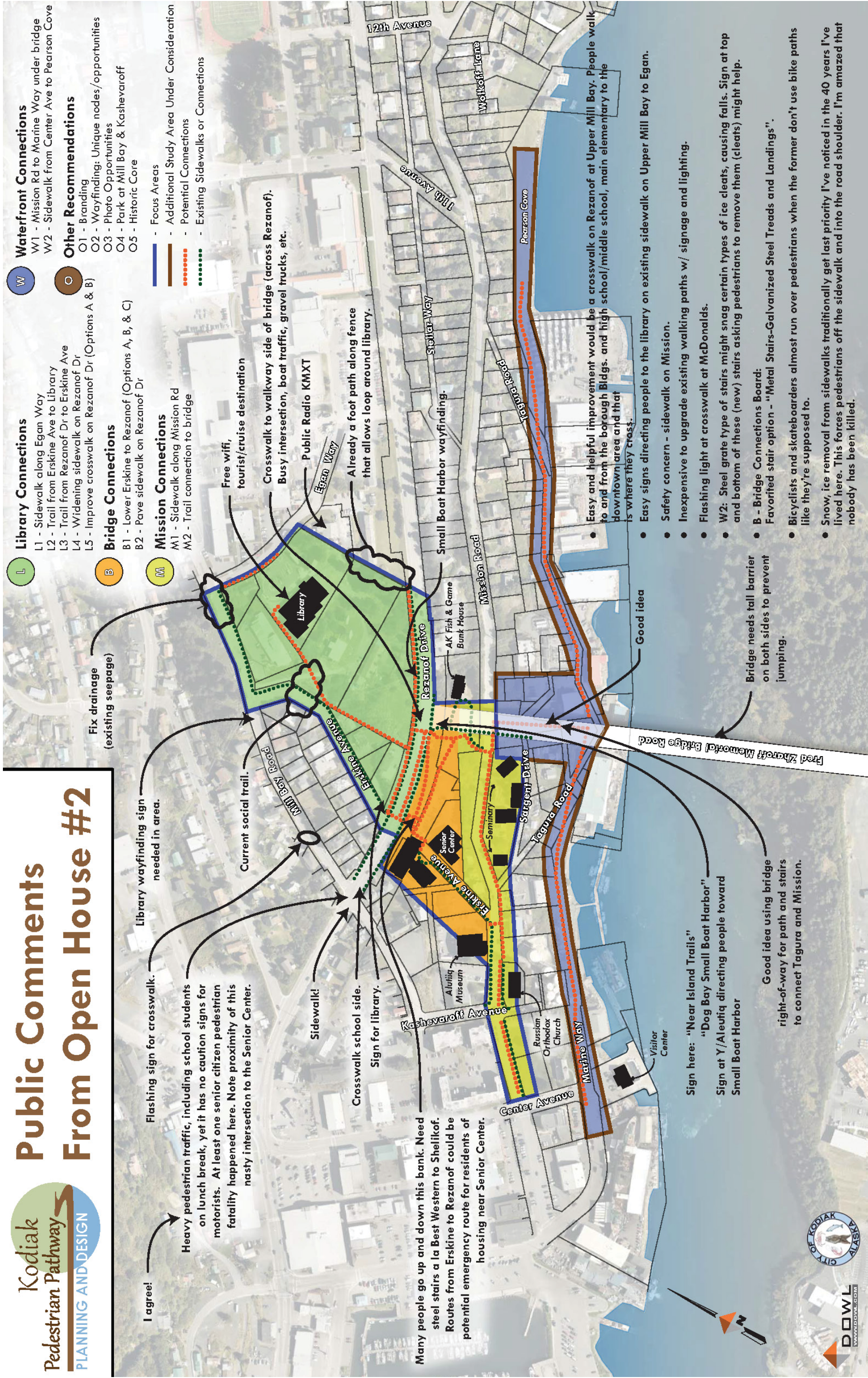


Figure 9: Consolidated graphic comments from the second open house.

SECTION III: RECOMMENDATIONS

Summary of Recommendations

The Overall Schematic Plan (Figure 9) shows the network of proposed pedestrian improvement projects in the Study Area. The Study Area is broken down into four smaller focus areas. Within each area is a list of potential projects. Potential projects include:

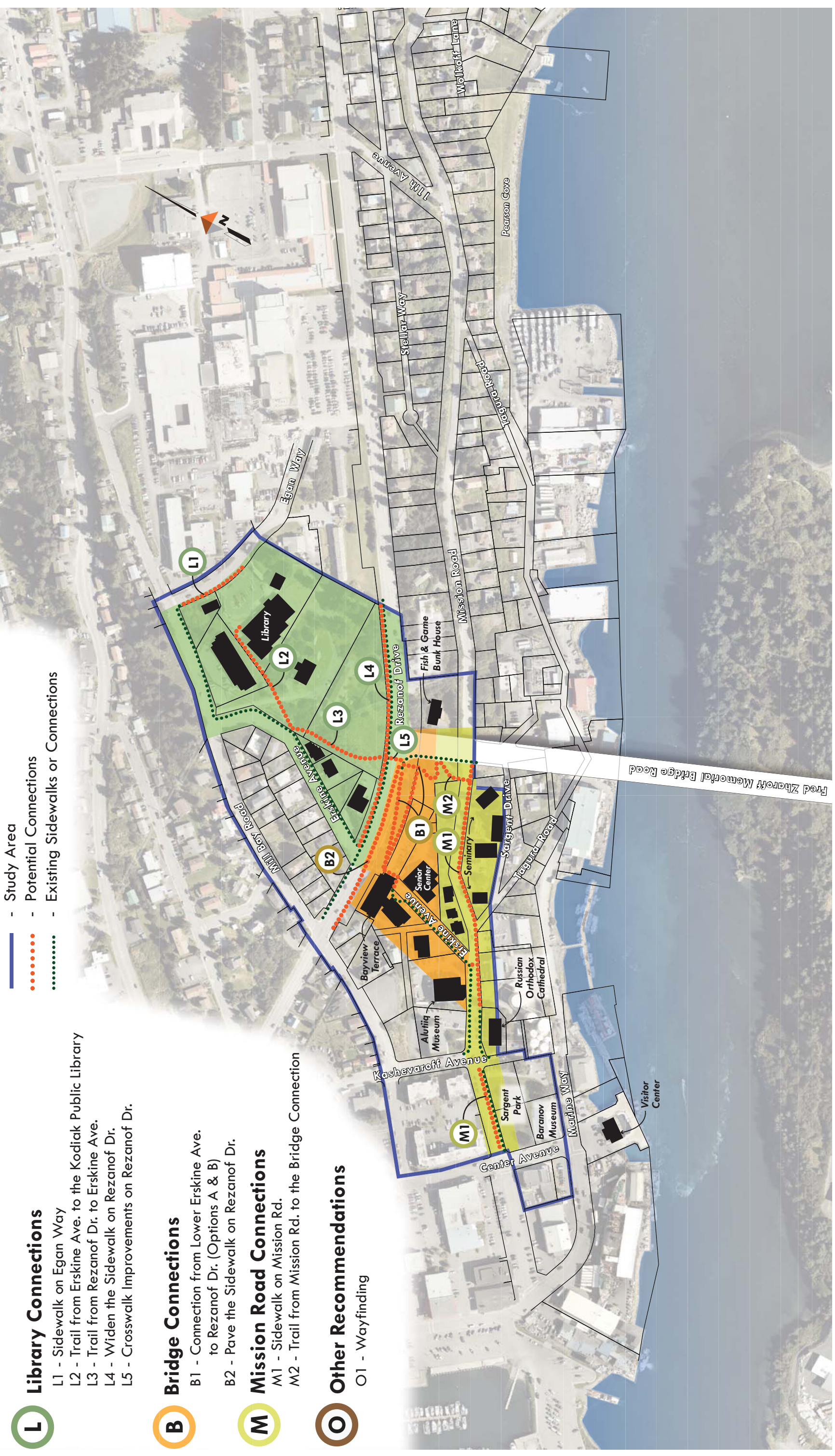
- L** Library Connections
 - L1 Sidewalk on Egan Way
 - L2 Trail from Erskine Avenue to the Kodiak Public Library
 - L3 Trail from Rezanof Drive to Erskine Avenue
 - L4 Widen Sidewalk on Rezanof Drive
 - L5 Crosswalk Improvements on Rezanof Drive
- B** Bridge Connections
 - B1 Connection from Lower Erskine Avenue to Rezanof Drive
 - B2 Pave sidewalk on Rezanof Drive
- M** Mission Road Connections
 - M1 Sidewalk Along Mission Road
 - M2 Trail from Mission Road to Bridge Connection
- O** Other Opportunities
 - O1 Wayfinding

The goal of the Plan is to illustrate projects that provide a network of pedestrian links that allow people to move safely and efficiently to points of interest within the Study Area. Each project is described in more detail in the following sections.



Figure 10: Isometric sketch of the Study Area and recommended projects.

Overall Schematic



L Library Connections

- L1 - Sidewalk on Egan Way
- L2 - Trail from Erskine Ave. to the Kodiak Public Library
- L3 - Trail from Rezanof Dr. to Erskine Ave.
- L4 - Widen the Sidewalk on Rezanof Dr.
- L5 - Crosswalk Improvements on Rezanof Dr.

B Bridge Connections

- B1 - Connection from Lower Erskine Ave. to Rezanof Dr. (Options A & B)
- B2 - Pave the Sidewalk on Rezanof Dr.

M Mission Road Connections

- M1 - Sidewalk on Mission Rd.
- M2 - Trail from Mission Rd. to the Bridge Connection

O Other Recommendations

- O1 - Wayfinding

- Study Area
- Potential Connections
- Existing Sidewalks or Connections

Figure 11: Overall schematic plan.

Project Ranking Criteria

It is unlikely that funding will be available to complete all the proposed projects at one time. Therefore the projects have been ranked to provide guidance regarding projects with the highest use or are the quickest to implement. The ranking categories include: use intensity, cost of effort, level of effort, public input, and maintenance. The ranking criterion used has been adopted and modified from the 2011 Kodiak Road System Trails Master Plan. Each project is assigned a score within each category and scores are totaled to rank each project. Highest scores equaling top priority.

Below is an outline of the scores within each ranking criteria. The outline describes how each criteria was weighed, and a description of each criteria. Scores are based on observations and professional judgment, therefore the scoring was subjective.

The goal of ranking projects is not to create a rigid order of how projects need to be developed, but to provide an idea of what project might be high impact or quick to implement. All projects should be considered valuable improvements that contribute to the overall pedestrian network. Given the effort, needed cooperation, and funding any project can be developed no matter the recommended order set by this scoring system.

RANKING CRITERIA:

Use Intensity

An estimate of how frequently the connection might be used by pedestrians based on observations, public feedback, and surrounding amenities. The goal is to better provide pedestrian connections for cruise ship

passengers, therefore a project that directly connects to a tourist destination would have a higher rank than a project that does not. Projects were also ranked high if there were signs of high-use, such as social trails (informal dirt trails that were not built or legally designated, but became distinct overtime by repeated use).

Design and Construction Costs

Preliminary cost estimates for the design and construction were completed for each project based on information observed on-site and on a very preliminary design concept of what the project might entail. No surveys of existing conditions, utilities, or topography were conducted to develop these estimates. The projects were divided into three costs categories:

1. Low: less than \$200,000
2. Moderate: \$200,000 – \$400,000
3. High: more than \$400,000

Level of Effort

This is an estimate of the time required to complete a project based on factors such as property acquisition, design, and construction. For example, a pedestrian project in COK right-of-way of relatively short length would receive a high score since no property acquisition is required and the design and construction requirements would be minimal. A project in DOT&PF right-of-way would require a longer time-frame due to the permitting processes required. A project on private or federal property could potentially have the longest time frame due to the acquisition process.

Public Input

At the second Open House, attendees voted for the projects they thought were the highest priority. Projects with four or more votes received the highest score, project with 2-3 votes received the median score and projects with no votes received the lowest score.

Maintenance

The impact of a project on city maintenance is an important factor in understanding the long term financial impacts of a project. Impacts to the maintenance budget were estimated based on whether the project was an

extension of existing maintenance practices or whether it would create new areas to maintain. A project that is an extension of existing sidewalks would be anticipated to be low maintenance; whereas a project outside of existing sidewalk and trail infrastructure, such as adding stairs, would be anticipated as a high impact on maintenance requirements.

Project Description & Scoring Tables

This section includes enlarged figures showing the four focus areas and two tables for each of the ten recommended projects. The first table is a 'Project Description' table that outlines:

- project description,
- purpose and need (this describes how each project was derived, whether from previous plans, site observations, public feedback, or a combination),
- design considerations,
- design and construction costs, and
- other comments.

The second table is a 'Project Scoring Table'. This outlines the scores assigned to each project based on the criteria outlined in the 'Project Ranking Criteria' section. The rankings have been assigned across the full suite of 10 projects.

The projects are grouped as follows:

- L: Library Connections (5 projects);
- B: Bridge Connections (2 projects);
- M: Mission Road Connections (2 projects); and
- O: Other Opportunities (1 project).

The purpose of providing the proposed projects in this format is to have descriptions for each project that can be used alongside the enlarged figures as a tool:

- to seek funding,
- for assistance with permitting processes,
- to assist with further studies, and
- for property acquisition discussions.

Ranking Criteria	Rank	Description	Weighted Multiplier	Score
Use Intensity	2	High	3	6
	1	Moderate		3
	0	Little		0
Design & Construction Costs	2	Low (<\$200,000)	3	6
	1	Moderate (\$200,000 - \$400,000)		3
	0	High(>\$400,000)		0
Level of Effort	2	Short time frame: COK right-of-way	2	4
	1	Moderate time frame: COK right-of-way/ property, DOT & PF right-of-way, or a short segment of private property		2
	0	Long time frame: private or federal property		0
	2	Highest scores at 2nd Open House voting (4+ votes)		1
Public Input	1	Second highest scores at 2nd Open House voting (2-3 votes)	1	1
	0	No votes at the 2nd Open House		0
	2	Low maintenance		2
Maintenance	1	Moderate maintenance	1	1
	0	High maintenance		0

Table 4: How ranking criteria is scored and weighted.

Library Connections

Legend

- Parcels
- Contours (10 foot)
- Existing Sidewalks
- Potential Connections (within City of Kodiak ROW or Property)
- Potential Connections (not in City of Kodiak ROW)
- Potential Crosswalk (DOT&PF ROW analysis needed)

Potential Projects

- L1** **Sidewalk on Egan Way**
 - Paved sidewalk
- L2** **Trail from Erskine Ave. to Library**
 - Gravel trail
 - Pedestrian lighting potentially
- L3** **Trail from Rezanof Dr. to Erskine Ave.**
 - Gravel trail
 - Pedestrian lighting recommended
- L4** **Widen Sidewalk on Rezanof Dr.**
 - Paved sidewalk
- L5** **Crosswalk Improvements**

Feasibility and location will depend on analysis

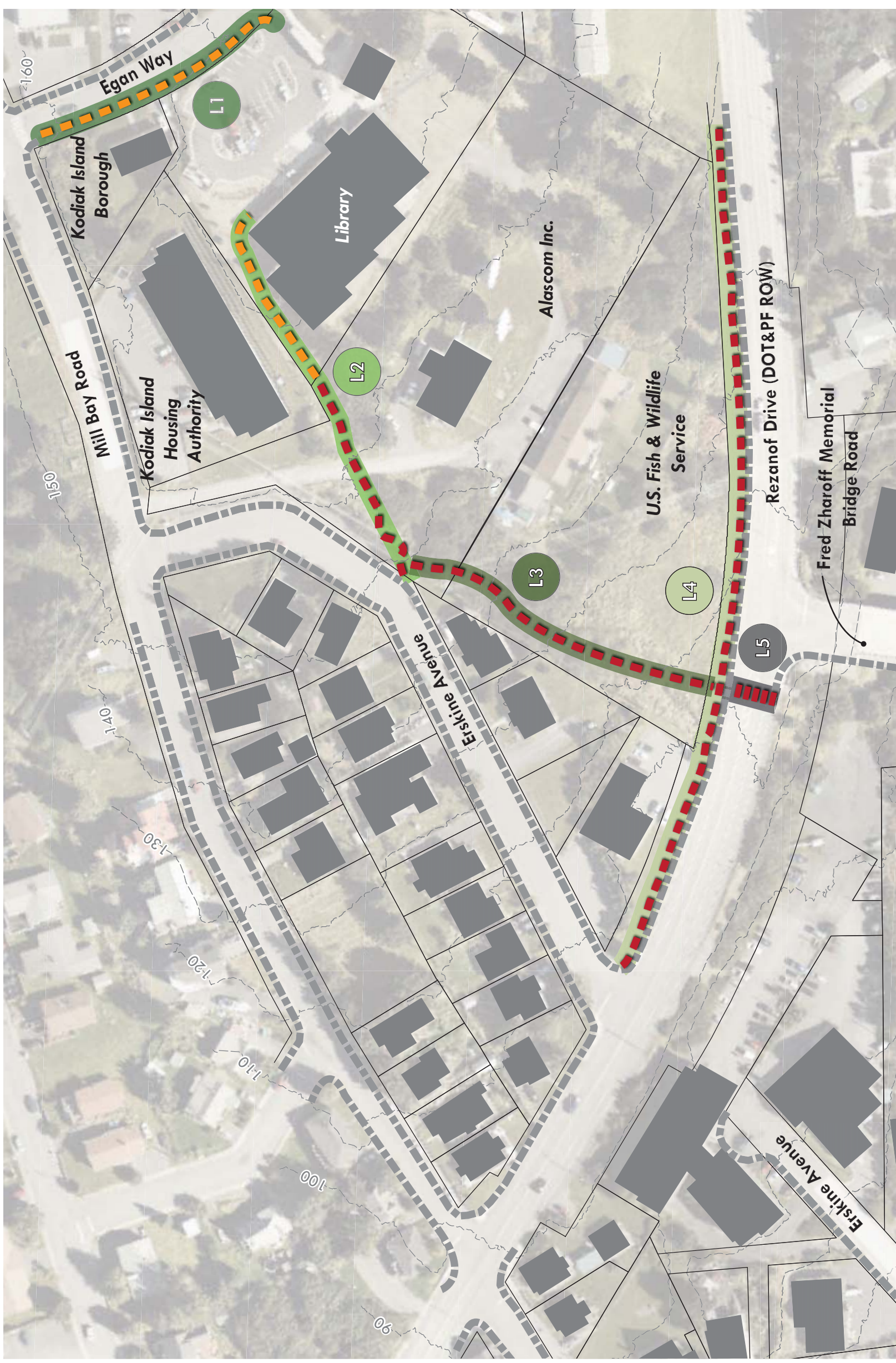


Figure 12: Library connections.

Project Description	
Project Description	A paved sidewalk along Egan Way in front of the Kodiak Public Library.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> This project was included because of on-site observations: no existing sidewalk from Mill Bay Road to the library property and social trails indicate people walk this route regularly. This received the second highest votes at the 2nd Open House
Design Considerations	<ul style="list-style-type: none"> Property: Within COK right-of-way Width: 6 feet Slope: Align running slope with Egan Way slope. Cross slopes not to exceed 2%. Surface: Concrete with 6-inch curb. Length: Approximately 250 linear feet. Retaining wall may be required along the Library property where there is a steep side slope. Include a short set of steps at the corner of the property where pedestrians currently cut through; ensure this can be tied into a pedestrian route on property. Include accessible curb-cut ramps.
Design & Construction Costs	Construction Cost: \$200,000 Design & Survey: \$50,000 Total: \$250,000 Assumptions: <ul style="list-style-type: none"> No additional right-of-way required. Existing curb and gutter is in serviceable condition and will be left in place. Existing storm drain facilities adequate. No new major landscaping elements.
Other Comments	<ul style="list-style-type: none"> Consider other sidewalk improvements along Egan outside of this Study Area. Public comments indicated there are drainage issues along the sidewalk on Mill Bay Road near where this proposed sidewalk would tie-in (specifically “seepage”) - investigate any drainage issues during design.

Table 5: Project L1 Description



Image 19: Egan Way looking northwest from the library driveway. Proposed sidewalk would be on the left side of the road in this image



Image 20: Library entry sign. Proposed Sidewalk would wrap right in front of sign.

Project Score				
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	High use is expected based on the existing social trail, and that it provides a direct connection to the library.	2	3	6
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000- \$400,000.	1	3	3
Level of Effort	Short time frame; the project falls within COK right-of-way and the segment of sidewalk is only 250 feet in length, therefore a short design and construction time period is anticipated.	2	2	4
Public Input	This project received four votes at the 2nd Open House.	2	1	2
Maintenance	This sidewalk will be an extension of existing pedestrian facilities; therefore low additional maintenance is expected.	2	1	2
Total Score				17
Project Rank				3

Table 6: Project L1 Score



Image 21: Egan Way looking southeast from the Mill Bay Road. Proposed sidewalk would be on the right side of the road in this image

Project Description

Project Description	Trail from the bend in Erskine Avenue to the Kodiak Public Library.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Improving this area would provide an alternative route to the library creating more direct access. Feedback at 1st Open House: <ul style="list-style-type: none"> Route G' (representing an alternative trail to the library) received the second highest votes. The public depicted this layout on several of the diagrams. On-site Observations: <ul style="list-style-type: none"> This route has a social trail indicating that it is well traveled. This route provides scenic views of the downtown waterfront and mountains beyond.
Design Considerations	<ul style="list-style-type: none"> Property: Beginning at Erskine Avenue the trail would start in COK right-of-way, cross private property (Alascom Inc.), then be on COK property (library). Access length across the Alascom Inc. property is for approximately 250 linear feet. Width: 6 feet. Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations. Surface: Compactable gravel (meeting accessible trail standards), or asphalt. Length: Approximately 425 linear feet. Consider lighting for improved security (may not be feasible on private property but possibly on library property). Signage directing people to the trail.
Design & Construction Costs	<p>Construction Cost: \$135,000</p> <p>Design & Survey: \$40,000</p> <p>Total: \$175,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Assumed 6-foot gravel surface. Three luminaires on public property and within COK right-of-way. Stop signs at driveway crossing. Private property access costs not included.
Other Comments	The trail crosses private property (Alascom Inc.) therefore an access agreement will need to be acquired.

Table 7: Project L2 Description



Image 22: West side of library looking south. The existing social trail can be seen in the foreground. The proposed trail would follow the social trail south towards Erskine Avenue.

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This would provide an alternative route to the library; since routes to the library already exist, the trail would have steep sections, and the existing route passes a cultural destination (the Russian Cemetery) moderate use is expected.	1	3	3	
Design & Construction Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6	
Level of Effort	Moderate time frame; this time frame is an estimate based on the need to coordinate private property access. Since it is a short segment of trail a short design and construction time line is anticipated.	1	2	2	
Public Input	This project received two votes at the 2nd Open House and was discussed with several attendees at the 1st Open House.	1	1	1	
Maintenance	This is outside of existing right-of-way and would have moderate slopes; therefore higher additional maintenance is expected.	0	1	0	
Total Score					12
Project Rank					6

Table 8: Project L2 Score

Project Description	
Project Description	Widen the existing sidewalk on Rezanof Drive to an 8-foot wide paved sidewalk.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Mentioned in the 2002 Community Design Workshop Final Report. Widening this sidewalk would provide for a more comfortable walking experience and allow larger groups of pedestrians, such as tourists, to walk together.
Design Considerations	<ul style="list-style-type: none"> Property: DOT&PF right-of-way (may require private property depending on how the right-of-way and sidewalk align). Width: 8 feet (6 feet minimum). Slope: Align running slope with Rezanof Drive slope. Cross slopes not to exceed 2%. Surface: Concrete with 6-inch curb. Length: Approximately 820 linear feet.
Design & Construction Costs	<p>Construction Cost: \$320,000</p> <p>Design and Survey: \$65,000</p> <p>Total: \$385,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Does not include right-of-way acquisition costs. Remove and replace existing sidewalk. Potentially replace 25% of existing curb and gutter. Sidewalk retaining wall required for approximately half of full length (due adjacent steep slopes and since offset from right-of-way is currently unknown).
Other Comments	<ul style="list-style-type: none"> May require grading outside of right-of-way or retaining walls along some sections. Public comments indicated there are drainage issues along the hillside adjacent to this sidewalk (specifically “seepage”) - investigate any drainage issues during design. This recommendation is for widening the portion of sidewalk on Rezanof Drive that falls within the Study Area of this plan. Consider widening the sidewalk on Rezanof Drive along other sections of road outside of this Study Area. For example the 2002 Community Design Workshop Final Report recommends widening the Rezanof sidewalk from Lower Mill Bay Road past the Erskine Avenue intersection. Also consider sidewalk widening on the south-side of Rezanof Drive outside of this Study Area.

Table 11: Project L4 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This is an area that tourists and locals currently walk and provides a connection from neighborhood areas to the bridge and library, therefore moderate pedestrian traffic is expected.	1	3	3	
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000-\$400,000.	1	3	3	
Level of Effort	Long time frame; due to the DOT&PF permitting and the potential coordination with private property.	0	2	0	
Public Input	This project received no votes at the 2nd Open House.	0	1	0	
Maintenance	This would be an improvement to an existing sidewalk therefore there would be little impact to current maintenance practices.	2	1	2	
Total Score				8	
Project Rank				10	

Table 12: Project L4 Score



Image 24: West side of Rezanof Drive looking north.



Image 25: West side of Rezanof Drive looking south.

L5 Crosswalk on Rezanof Drive



Image 26: Pedestrians crossing Rezanof Drive to access the bridge to Near Island. This is where one of the proposed crosswalks would be located.

Project Description	
Project Description	Install striped and signed crosswalk on Rezanof Drive near the bridge to Near Island.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Crossing Rezanof Drive can feel unsafe due to the width of the road, the speed of the cars, and sight distance issues along some sections; this sentiment was reiterated by the public at both open houses. Voted the top priority project at the second open house.
Design Considerations	<ul style="list-style-type: none"> Property: DOT&PF right-of-way. Width: 10 feet (as recommended by DOT&PF). Surface: White paint on existing asphalt. Length: Approximately 45 linear feet. Signage as warranted. An analysis is needed to determine whether a cross-walk at this location meets the standard criteria.
Design & Construction Costs	<p>Construction Cost: \$60,000</p> <p>Engineering: \$40,000</p> <p>Total: \$100,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Assumes warrant analysis of \$30,000 as a part of engineering costs. No flashing beacon assumed in construction costs. No right-of-way or utility issues.
Other Comments	Pedestrians also cross Rezanof Drive at Erskine Avenue and Mill Bay Road. The crossing at Mill Bay Road currently has a pedestrian crossing sign for east bound traffic as seen in the image on the right. These crossings did not appear to have appropriate sight distances for pedestrians to cross safely. Any crosswalk study should analyze the conditions at these crossings as well. The proper location and signage of a crosswalk will need to be determined through full analysis.

Table 13: Project L5 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	High use is expected based on observations of people crossing and comments received at both open houses.	2	3	6	
Design & Construction Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6	
Level of Effort	Short time frame; the warrant analysis is expected to take approximately 3 months and a short design and construction time frame is expected due to the small amount of improvements.	2	2	4	
Public Input	This project received 7 votes at the 2nd Open House.	2	1	2	
Maintenance	Little additional maintenance is expected, this may include restriping.	2	1	2	
				Total Score	20
				Project Rank	1

Table 14: Project L5 Score



Image 27: As noted in the "Other Comments" row there is a pedestrian sign for crossing Rezanof Drive at the intersection of Mill Bay Road. But as seen in this image the crest in the hill above this crossing makes it hard to see oncoming traffic.

Potential Projects

Connection from Lower Erskine Ave. to Rezanof Dr.

Two Potential Options:

Option will depend on cost of effort, public feedback, and coordination with DOT&PF

Option A B1-A Sloped Walkway

- Paved trail
- Sloped trail with rest spot mid-way
- Fill and retaining wall as needed
- Design coordination with DOT&PF along existing road bed

Option B B1-B Sidewalk & Stair Near Bridge

- Approximately 20-foot tall stair with multiple landings
- Enters U.S. Fish & Wildlife Property / DOT&PF ROW

B2

Pave Sidewalk on Rezanof

- Paved sidewalk along existing shoulder

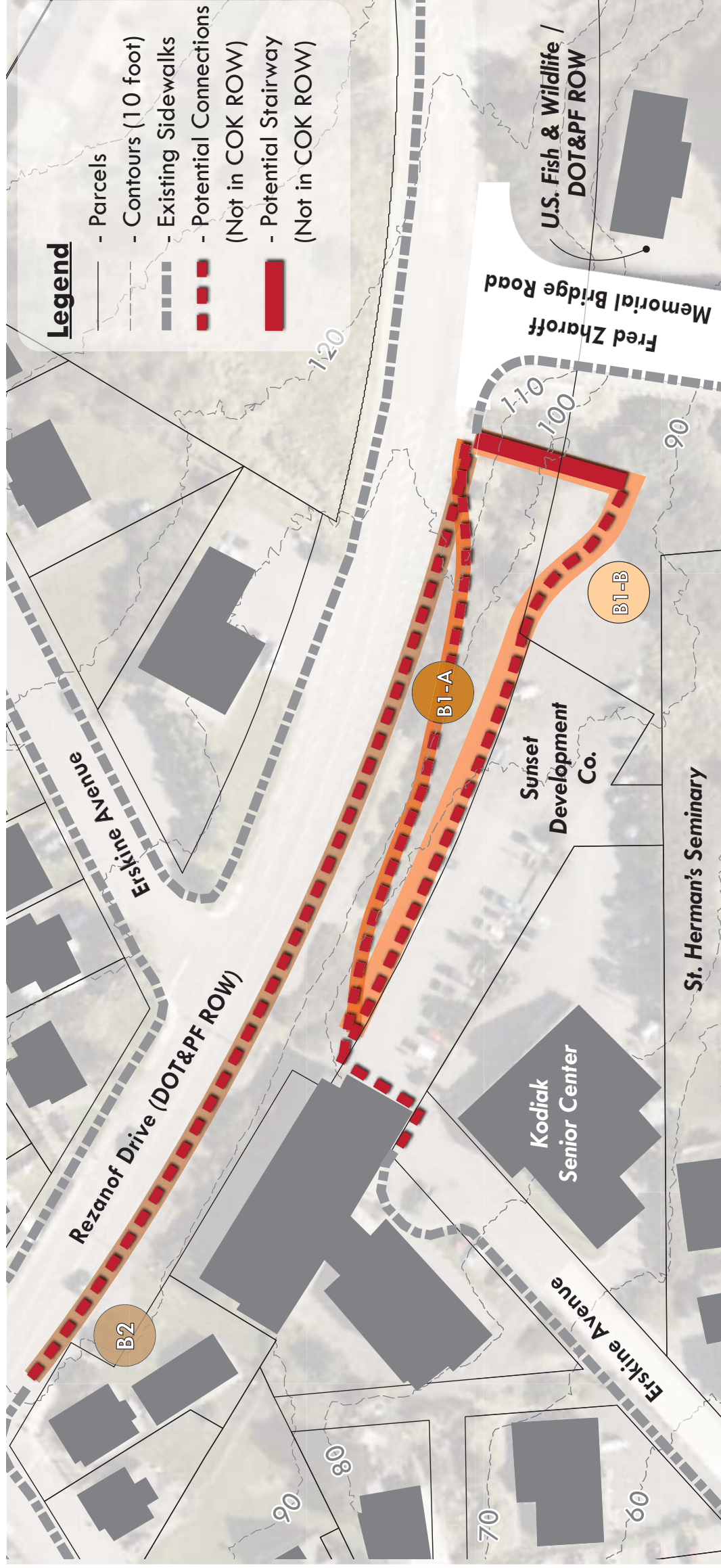


Figure 13: Bridge connections.

Project Description	
Project Description	A pathway (B1-A) or stairway (B1-B) connecting the dead end of lower Erskine Avenue to the bridge to Near Island.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Identified as a project in the 2002 Community Design Workshop Final Report (but as a stairway on the northeast side of the bridge). Several social trails exist in this area. Would provide an alternative trail to the bridge, which passes by a tourist destination (Alutiiq Museum). Is a vantage point providing views of the waterfront and downtown.
	<p>General</p> <ul style="list-style-type: none"> Incorporate wayfinding and interpretive signage at vantage points. Either option will need to be permitted by DOT&PF. Protect large trees to extent feasible. Provide adequate lighting. <p>B1-A Sloped Pathway</p> <ul style="list-style-type: none"> Property: Private property (Sunset Development Company), and DOT&PF right-of-way. Width: 6 feet. Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations. Surface: Paved with asphalt. Length: 460 linear feet consisting of 72 feet crossing private property and 388 feet of sloped trail (based on an average 6% slope) in DOT&PF right-of-way. <p>B1-B Stairway</p> <ul style="list-style-type: none"> Property: Private property (Sunset Development Company), DOT&PF right-of-way, and U.S. Fish & Wildlife Service property on which DOT&PF is a permitted use for operation and maintenance of the bridge to Near Island. Width: Trail segment: 6 feet; Stairs segment: 8 feet minimum Slope: Wherever feasible keep the running slope less than 5%; otherwise follow accessible trail design recommendations. Follow standard riser-to-tread ratios for outdoor stairways. Minimum riser height: 4.5 inches, maximum riser height: 7 inches. Minimum tread length 11 inches. Provide landings every 9-11 risers. Surface: Potential options include: open grate, concrete, or wood. Open grate or concrete will be longer lasting. Length: 460 linear feet of flat trail; 20-foot tall stairway with 6-inch risers and landings every 10 steps. If using open grate steps, provide a sign warning people walking with strap-on cleats to remove them. Consider a covered stairway to reduce long term maintenance. Provide a channel for bicyclists to walk their bike up and down the steps. Stairway could be shortened by elevating the trail to the base of the stairway.
Design Considerations	

	<p>B1-A Sloped Sidewalk</p> <p>Construction Cost: \$460,000 Design & Survey: \$80,000 Total: \$540,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Assumed existing ground at a 2 to 1 slope. Assumed use of cast in place sidewalk retaining wall (modular block wall was investigated, but appeared more costly). This will be difficult to construct, as 2 to 1 embankment will require extensive excavation to build retaining wall and pathway structural fill. Four feet of concrete pathway and four feet of asphalt pathway was assumed. This will be difficult to design and construct due to the existing steep embankment; stairs may be the best solution (and likely least cost). Does not include right-of-way or easement acquisition.
Design & Construction Costs	<p>B1-B Stairway</p> <p>Construction Cost: \$440,000 Design & Survey: \$80,000 Total: \$520,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Utilized a previous stairs design and estimate from 2004 and applied an inflation cost. The sidewalk along the existing parking area would be installed with concrete curb and gutter. This may be the more appropriate solution from an engineering standpoint due to challenges around constructing a sloped sidewalk on a steep embankment.
	<p>Other Comments</p> <ul style="list-style-type: none"> Two design options for this connection were included to show potential alternatives. Both have advantages and disadvantages; the sloped sidewalk is ideal for cyclists but may be more difficult to design and construct. The stairs are more direct but exclude wheelchairs and cyclists. Any scenario should be investigated for engineering feasibility and permitting potential with DOT&PF. For both scenarios the first section of sidewalk crosses private property (Sunset Development Co.) for approximately 52 feet of the property (for 72 linear feet of sidewalk); therefore any development will need to be coordinated with the property owner. The stair option will require crossing the U.S. Fish & Wildlife Service property on which DOT&PF is a permitted use and operation for the bridge to Near Island. DOT&PF should be coordinated with to confirm the required permitting, and if additional entities need to be notified.

Table 15: Project B1 Description



Image 28: West side of the bridge abutment.



Image 29: Approach to the west side of the bridge.

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This area has several social trails and provides views of downtown Kodiak and the waterfront, therefore high use is expected.	2	3	6	
Design & Construction Costs	This project estimate falls within the high range of costs: > \$400,000.	0	3	0	
Level of Effort	Long time frame; this will require DOT&PF permitting and the coordination with a private property owner.	0	2	0	
Public Input	This project received four votes at the 2nd Open House.	2	1	2	
Maintenance	This would require additional maintenance outside of the current infrastructure; therefore higher additional maintenance is expected.	0	1	0	
Total Score				8	
Project Rank				8	

Table 16: Project B1 Score



Image 30: Existing social trail to the bridge.

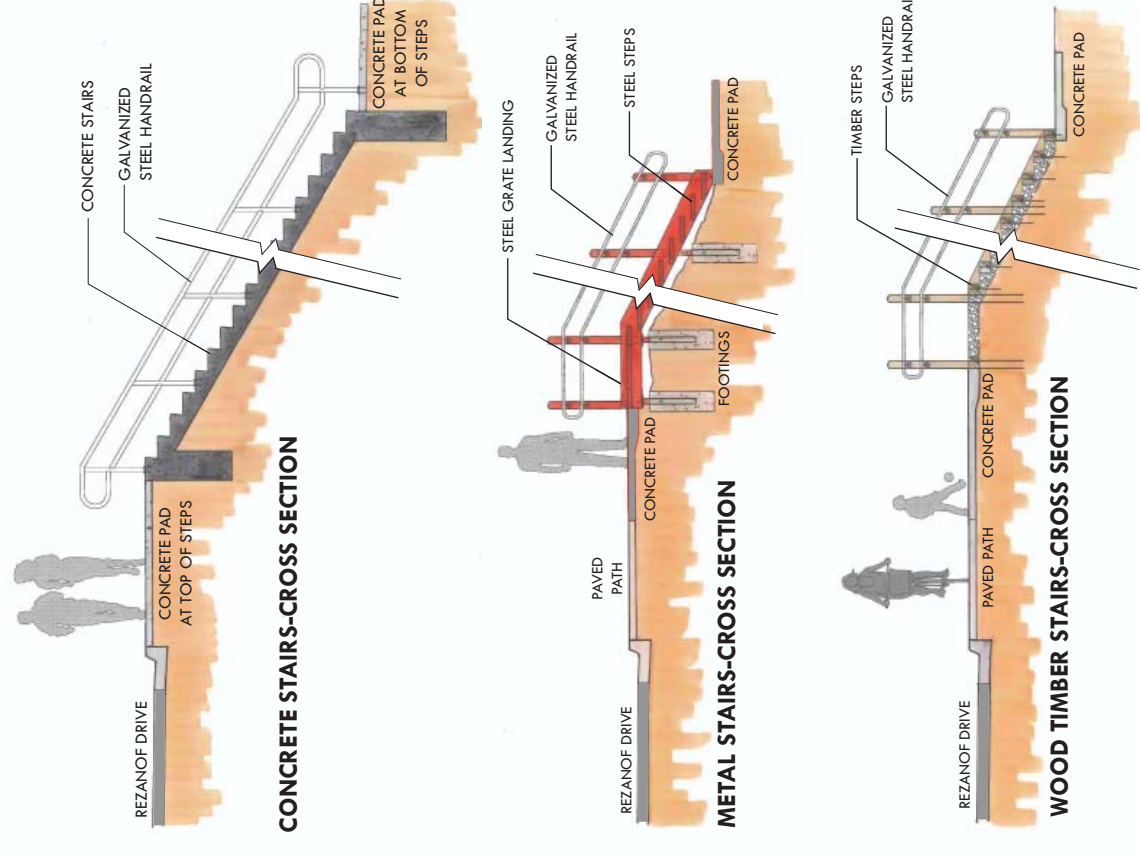


Figure 14: Sections of potential stair types including concrete, metal, and wood. Given the steep existing slope the concrete and wood stairs would require additional fill. The B1-B project design and construction cost estimate is based on an estimate completed in 2004 for a metal stair design.

Project Description	
Project Description	Pave the existing gravel shoulder on Rezanof Drive.
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Increases accessibility; a paved surface is more amenable to accessible means of travel than the current gravel surface.
Design Considerations	<ul style="list-style-type: none"> Property: DOT&PF right-of-way. Width: 6 feet minimum; feasible width will need to be determined based further investigations and coordination with DOT&PF. Slope: Align running slope with Rezanof Drive. Cross slope maximum 2% Surface: Paved with either concrete or asphalt. Length: Approximately 625 linear feet.
Design & Construction Costs	<p>Construction Cost: \$160,000 Design & Survey: \$50,000 Total: \$210,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Assumed 6-foot asphalt pavement and 2-foot gravel due to proximity of steep slope. May require handrail in certain locations due to proximity of steep slope (not included in estimate).

68

Table 17: Project B2 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	This is the current link between the downtown area and the access to the bridge, therefore high use is expected.	2	3	6	
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000-\$400,000.	1	3	3	
Level of Effort	Short time frame; this project would require DOT&PF permitting which would take time, but a short time frame for design and construction is expected since there is an existing gravel surface.	2	2	4	
Public Input	This project received two votes at the 2nd Open House.	1	1	1	
Maintenance	This would be an improvement within an existing roadway, therefore there would be little impact to current maintenance practices.	2	1	2	
Total Score				16	
Project Rank				4	

Table 18: Project B2 Score

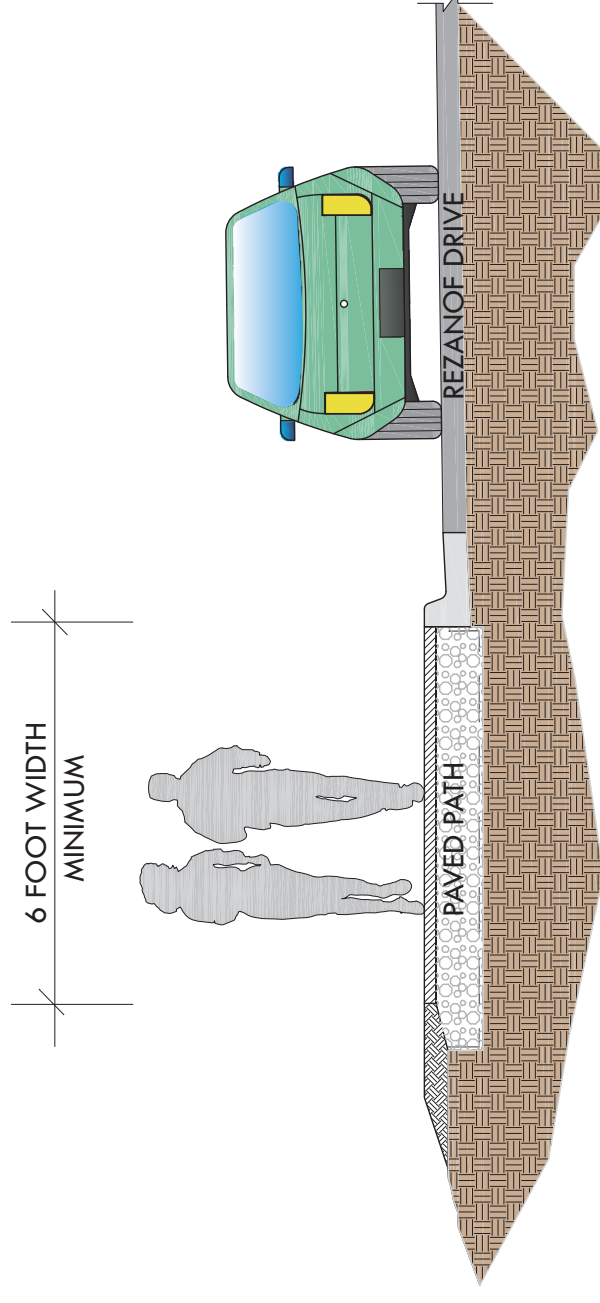


Figure 15: Rezanof Drive sidewalk section.



Image 31: Existing gravel edge along Rezanof Drive. View from the bridge looking west on Rezanof Drive.

Mission Road Connections

Legend

- - - Parcels
- - - Contours (10 foot)
- - - Existing Sidewalks
- - - Potential Connections (within City of Kodiak ROW or property)
- - - Potential Connections (not in City of Kodiak ROW)
- - - Potential Crosswalk (Depends on recommendations of an analysis)

Potential Projects

- M1**
Sidewalk on Mission Road
 - Widen existing sidewalk and add new paved sidewalk along Mission
- M2**
Connection from Mission to Bridge Connection
 - Gravel trail and/or stairs and retaining walls as needed
 - Pedestrian lighting recommended



Figure 16: Mission road connections.



Image 32: Cruise ship passengers walking down Mission Road near the bridge to Near Island.



Image 33: Existing conditions on Mission Road.

Project Description

Paved sidewalk along the south-side of Mission Road. Widen the existing sidewalk along Sargent Park, and construct new sidewalk from the Russian Orthodox Church to just under the bridge to Near Island.

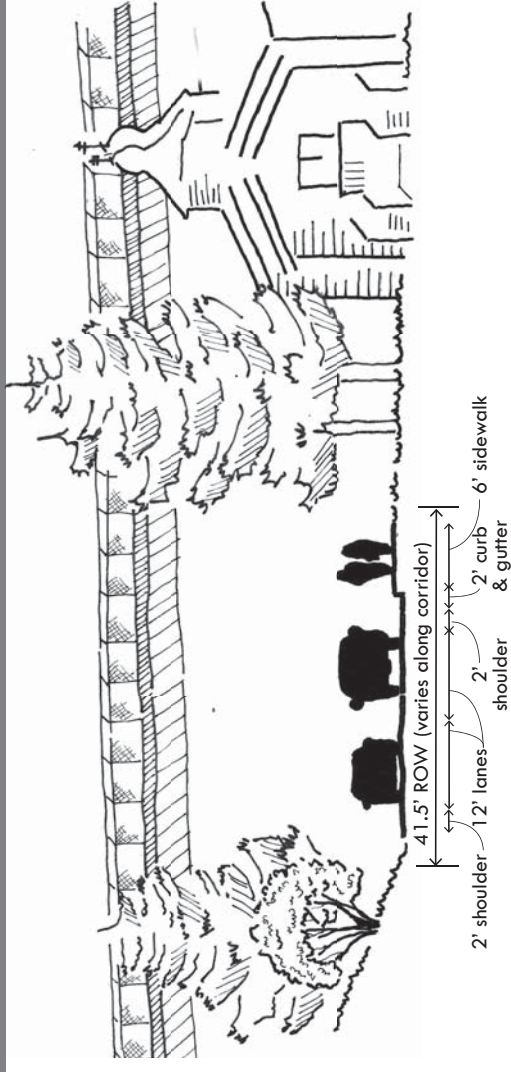
- Identified as an area for pedestrian improvements in previous plans: 2002 Community Design Workshop, 2011 Kodiak Road System Trails Master Plan.
- Social trails along roadway imply frequent pedestrian use.
- Observation of tourists and locals walking the area in the street corridor.
- Property: COK right-of-way, may require some right-of-way acquisition.
- Width: 6 feet minimum.
- Slope: Align running slope with Mission Road, cross slope maximum 2%.
- Surface: Paved with concrete and 6" curb.
- Length: 288 linear feet of widened sidewalk (4.5 feet to 8 feet); 800 linear feet of new sidewalk (6 feet wide).
- Potential short retaining wall (approximately 2-3 feet tall for up to 125 linear feet).
- Will need to accommodate driveways and potentially on-street parking across from Erskine Avenue.

Construction Cost: \$620,000
 Design and Survey: \$100,000
 Total: \$720,000

- Design & Construction Costs**
- Assumptions:**
- Does not include right-of-way acquisition costs.
 - Existing storm drain system to tie into with new curb and gutter.
 - May require some right-of-way acquisition and parking re-configuration at the properties west of Tagura Road.
 - Re-use existing curb and gutter along Mission Road.

Due to the scope of this study this recommendation shows the sidewalk terminating at the bridge; consider extending pedestrian improvements further along Mission Road. The 2011 Kodiak Road System Trails Master Plan includes creating a trail on Mission Road all the way out to Mission Beach.

Table 19: Project M1 Description



Mission Road Section

In front of St. Herman's Seminary facing the bridge to Near Island

Project Score

Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	High use is expected based on observed pedestrian traffic, and that this sidewalk will pass the St. Herman Catholic Seminary, a destination for locals and tourists.	2	3	6
Design & Construction Costs	This project estimate falls within the high range of costs: >\$400,000.	0	3	0
Level of Effort	Short time frame; the majority of the project is in COK right-of-way and would be along the edge of an existing roadway, therefore relatively short design and construction would be anticipated.	2	2	4
Public Input	This project received two votes at the 2nd Open House and was discussed with several attendees at the 1st Open House.	1	1	1
Maintenance	This sidewalk will be an extension of existing pedestrian facilities but is for a relatively long distance compared to other projects in this plan, therefore moderate additional maintenance is expected.	1	1	1
Total Score				12
Project Rank				5

Table 20: Project M1 Score

Project Description	
Project Description	Pathway connecting Mission Road to the improvements between lower Erskine Avenue and the bridge (see project: Lower Erskine Avenue to Bridge).
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> Existing social trails. Provides a route for tourists to walk down Mission Road then directly up to the bridge to connect to Near Island or to the library.
Design Considerations	<ul style="list-style-type: none"> Property: starts on COK right-of-way then continues in DOT&PF right-of-way, and U.S. Fish & Wildlife Service property on which DOT&PF is a permitted use for operation and maintenance of the bridge to Near Island. Width: 6 feet. Slope: Follow accessible trail design recommendations. Surface: Compactable gravel (meeting accessible trail standards), or asphalt. Length: 230 linear feet (based on an average slope of 7%-8%) – this would be the length required to meet the base of a stairway connecting to the bridge. Additional trail and/or stairs would be needed if a sloped sidewalk were developed from Erskine Avenue. May require short segments of retaining wall, potentially 100 linear feet long total to reduce the amount of cut. Recommended material: dry stack boulder wall. (i.e. rockery wall). Consider lighting for improved security. Provide crosswalk from sidewalk improvements on Mission Road if warranted.
Design & Construction Costs	<p>Construction Cost: \$260,000</p> <p>Design & Survey: \$50,000</p> <p>Total: \$310,000</p> <p>Assumptions:</p> <ul style="list-style-type: none"> Assumed use of class II Rip Rap for slope protection at 1.5 to 1 slope. Stairs, gabion baskets or concrete retaining wall may be a more suitable option. This will require fairly extensive excavation and re-grading of area. This will be difficult to construct if some right-of-way is not acquired from St. Herman’s Seminary, unless stairs are used. Asphalt pavement used in cost estimate for trail surface. Crosswalk on Mission Road is not included in estimate, as an analysis is needed. Cost to obtain easement or right-of-way not included.
Other Comments	<ul style="list-style-type: none"> The portion of improvements that cross U.S. Fish & Wildlife Service property, on which DOT&PF is a permitted use for operation and maintenance of the bridge, will need to be permitted by DOT&PF. This property is currently used as a State of Alaska Fish & Game Bunk House. It will be important to maintain separation between the existing driveway to the bunk house and the trail alignment.

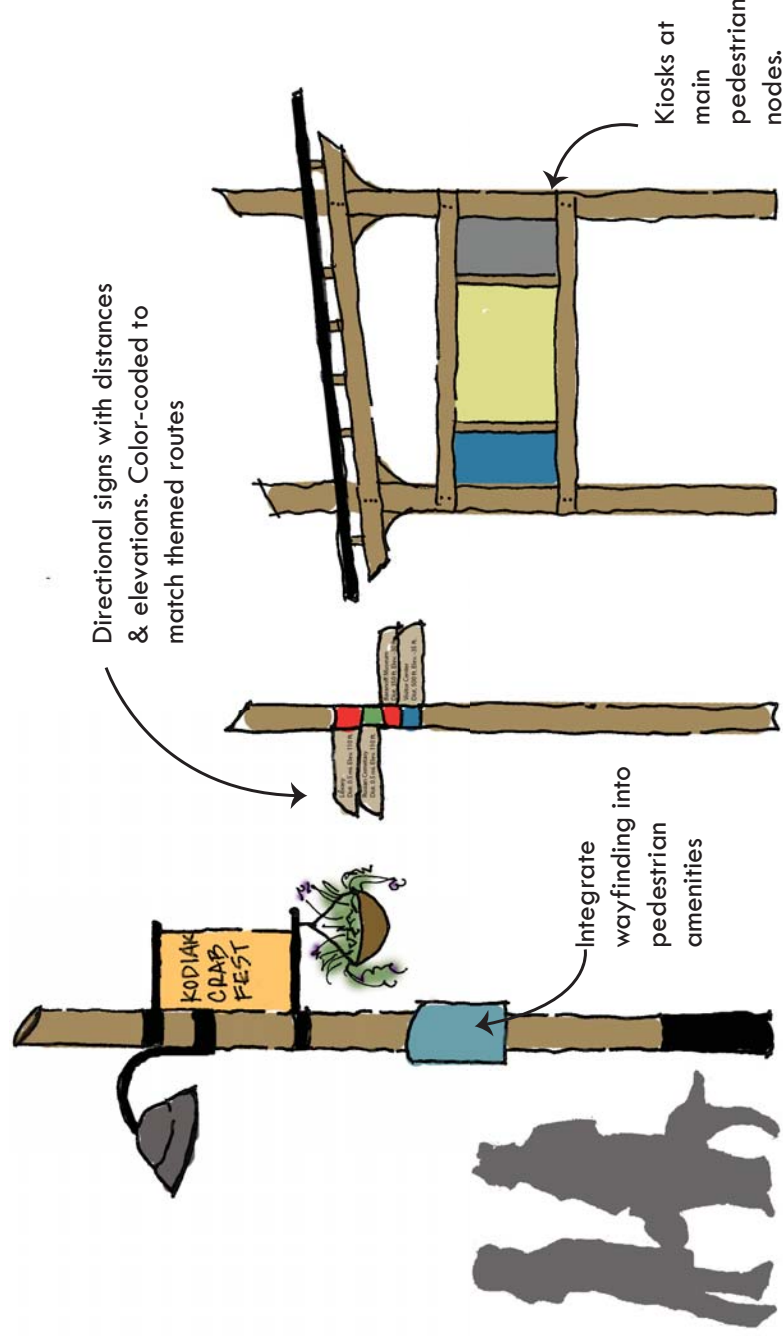
Table 21: Project M2 Description

Project Score					
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal	
Use Intensity	Based on social trails this area is used by pedestrians; moderate use is expected since there are other means of access to the bridge.	1	3	3	
Design & Construction Costs	This project estimate falls within the middle range of costs: \$200,000 - \$400,000.	1	3	3	
Level of Effort	Moderate time frame; this project would require DOT&PF permitting but since the length of trail is short, a shorter design time frame is anticipated.	1	2	2	
Public Input	This project received no votes at the 2nd Open House.	0	1	0	
Maintenance	This trail would require maintenance outside of existing right-of-way.	0	1	0	
Total Score				8	
Project Rank				9	

Table 22: Project M2 Score



Image 34: Existing driveway off of Mission Road. The driveway runs under the bridge to the ADF&G bunk house. The proposed trail would start here and switch back to the left and connect to a connection to the bridge.



72

Image Source: left: <http://nutshell.com.au/projects/shire-of-central-goldfields-interpretive-trail/>;
 middle: <https://www.smashingmagazine.com/street-and-wayfinding-signs-part-4/> ; right:
susanjweit.com (Monarch Spur Trailhead)

Above Figure 17: Other opportunities - wayfinding.
 Above Right Table 23: Project 01 Description
 Right Table 24: Project 01 Score

Project Description				
Project Description	Develop a wayfinding plan for the City of Kodiak, specifically for the core areas that tourists walk. A wayfinding system consists of directional signs that help locals and visitors orient themselves. They should be designed to reflect the community's aesthetic and character.			
Purpose & Need (For Tourist & Locals)	<ul style="list-style-type: none"> • Navigating the pedestrian routes from downtown to the bridge and library is not intuitive and the use of maps is required, wayfinding would help navigation significantly. • Several comments from the public stressed the need for improved wayfinding signage. • This project received four votes at the 2nd Open House. 			
Design Considerations	<ul style="list-style-type: none"> • Signs that give distances and elevations to destinations. • Are color coded based on themed routes (themed routes adopted from Discover Kodiak Map and updated to reflect projects as they get implemented). • Are subtle in design and use materials and styles reflective of the historic and contemporary character of the COK and the surrounding natural beauty. • Include nodes with larger contextual maps and interpretive signs, and directional arrow signs in-between destinations. • Signs should direct people to main attractions including the library and attractions on Near Island (including the small boat harbor). 			
Plan Costs	Wayfinding Plan: +/- \$100,000 Assumptions: <ul style="list-style-type: none"> • The wayfinding plan would be for the area from Pier 2, through downtown and up to the library. • This estimate is just for the planning portion of developing wayfinding, the costs of detailed design and construction would depend on the outcomes of the plan. 			
Other Comments	Consider doing a city branding plan prior to a wayfinding effort. A branding plan is a way for Kodiak to help identify and brand itself, it can be used for marketing, developing design standards, and developing wayfinding that matches and complements branding themes.			
Project Score				
Scoring Criteria	Scoring Analysis	Score	Multiplier	Subtotal
Use Intensity	Wayfinding is an amenity that can be used by visitors to navigate to areas of interest. This could be highly used by all types of visitors.	2	3	6
Plan Costs	This project estimate falls within the low range of costs: <\$200,000.	2	3	6
Level of Effort	Short time frame; developing the wayfinding plan could be done relatively quickly. The actual implementation would be a longer time frame.	2	2	4
Public Input	This project was within the highest group of votes at the 2nd Open House (4 total).	2	1	2
Maintenance	This would add a whole new set of signage that would need to be maintained from vandalism and general weathering; therefore higher additional maintenance is expected.	0	1	0
Total Score		18		
Project Rank		2		

General Considerations

Beyond the ten specific projects outlined in the previous section this section provides additional recommendations that should be considered. These are either:

- outside of the Study Area of this plan, but were included based on public feedback (waterfront connections, and pedestrian nodes);
- programming considerations, such as continued coordination with Discover Kodiak and cruise ships; or
- general considerations that apply to all or most of the ten specified projects, such as pedestrian design recommendations.

WATERFRONT CONNECTIONS

During the public involvement process feedback regarding potential pedestrian connections outside of the Study Area was provided. People were interested in pedestrian routes from:

- the bridge to Marine Way (under the bridge), and
- Center Avenue to Pearson Cove, a small park along the waterfront.

Both of these projects would provide great walking opportunities for visitors.

The connection under the bridge provides a direct pedestrian route from the bridge all the way to waterfront. This connection includes various scenic vantage points and would direct visitors to businesses along the waterfront.

The pedestrian connection along Marine Way would provide an opportunity for visitors to see the working waterfront with an easy, flat walk to Pearson Cove. The cove is a small park offering views of Near Island, boat activity, and marine life. Both improvements fall outside of this plan's Study Area, but should be considered for further study.

Potential Projects

-  - Concept Potential Connections
-  - Parcels

-  **W1**
Sidewalk on Marine Way From Center Avenue to Pearson Cove
 - 6 to 8-foot wide paved sidewalk

-  **W2**
Connection from Mission to Marine Way (Under Bridge)
 - Combination of stairs and trails to provide a pedestrian connection from Mission Road to Marine Way

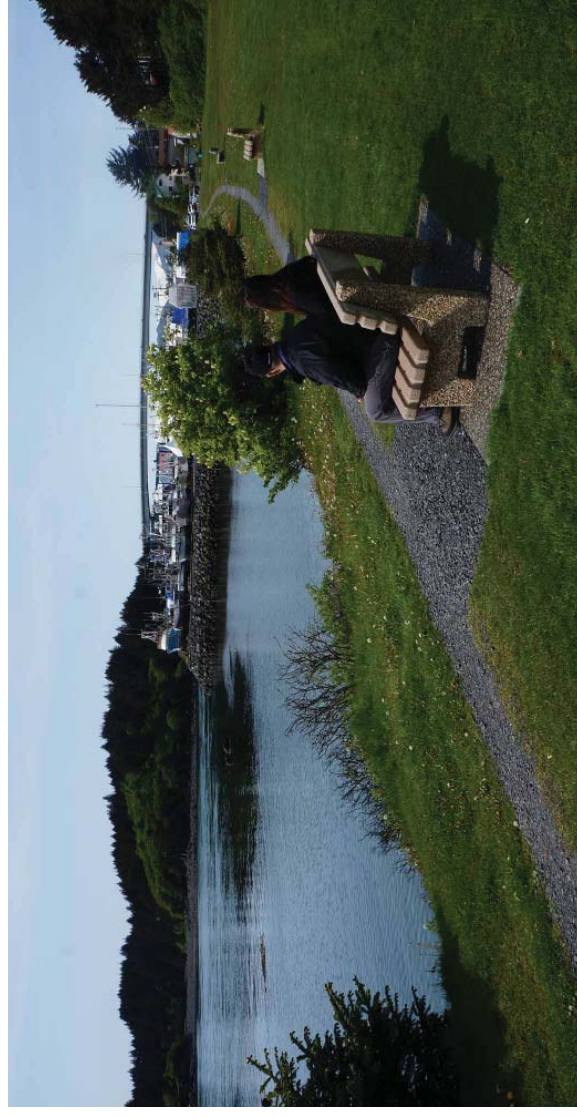


Image 35: Pearson Cove Park. View of waterfront looking towards Near Island.



Image 36: Marine Way, view looking northeast towards Pearson Cove.

Figure 18: Potential waterfront connections.

CONTINUED COORDINATION BETWEEN DISCOVER KODIAK & CRUISE LINES

Discover Kodiak is a nonprofit 501(c) 6 organization that works to promote tourism within the KIB and works directly with cruise-ship operators to provide visitor information to passengers. Discover Kodiak publishes a map of walking routes through downtown Kodiak. As connections and wayfinding infrastructure are developed, the maps should be updated to reflect changes. Figure 16 is the existing Discover Kodiak map which outlines themed routes, and Figure 17 shows how this map could be updated to reflect new routes that this plan outlines. The updates should also include information on the distance and elevation change of the various walks so that visitors can gauge their time and level of effort of the walk.

Local business owners expressed concern that cruise ship passengers are bussed directly from Pier 2 to destinations, thus missing opportunities to walk through the town and visit local businesses. Although Discover Kodiak passes out walking information at a booth at Pier 2 during docking, by the time the passengers disembark many have already determined their plans on shore. To help address this, Discover Kodiak should continue to work with the cruise ship companies to ensure passengers are aware of the walking opportunities that connect to shopping, scenic, and cultural destinations prior to debarking. As shown in Table 23, the 2016 cruise ship schedule, some boats are only docked for seven hours; this may not be sufficient time for activities outside of the city but could be enough to walk from Pier 2 to destinations in and around downtown.

PEDESTRIAN FACILITIES DESIGN RECOMMENDATIONS

Sidewalk and Trail Design Standards

Sidewalks and trails should be designed in a sustainable way that will minimize long term maintenance and degradation. Wherever possible, pedestrian pathways should be designed to accommodate multiple types of users (i.e., for walking, jogging, cycling); this may not be feasible where steep grades exist. In such cases Americans with Disabilities Act (ADA) accessible pathway

and stairway design standards should be followed. This includes providing level stopping points on long sections of steep trail and providing adequate handrails on any steps or stairways.

Sidewalk widths should vary based on expected pedestrian volume and the character of location. For example downtown sidewalks should be wider than sidewalks within residential areas. Typically, sidewalks should provide a five-foot minimum width; this would allow two people moving in opposite directions to pass



Figure 19: Discover Kodiak visitor map.

Table 25: Port of Kodiak, cruise ship schedule 2016

Vessel Name	PAX	Cruise Line	Length	Arrival Date	Day	Arrival Time	Dep Time	Location
Crystal Serenity	922	Crystal Cruise Lines	781	4/25/2016	Monday	8:00 AM	5:00 PM	P-2
Volendam	1,432	Holland America	781	4/28/2016	Thursday	8:00 PM	5:00 PM	P-2
Silver Shadow	382	Silver Seas	610	5/17/2016	Tuesday	8:00 AM	6:00 PM	P-2
Maasdam	1,898	Holland America	610	6/1/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	6/15/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	6/29/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	7/13/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	7/27/2016	Wednesday	7:00 AM	2:00 PM	P-2
Crystal Serenity	922	Crystal Cruise Lines	781	8/10/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	8/17/2016	Wednesday	7:00 AM	2:00 PM	P-2
Maasdam	1,898	Holland America	610	8/24/2016	Wednesday	7:00 AM	2:00 PM	P-2
Silver Shadow	382	Silver Seas	610	9/20/2016	Tuesday	9:00 AM	3:00 PM	P-2

Table 25: Port of Kodiak, cruise ship schedule, 2016.

each other. Wider sidewalks are desirable in areas where people are expected to walk in pairs or groups. Having wider sidewalks in areas with higher volumes of vehicle traffic provides pedestrians more separation so that they are not right next to traffic. All the sidewalk and pathway improvements recommended in this plan provide a six-foot minimum width for paths on routes that lead to destinations where visitors and locals are expected to walk in larger groups.

Several comments from the public noted issues with ice and snow on sidewalks. Icy sidewalks can create a hazard and cause pedestrians to walk in the streets. The City should maintain sidewalks as snow and ice free as possible. During site visits it was observed that cars parked on sidewalks with rolled curbs. Cars block pedestrians from using the sidewalk facilities. The city should enforce parking violations that create conflicts with pedestrian use of sidewalks and paths.

Create Pedestrian Amenity Nodes

Create pedestrian destination nodes for tourists. These could be specific points along pedestrian routes that have amenities such as benches, informative/

interpretive signs, and provide scenic views. A recommendation from Discover Kodiak is to create structures that frame views of Kodiak that visitors can take photos through. This element is adopted from the National Parks Service (NPS) “#FindYourPark” campaign in which frame structures are provided at tourist destinations offering a “framed” photograph opportunity for visitors.

Standardized Pedestrian Lighting

Pedestrian lighting can transform a street from a corridor just for cars to a comfortable route for pedestrians. Lighting at the pedestrian scale is both an aesthetic improvement and safety improvement. It enhances visibility between people and cars. Pedestrian lighting should be considered along pedestrian routes that are not along roadways (i.e., trails or stairways), and on sidewalks where street lights are not sufficient or that pedestrians frequently use. The COK should consider adopting a standard pedestrian scale light fixture and luminaire.

Benefits of having a standard style of lighting include:

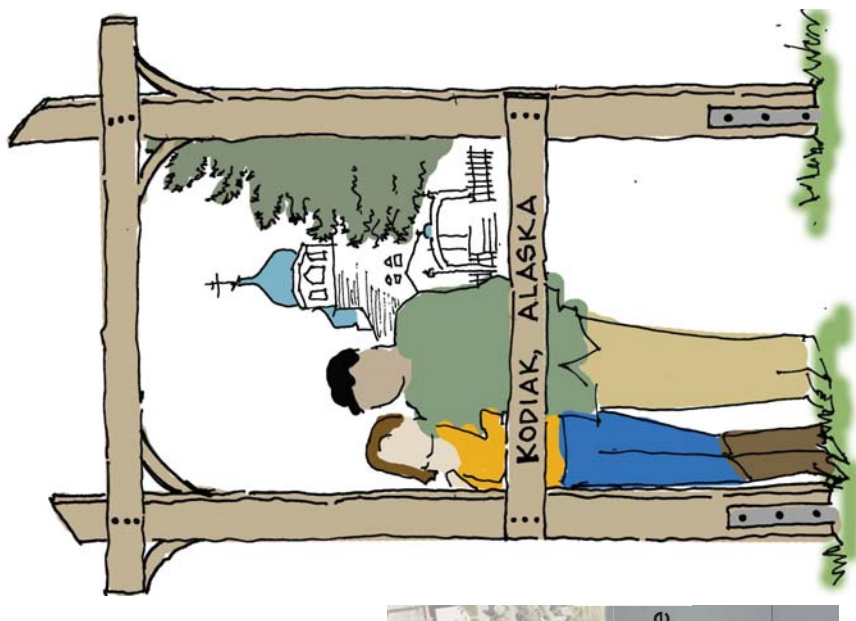
- maintenance personnel can become familiar and

- efficient with one type of fixture;
- the City will only need to keep spare parts for one product;
- the City can make bulk orders which can reduce costs; and
- one style of lighting will provide a uniform appearance throughout walking areas.

Pedestrian Nodes



Figure 21: Potential pedestrian nodes with frame structures that take advantage of views.



Update Themed Walking Routes On The Discover Kodiak Map

- Cultural
- Shopping
- Nature Hike
- Waterfront



Figure 20: Diagram of how pedestrian routes on the Discover Kodiak Map could be updated once projects are implemented.



Image 37: View of where a pedestrian amenity node could be.

SECTION IV: IMPLEMENTATION

Implementation Order

The ranking that resulted from project scores is a recommended order of project implementation. The project scores, resulted in the following Rank Order:

1. Crosswalk Improvements on Rezanof Drive- L5 (score 20)
2. Wayfinding Plan- O1 (score 18)
3. Sidewalk on Egan Way- L1 (score 17)
4. Pave Sidewalk on Rezanof Drive- B2 (score 16)
5. Sidewalk along Mission Road- M1 (score 12)
6. Trail from Erskine Avenue to Library- L2 (score 12)
7. Trail from Rezanof Drive to Erskine Avenue- L3 (score 9)
8. Connection from Lower Erskine Avenue to Rezanof Drive- B1 (score 8)
9. Trail from Mission Road to Bridge Connection- M2 (score 8)
10. Widen Sidewalk on Rezanof Drive- L4 (score 8)

Ranked at number one with an overall score of twenty is providing a crosswalk between the bridge to Near Island and Rezanof Drive's north side. This could be a quick, low cost, and potentially highly used project. This project requires a study to determine if this area warrants a crosswalk and what appropriate signs and lighting would be required by DOT&PF. If a study does show a need, a permit from DOT&PF would be required.

Two projects tied with a score of twelve: the sidewalk along Mission Road and the trail from Erskine Avenue to the Library. The Mission Road sidewalk was ranked higher due to a higher expected use by pedestrians. It would serve a section of road that currently has no sidewalk, whereas, the trail to the library would be an additional alternative connection to the library.

Three projects received a score of eight:

- the connection from lower Erskine Avenue to Rezanof Drive,
- the trail from Mission Road to the bridge connection, and
- widening the sidewalk on Rezanof Drive.

sufficient site control for the proposed pedestrian systems described in this plan will require multiple entities to either permit such interests or that these interests be acquired. The anticipated entities that will likely be contacted to gain site control include, the KIB, COK, DOT&PF, Alaska Department of Fish and Game (ADF&G), United States Fish and Wildlife Service (USFWS), and private property owners.

A majority of the proposed pedestrian facilities will likely be permissible within the various rights-of-way held by KIB, COK, and DOT&PF. There is one parcel where multiple entities may have to agree to trail construction and use, parties include the USFWS and DOT&PF. The remaining areas include a few properties where public use easements will need to be acquired from private property owners.

FUNDING

There are several funding sources available for bicycle and pedestrian projects. "Pedestrian and Bicycle Funding Opportunities U.S. Department of Transportation Transit, highway, and Safety Funds", is attached to this Plan as an appendix for reference. Given that recent program budgets have been shrinking nationwide, funding partners are seeking to leverage funds¹ from multiple sources to complete transportation projects. The funding sources discussed below are those most commonly involved in funding bike and pedestrian-related projects statewide.

FEDERAL:

U.S. Department of Transportation - Transportation Investment Generating Economic Recovery (TIGER)

The U.S. Department of Transportation (U.S. DOT) has made nearly \$500 million available annually for transportation projects since 2010 through the TIGER grant program. Applications under this program are successful if the project being applied for demonstrates construction readiness (National Environmental Protection Agency (NEPA) and design work is complete),

¹ Leveraging funding is the process of using multiple funding sources for one project. For example, a grant program may require the grant applicant provide a 20% cost share (match). The grant applicant can use local funds or other funding sources to provide the cost share for the project. Funding agencies are more likely to fund a project if other funds are assigned to the project already.

the ability to leverage funds, create jobs, and enhance the economic well-being within a community. The TIGER grant program supports innovative projects,

including multi-modal and multi-jurisdictional projects; bicycle lanes, parking, transit, bus shelters and benches, crosswalks; sidewalk improvements such as lighting, curb cuts and American with Disabilities Act (ADA) ramps; and paved shoulders for pedestrian and bicyclist use. The TIGER grant program is focused on capital projects that generate economic development and improve access to reliable, safe and affordable transportation for communities, both urban and rural. TIGER grant program funding opportunities are highly competitive, and are typically announced in late spring.

U.S. Department of Transportation - Federal Transit Administration (FTA)

The FTA provides formula funding to help cities, towns, and rural areas invest in bicycle infrastructure that can improve mobility and help people access public transportation. A local transit provider, such as Kodiak Area Transit System (KATS), may be interested in partnering with a local government to improve sidewalks and bicycle paths so that residents have better access to their transit system. This opportunity is developed through partnerships and leveraging funding. There is no solicitation for this opportunity.

U.S. Economic Development Administration

The Economic Development Administration (EDA) solicits applications from applicants in rural and urban areas to provide investments that support construction, non-construction, technical assistance, and revolving loan fund projects under EDA's Public Works and other programs. Grants and cooperative agreements made under these programs are designed to leverage existing regional assets and support the implementation of economic development strategies that advance new ideas and creative approaches to advance economic prosperity in distressed communities. EDA provides strategic investments on a competitive- merit-basis to support economic development, foster job creation, and attract private investment in economically distressed areas of the United States. This opportunity is open year round. A grant applicant can meet with the local EDA Program Manager out of Anchorage to determine eligibility.

DEVELOPMENT RIGHTS

Development rights establishes that there are sufficient enforceable rights to build and maintain any of these pedestrian projects for the life of the facilities. To gain

Federal Highway Administration

The Federal Highway Administration (FHWA) administers what is often referred to as Chapter 1 funds authorized under current transportation legislation. (Chapter 1 of 23 USC is where the regulations governing these funds are located.) This agency works with each state to expend the state's share of these federal transportation dollars for programs such as the Surface Transportation Program, Transportation Alternatives Program, Tribal Transportation Program, and Tribal Transportation Safety programs, and all transit programs. In Alaska, FHWA's partner agency is the Alaska Department of Transportation & Public Facilities (DOT&PF). Funding is administered through DOT&PF.

Western Federal Lands Highway Division

Western Federal Lands Highway Division (WFLHD) of the Office of Federal Lands Highway (FLH) jointly administers the Tribal Transportation Program with the Bureau of Indian Affairs (BIA). Each federally recognized tribe is eligible for this funding and is allocated an annual dollar amount based on a codified formula that takes into account tribal population, road mileage, and average tribal shares. These funds are often referred to as Chapter 2 funds and can be used by tribes as local match funds on projects funded with Chapter 1 funding.

WFLHD also administers Alaska's Federal Lands Access (FLAP) Program; a program for surface transportation facilities providing access to, or within, federally-owned lands. This program is designed to encourage cooperation and coordination among federal land management agencies, state agencies, and local and tribal governments. Funding is administered through DOT&PF.

Bureau of Indian Affairs

The Tribal Transportation Program (TTP) provides tribes with a percentage of funding allocated based on highway legislation to plan, design, construct, and maintain their local transportation system. TTP funds can be used for safety, transit, administration, maintenance, bike and pedestrian facilities, and planning. These funds can be leveraged and used as non-federal match. This opportunity is developed through partnerships and leveraging funding. There is no solicitation for this opportunity.

STATE:

Alaska Surface Transportation Block Grant (STBG) Set-Aside Program

The Fixing America's Surface Transportation (FAST) Act of 2015 includes the Surface Transportation Block Grant (STBG) Program, which sets aside funds for use for a broad range of projects, including walking and bicycling projects. Funding levels in the STBG Set-Aside Program are set at \$835 million for financial years 2016 and 2017, rising to \$850 million in financial years 2018-2020. This funding is distributed to the state and local level on a formula-set apportionment basis. A local match is required for all projects. Funding opportunities are typically made available in late spring.

Legislature

Each year the Alaska Legislature develops both capital and operating budgets for the state. In years when the state's fiscal situation allows, transportation projects for areas across the state are often included as line items in the capital budget. Additionally, the legislature periodically drafts bond bills that are then voted on by state residents during general elections. Unlike capital budget line items, items identified in an approved bond bill are funded through the sale of general obligation bonds, which are repaid at a later date using specified state revenues.

Department of Transportation & Public Facilities

As FHWA's partner agency for the State of Alaska, DOT&PF is responsible for the planning and programming of funding under the purview of FHWA. Several types of funding DOT&PF administers allow tribal governments, municipal governments, and other similar entities to nominate projects for inclusion in the Statewide Transportation Improvement Program (STIP) or compete for grant-like funding to complete projects. The State of Alaska also has a State Highway Safety Improvement Program (HSIP). The HSIP is a core Federal-aid program with the purpose of achieving a significant reduction in fatalities and serious injuries on all public roads. Funding is administered through DOT&PF.

Department of Commerce, Community, and Economic Development

Alaska's Department of Commerce, Community, and Economic Development (DCCED) administers several programs of interest in terms of developing and maintaining transportation infrastructure vital to a community's success. Most notably, it administers the Community Development Block Grant (CDBG) program, funded by the U.S. Department of Housing and Urban Development (HUD). Once each year, municipal governments are able to apply for CDBG funding for an array of project types, which include transportation improvements such as bicycle and pedestrian facilities. In addition to capital projects, HUD also allows CDBG funding to be used for planning efforts. Funding is administered through the State of Alaska.

DESIGN & CONSTRUCTION

The level of design and construction for each project varies. Some projects are technically complex, requiring retaining walls or footings in the case of elevated stairs. Others have relatively minimal design and construction efforts, such as short sections of gravel trail. In either case the design will need to be reviewed and permitted by the appropriate authority such as the COK Building Safety Department or DOT&PF.

Depending on the complexity of the project, the construction work will likely need to be contracted out to professional contractors. In cases where construction is minimal, there may be opportunities for volunteers to assist with construction. This may be possible for short sections of gravel trail that have minimal grading restrictions, or sign installation.

Conclusion

As a coastal town the COK has a unique, authentic working industrial waterfront with ocean and mountainous views. By improving pedestrian access to the waterfront, downtown, the Study Area in this plan and beyond, tourists and locals will have opportunities to walk to local businesses, cultural sites and community events. A pedestrian network improves alternative transportation, provides health benefits, contributes to the local economy, and can contribute to an overall sense of place.

Appendix A: Public Comment

The following table outlines the public comment received over the duration on the project. The comments are categorized into two categories:

- Specific Route Comments
- General Comments

Within those two categories the comments are further divided into topics, this includes:

- Specific Route Comments:
 - Crosswalks
 - Connection at the Bridge
 - Connections Under the Bridge (to Marine Way)
 - Mission Road
 - Marine Way/Park at Pearson Cove
 - Connection to the Library
 - Other Route Considerations
- General Comments:
 - Wayfinding/Signage
 - Visitor Circulation & Cruise Ship Passenger Drop-Off
 - Miscellaneous
 - Scoring Criteria

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Specific Route Improvements				
Crosswalk				
1	Crosswalks on Rezanof should have light up crossing signs to alert motorists on this blind corner.	Web	1	A crosswalk is proposed across Rezanof to the bridge (project L5). Whether or not a crosswalk is feasible and what features are appropriate will need to be determined with appropriate analysis as noted in the L5 project description table.
2	Near the intersection of Rezanof Drive and the bridge, an inlaid crosswalk is needed. Inlaid crosswalks may cost more upfront, but they last much longer so the cost is recovered.	Comment Form	1	See response to comment 1.
3	Flashing sign for crosswalk. (Arrow pointing to crossing on Upper Mill Bay Road near the intersection with Wilson Ave.)	Map Comments (Open House #2)	1	Since this plan is for improving the pedestrian connections for tourist destinations this was not included as a potential pedestrian improvement because it does not appear to connect to a tourist destination or route.
4	I worry about elderly people crossing Rezanof. Is there some way to connect route "G" directly to the Bridge and route "C" to downtown? At least this is a major intersection with excellent visibility both up and down Rezanof.	Web	1	A series of projects are proposed to create this connection between route "C" and "G"; this would include projects: L2, L3, L4, L5, and B1 (A or B). See the overall schematic for how each project is linked.
5	"Route B" is my preferred route to Near Island so I would suggest a pedestrian walkway on the south side of Rezanof."	Web	1	Project B2 is to pave a sidewalk along this section of Rezanof.
6	Seepage from natural spring down the hill from the library can make the sidewalk slick.	Map Exercise (Open House #1)	1	This has been noted on the public comment map, and noted as a design consideration in the L4 sidewalk project.
7	Dirt path along Rezanof prevents ice.	Map Exercise (Open House #1)	1	Project B2 is to pave a sidewalk along this section of Rezanof. Currently the surface is dirt and gravel which is less accessible than a paved sidewalk. Although this section of sidewalk seems less slick than the north sidewalk it may be because there appears to be drainage seeping from the hillside at that location as noted by other members of the public.
8	Easy and helpful improvement would be a crosswalk on Rezanof at Upper Mill Bay. People walk to and from the Borough buildings, an high school/middle school, main elementary to the downtown area and that is where they cross.	General Comments (Open House #2)	1	Project L5 recommends a crosswalk at the bridge, but the best location for a crosswalk will need to be determined with proper analysis. The crosswalk option at Mill Bay Road has been noted in the L5 project table under other comments.
9	Crosswalk school side. (Intersection of Upper Mill Bay Road and Rezanof Drive.)	Map Comments (Open House #2)	1	See response to comment 8.
10	Crosswalk to walkway side of bridge. (Across Rezanof)	Map Comments (Open House #2)	1	A crosswalk is proposed across Rezanof to the bridge (project L5).
11	Heavy pedestrian traffic, including school students on lunch break, yet it has no caution signs for motorists. At least one senior citizen pedestrian fatality happened here. Note proximity of this nasty intersection to the Senior Center. (X2)	Map Comments (Open House #2)	1	A cross-walk is proposed across Rezanof to the bridge (project L5). Whether or not a cross-walk is feasible and what features are appropriate will need to be determined with full analysis.
12	Sidewalk! (Arrow pointing to intersection of Upper Mill Bay Road and Rezanof Drive.)	Map Comments (Open House #2)	1	This has been noted on the public comment map.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Connection at Bridge				
13	We should avoid doing a stair set if at all possible to maintain access to bicycles.	Web	1	As an alternative to a stairway Project B1-A, a sloped trail, has been included in the potential projects for this area.
14	Route C- it's used already but muddy, steep, unsafe.	Comment Form	1	Project B1 is proposed to improve these conditions; this would either be a sloped sidewalk or stairway.
15	There is a social trail that cuts up the hill to Rezanof from the end of Erskine.	"Map Exercise (Open House #1)"	1	See response to comment 14.
16	Walk up from Senior Center parking to the bridge can feel unsafe due to brush and poor visibility.	"Map Exercise (Open House #1)"	1	See response to comment 14.
17	Stairs at bridge.	"Map Exercise (Open House #1)"	1	See response to comment 14.
18	Stairs with lighting (at bridge).	"Map Exercise (Open House #1)"	1	See response to comment 14.
19	Information, signage, views (at bridge stairs).	"Map Exercise (Open House #1)"	1	Noted in the project description of B1.
20	Possible connection from end of lower Erskine up the hill to Rezanof.	"Map Exercise (Open House #1)"	1	See response to comment 19.
21	New Mortuary along lower Erskine.	"Map Exercise (Open House #1)"	1	This has been noted on the public comment map.
22	Route pedestrians around parking lot at Senior Center.	"Map Exercise (Open House #1)"	1	Project B1 can address this by tying a new sidewalk into the existing one at the end of Erskine Avenue.
23	Difficult to cross parking lot near Senior Center.	"Map Exercise (Open House #1)"	1	See response to comment 22.
24	Busy intersection: boat traffic, gravel trucks, etc. (intersection at bridge to Near Island)	Map Comments (Open House #2)	1	This has been noted on the public comment map.
25	(Arrow pointing out metal stairs, galvanized steel treads and landings.)	Map Comments (Open House #2)	1	This is included in the design considerations for project B1-B.
26	Many people go up and down this bank (slope along Rezanof Dr. north of the Senior Center). Need steel stairs a la Best Western to Shellikof.	Map Comments (Open House #2)	1	Included as a recommendation in project B1-B.
27	Routes from Erskine to Rezanof could be potential emergency route for residents of housing near Senior Center.	Map Comments (Open House #2)	1	This has been noted on the public comment map.
28	Steel grate type of stairs might nag certain types of ice cleats, causing falls. Sign at top and bottom of these (new) stairs asking pedestrians to remove them (cleats) might help.	Map Comments (Open House #2)	1	Noted in the project description of B1.
29	The idea of developing better crossing paths to the library from the bridge and downtown area is definitely a must, this is used frequently.	Draft Plan Posted for Public Review	1	Great, this comment is consistent with our observations and other feedback we received.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Connections Under Bridge (to Marine Way)				
30	What about access from Marine Way, down by F&A and Power house?	Comment Form	1	This is outside of the Study Area of this plan but has been noted in the General Consideration section for further exploration.
31	It would be nice to have a pathway that connects Marine Way (under the bridge) to the bridge.	Comment Form	1	See response to comment 30.
32	KEA easement? (noted under bridge near Marine Way).	Map Exercise (Open House #1)	1	See response to comment 30.
33	Link from Mission under bridge to Powerhouse drawn on map.	Map Exercise (Open House #1)	2	See response to comment 30.
34	Route from Marine Way to bridge drawn.	Map Exercise (Open House #1)	4	See response to comment 30.
35	Good idea (trail under the bridge connecting Mission Road to Marine Way).	Map Comments (Open House #2)	1	See response to comment 30.
Mission Road				
36	Old sidewalk under dirt/gravel along Mission Road past the church.	Map Exercise (Open House #1)	1	This has been noted on the public comment map.
37	Poor or no lighting along Mission.	Map Exercise (Open House #1)	1	Under the General Considerations section there are recommendations to enhance pedestrian lighting where existing lighting is nonexistent or insufficient.
38	Mission Road "not pedestrian friendly".	Map Exercise (Open House #1)	1	Project M1 is included to improve pedestrian connections along Mission Road.
39	Ideal route because already sidewalks (along Mission near Center).	Map Exercise (Open House #1)	1	The Mission Road projects include M1 & M2. M1 is a sidewalk along the southeast side of the road and M2 is a trail that connects up to the bridge pedestrian connection.
40	Two houses along Mission near Erskine are condemned.	Map Exercise (Open House #1)	2	This has been noted on the public comment map.
41	Potential hoop house as a part of St. Herman's Chapel on Mission.	Map Exercise (Open House #1)	1	See response to comment 40.
42	Properties along Mission belong to Seminary.	Map Exercise (Open House #1)	1	See response to comment 40.
43	House between two condemned buildings along Mission belongs to Seminary.	Map Exercise (Open House #1)	1	See response to comment 40.
44	Main Seminary parking lot along Mission.	Map Exercise (Open House #1)	2	Because of the Seminary location the sidewalk along Mission Road is recommended to be on the southeast side of the road to improve pedestrian access to the Seminary (project M1).
45	Sidewalk on Mission after Tagura would require retaining wall.	Map Exercise (Open House #1)	1	This has been considered for the preliminary cost estimate for project M1.
46	High pedestrian traffic along Mission from Seminary.	Map Exercise (Open House #1)	1	Noted, this is one of the reasons that the sidewalk (project M1) was recommended to be on the Seminary side of Mission Road.
47	Existing driveway below seminary potential- pedestrian path.	Map Exercise (Open House #1)	1	This is outside of the study area but has been noted on the public comment map.
48	Safety concern- sidewalk on Mission.	General Comments (Open House #2)	1	A sidewalk (project M1) has been proposed to improve safety.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Mission Road				
49	The plan of connecting the bridge pathway to Mission Road is really not at all important for our cruise passengers. We advocate fairly heavily that they avoid Mission Road altogether, not only because there is no walking path, but because there are no businesses. While a handful of tourists may decide to take the “unbeaten” route here, it really will not get usage enough to justify those costs. Secondly, many of our cruise passengers have trouble with stairs; so linking the paths with stairs is really a very feasibly idea, either.	Draft Plan Posted for Public Review	1	<p>Thank you for this insight. Project M1 is a sidewalk on Mission Road that would end next to the St. Herman’s Chapel. We reviewed this proposed project and have determined it should remain as part of the recommendations because:</p> <ul style="list-style-type: none"> • St. Herman’s Chapel is identified as a tourist destination on the Discover Kodiak Map on page 33; • No sidewalk exists along this section of road, and • Cruise ship passengers have been observed walking in the road in this area as seen in image 32 on page 29. <p>Although Discover Kodiak currently does not direct tourists to the Chapel installing a sidewalk here will hopefully allow Discover Kodiak to feel more comfortable to direct cruise ship visitors to see this cultural destination safely.</p> <p>The recommendation for project M2 is to connect Mission Road to the bridge with a section of trails and/or stairs. Without this connection, tourists walking along Mission Road would have to back track to Mill Bay Road in order to reach the bridge. Providing this connection would create a more efficient route to the bridge and give them unique scenic vantage points. We agreed that this link will probably be used less than other proposed projects; that is why this project received a ‘1’ for “Use Intensity”.</p>
50	Again, the sidewalk on Mission Road will not ever have enough usage to justify spending for CRUISE passengers. What are they walking to?	Draft Plan Posted for Public Review	1	See response to comment 49.
51	Mission Road needs a sidewalk. The deterioration of the road and its narrow lanes make it very hard to walk to town without getting hit by a vehicle, especially when snow and ice is plowed to its sides. This is a heavily populated community and would benefit greatly with a sidewalk.	Draft Plan Posted for Public Review	1	This comment is consistent with our observations and other feedback we received. Project M1 is a sidewalk on Mission Road that would end next to the St. Herman’s Chapel. This project would improve pedestrian safety at this location.
Marine Way/Park at Pearson Cove				
52	Marine Way drawn as a route on comment map notes; "Marine Way is a great walk".	Map Exercise (Open House #1)	1	This is outside of the Study Area of this plan but has been noted in the General Consideration section for further exploration.
53	Pearson Cove Park drawn on map.	Map Exercise (Open House #1)	1	See response to comment 52.
54	Route drawn along Marine Way.	Map Exercise (Open House #1)	1	See response to comment 52.
55	Connection to park at Pearson Cove drawn.	Map Exercise (Open House #1)	1	See response to comment 52.
56	Route drawn from Pearson Cove drawn to Steller Way.	Map Exercise (Open House #1)	1	See response to comment 52.
57	The development of a better walking path on Marine Way to the overlook park is a great idea; that spot is undeveloped and untouched, but if there was an opportunity, it would be a nice place to send visitors with a little extra time.	Draft Plan Posted for Public Review	1	Agreed, this route would be a great walking alternative for visitors that would prefer a flatter route. This area is outside of our Study Area and would require coordination with a variety of private landowners. We recommend that the feasibility of this project be looked at further as a separate analysis.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Connection to Library				
58	Coming down "A" or "B" there is very little to see except a graveyard and possibly a view of Kodiak with mountains in the background, which I imagine you can get from "G".	Web	1	Route "G" developed into projects L2 & L3 which provide scenic views of the water and surrounding mountains.
59	Route from Mill Bay Road to Library noted as "Trail?"	Map Exercise (Open House #1)	1	We did not see a trail at this location but did see one coming south from the library which was proposed as project L2.
60	Route drawn from Rezanof to Library via trail.	Map Exercise (Open House #1)	1	This social trail was located and has been included as project L2.
61	Route drawn from curve in Erskine to Library.	Map Exercise (Open House #1)	6	See response to comment 60.
62	Free wifi, tourist/cruise destination (Library)	Map Comments (Open House #2)	1	This has been noted on the public comment map.
63	Fix drainage (existing seepage). (Sidewalk on Upper Mill Bay Road near intersection with Egan Way)	Map Comments (Open House #2)	1	This has been noted on the public comment map, and noted as a design consideration in the L1 sidewalk project.
64	Pubic Radio KMXT noted on plan next to Library.	Map Comments (Open House #2)	1	See response to comment 63.
65	Already a foot path along fence that allows loop around Library. (Along the east side of the U.S. Fish & Wildlife Service property and Alascom property).	Map Comments (Open House #2)	1	Noted, projects L2 & L3 would formalize a trail through this area.
Other Route Considerations				
66	Route drawn from Upper Mill Bay Road going northwest to Post Office.	Map Exercise (Open House #1)	1	This is outside of the Study Area but has been noted on the public comment map.
67	Students from High School go down Mill bay to Subway almost every day.	Map Exercise (Open House #1)	1	This has been noted on the public comment map.
68	Route on Tagura Road drawn.	Map Exercise (Open House #1)	2	See response to comment 66.
69	Trident workers walk from housing down Center Avenue and Mission Road to Trident facility.	Map Exercise (Open House #1)	2	See response to comment 67.
General Considerations				
Wayfinding/Signage				
70	How long they(tourists) spend in Kodiak to see the sights is key, thus every attraction should be marked with how long it takes to walk there and perhaps the stress involved e.g. uphill.	Web	1	This has been included in the Wayfinding project O1.
71	The weather will play a big part in what visitors get to see and do, so you need to divide attractions and the routes to them by indoors/outdoors.	Web	1	This has been noted on the public comment map.
72	Rather than putting all this information on signs, it would be best to produce a "walking map/brochure" that the cruise ships can give to their passengers before they dock. This will give passengers time to plan.	Web	1	Discover Kodiak publishes a map that is distributed to visitors. Part of the recommendations of this plan is to update that map with new routes as they are developed.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Wayfinding/Signage				
73	Plan on incorporating interpretive signs into all pathways.	Comment Form	1	The design considerations for project O1 (wayfinding) include a recommendation to have interpretive signs.
74	Easy signs directing people to the library on existing sidewalk on upper Mill Bay to Egan.	General Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to have signage to the library.
75	Inexpensive to upgrade existing walking paths with signage and lighting.	General Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to develop signage. The General Recommendations section recommends pedestrian lighting improvements.
76	Small Boat Harbor wayfinding. (at Rezanof intersection with bridge)	Map Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to have signage to attractions on Near Island.
77	Sign for Library (at intersection of upper Erskine Ave. with Rezanof Dr.).	Map Comments (Open House #2)	1	The design considerations for project O1 (wayfinding) include a recommendation to have signage to the library.
78	Library wayfinding sign needed in area. (Area near Mill Bay Road north of Rezanof Drive.	Map Comments (Open House #2)	1	See response to comment 77.
79	Sign "Near Island Trails" & "Dog Bay Small Boat Harbor". Sign @ Y/Alutiiq directing people toward Small Boat Harbor.	Map Comments (Open House #2)	1	See response to comment 77.
80	We love the wayfinding, lighting and photo opportunities throughout the plan. They are great!	Draft Plan Posted for Public Review	1	Good to hear.
Visitor Circulation Considerations & Cruise Ship Passenger Drop-off				
81	If visitors are dropped off at the Alutiiq Museum there is a lot of uphill to reach the library or the bridge. Maybe taxis and vans could drop visitors at the library and then its all downhill from there.	Web	1	This has been noted on the public comment map.
82	Many of the visitors I have seen appear to be elderly, so unless you plan on building a tunnel or overpass, I think that the safest route to cross Rezanof would be coming downhill.	Web	1	See response to comment 81.
83	From the visitors perspective, presumably most will come from cruise ships.	Web	1	See response to comment 81.
84	Cruise ship passenger bus stop passenger load/unload, 60-80 people, in front of Alutiiq Museum.	Map Exercise (Open House #1)	1	See response to comment 81.
Miscellaneous				
85	The City should enforce its ordinance prohibiting parking on sidewalks.	Comment Form	1	This has been noted on the public comment map.
86	Love the bulkhead parking along Shelikof and I would love even more to incorporate the Following: interpretive signs, covered bike shelter, public art.	Comment Form	1	See response to comment 85.
87	There aren't sidewalks in places that are actually useful for pedestrians, especially downtown.	Comment Form	1	The goal of this plan is to highlight potential projects that will improve the network of pedestrian connections. If implemented these projects will add more useful sidewalks and trails.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Miscellaneous				
88	There aren't useful pathways that connect Rezanof/Mission/Mill Bay, in fact, there are only (maybe) 3 roads that directly connect what are the major arteries in Kodiak. How can we create paths that will do what roads can't? I think we can.	Comment Form	1	The projects proposed in this plan will increase pedestrian options in the Study Area. See the overall schematic for the proposed network of paths.
89	"Old Dorms" at corner with Library.	Map Exercise (Open House #1)	1	This has been noted on the public comment map.
90	Future bronze bear statue at the Kodiak National Wildlife Refuge Visitor Center summer 2016.	Map Exercise (Open House #1)	1	See response to comment 89.
91	Bicyclists and skateboarders almost run over pedestrians when the former don't use bike paths like they're supposed to.	Map Comments (Open House #2)	1	See response to comment 89.
92	Snow, ice removal from sidewalks traditionally get last priority, I've noticed in the 40 years I've lived here. This forces pedestrians off the sidewalk and into the road shoulder. I'm amazed that nobody has been killed yet!	Map Comments (Open House #2)	1	Maintaining the sidewalks free of snow and ice was included in the General Recommendations section.
93	Flashing light @ crosswalk @ McDonald's	Map Comments (Open House #2)	1	This is outside of the Study Area of this plan but has been noted on the public comment map.
94	Bridge needs tall barrier on both sides, as a teenage suicide almost happened here. Companions with would be victim grabbed her before she jumped.	Map Comments (Open House #2)	1	See response to comment 93.
95	Noted AK Fish & Game Bunkhouse on building near bridge.	Map Comments (Open House #2)	1	This has been noted on the public comment map.

Comment Number	Comment	Comment Source	Number of Times Comment Appeared	Response
Scoring Criteria				
96	<p>Thank you for the opportunity to commend on the draft Kodiak Pedestrian Pathway Plan.</p> <p>Although I tried to frame my comments from the viewpoint of tourists, I am also thinking of the greater community, particularly residents of Bayview Terrace, Kodiak Plaza, Kodiak Senior Center, and hundreds or perhaps thousands of people who live and work in lower downtown.</p> <p>The scoring criteria seemingly fails to recognize the value of a shorter walking routes to pedestrians. In two of the nine projects, proposed routes that are significantly shorter than existing routes score poorly in the heavily weighted “Use Intensity” category, explaining in both cases that “routes already exist.” I believe pedestrians will take the shortest route available to them, particularly when wayfinding is provided, such as has been recommended. Cruise ship passengers, who are generally of an older demographic and with limited time ashore, may particularly appreciate the most efficient walking routes.</p> <p>The cost of individual projects ranges from \$100,000 to \$720,000, which groups them rather closely in terms of public works projects. Assigning a heavily weighted multiplier to a number projects over such a narrow range costs might place undue emphasis on cost, lessening the relative importance of routing the trail to serve critical historical sites, cultural assets, and tourist destinations.</p> <p>The resultant priority list, whether it intends to or not, is biased toward routing the path along the busy street Rezanof Drive between Lower Mill Bay Road and the Fred Zharoff memorial bridge rather than traversing the more peaceful, culturally rich lower downtown. I believe that the projects along Mission Road, Erskine and proximate to the bridge provide a more direct route that serves more tourist destinations. I am in agreement with the proposed continuation of the path across an improved crosswalk at Rezanof Drive and up borough hill toward the Kodiak Public Library.</p> <p>I hope projects will be evaluated by taking a holistic view of the best pedestrian routes through downtown, and not compromise the most sensible route for what may later seem like minimal differences in cost.</p> <p>Whatever the outcome, I am pleased to see each these projects highlighted in a city planning document. I would ask that language be included to emphasize the value in all the projects, as and any of these might become uniquely eligible for specialized funding, and investments in walkable communities provide an unbeatable return on investment.</p>	Draft Plan Posted for Public Review	1	<p>The two projects referred to in the first paragraph of this comment include projects L2 and L3. It is a good point that routes should not be ranked lower just because they are alternative routes, especially when they are shorter routes than those existing. In these instances, the alternative routes are shorter in distance but both go through sections of terrain that are steep. The existing route to the library also passes by the Russian Cemetery which is a cultural destination for tourists. This was also factored into the “Use Intensity” score being given a “1” (moderate expected use) for these projects. We will add language in the Project Score table “Scoring Analysis” column that explains the other factors in giving a ranking of “1”.</p> <p>Great points regarding the scoring and ranking system of the projects. The ranking was provided as a general guide and order to how projects could be implemented. We agree that the overall goal is to install all projects to improve the entire network. The project ranking should not be considered a strict order of how to implement projects. Therefore, we added language that although projects are scored and ranked they should all be considered important pedestrian infrastructure improvements:</p> <p><i>“The goal of ranking projects is not to create a rigid order of how projects need to be developed, but to provide an idea of what project might be high impact or quick to implement. All projects should be considered valuable improvements that contribute to the overall pedestrian network. Given the effort, needed cooperation, and funding any project can be developed no matter the recommended order set by this scoring system.”</i></p> <p>This text has been added to page 17 under the “Project Ranking Criteria” section as well as additional text on page 35.</p>

Appendix B: Federal Pedestrian and Bicycle Funding Opportunities

Pedestrian and Bicycle Funding Opportunities U.S. Department of Transportation Transit, Highway, and Safety Funds

Revised August 12, 2016

This table indicates potential eligibility for pedestrian and bicycle projects under U.S. Department of Transportation surface transportation funding programs. Additional restrictions may apply. See notes and basic program requirements below, and see program guidance for detailed requirements. Project sponsors should fully integrate nonmotorized accommodation into surface transportation projects. Section 1404 of the Fixing America's Surface Transportation (FAST) Act modified 23 U.S.C. 109 to require federally-funded projects on the National Highway System to consider access for other modes of transportation, and provides greater design flexibility to do so.

Activity or Project Type	U.S. Department of Transportation Transit, Highway, and Safety Funds														
	TIGER	TIFIA	FTA	ATI	CMAQ	HSIP	NHPP	STBG	TA	RTP	SRTS	PLAN	NHTSA 402	NHTSA 405	FLTP
Access enhancements to public transportation (includes benches, bus pads)	\$	\$	\$	\$	\$		\$	\$	\$						\$
ADA/504 Self Evaluation / Transition Plan								\$	\$			\$			\$
Bicycle plans			\$					\$	\$			\$			\$
Bicycle helmets (project or training related)								\$	\$SRTS			\$	\$*		
Bicycle helmets (safety promotion)								\$	\$SRTS			\$			
Bicycle lanes on road	\$	\$	\$	\$	\$	\$	\$	\$	\$	\$	\$	\$			\$
Bicycle parking	~\$	~\$	\$	\$	\$		\$	\$	\$	\$	\$	\$			\$
Bike racks on transit	\$	\$	\$	\$	\$			\$	\$			\$			\$
Bicycle share (capital and equipment; not operations)	\$	\$	\$	\$	\$		\$	\$	\$			\$			\$
Bicycle storage or service centers at transit hubs	~\$	~\$	\$	\$	\$			\$	\$			\$			\$
Bridges / overcrossings for pedestrians and/or bicyclists	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$	\$			\$
Bus shelters and benches	\$	\$	\$	\$	\$		\$	\$	\$			\$			\$
Coordinator positions (State or local)					\$ 1 per State			\$	\$SRTS			\$			
Crosswalks (new or retrofit)	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$	\$			\$
Curb cuts and ramps	\$	\$	\$	\$	\$*	\$	\$	\$	\$	\$	\$	\$			\$
Counting equipment			\$	\$		\$	\$	\$	\$	\$	\$	\$*			\$
Data collection and monitoring for pedestrians and/or bicyclists			\$	\$		\$	\$	\$	\$	\$	\$	\$*			\$
Historic preservation (pedestrian and bicycle and transit facilities)	\$	\$	\$	\$				\$	\$						\$
Landscaping, streetscaping (pedestrian and/or bicycle route; transit access); related amenities (benches, water fountains); generally as part of a larger project	~\$	~\$	\$	\$			\$	\$	\$						\$
Lighting (pedestrian and bicyclist scale associated with pedestrian/bicyclist project)	\$	\$	\$	\$		\$	\$	\$	\$	\$	\$	\$			\$
Maps (for pedestrians and/or bicyclists)			\$	\$	\$			\$	\$			\$*			
Paved shoulders for pedestrian and/or bicyclist use	\$	\$			\$*	\$	\$	\$	\$	\$	\$	\$			\$

Key: \$ = Funds may be used for this activity (restrictions may apply). \$* = See program-specific notes for restrictions. ~\$ = Eligible, but not competitive unless part of a larger project.

Pedestrian and Bicycle Funding Opportunities															
U.S. Department of Transportation Transit, Highway, and Safety Funds															
Activity or Project Type	TIGER	TIFIA	FTA	ATI	CMAQ	HSIP	NHPP	STBG	TA	RTP	SRTS	PLAN	NHTSA 402	NHTSA 405	FLTP
Pedestrian plans			\$						\$		\$	\$			\$
Recreational trails	~\$	~\$							\$		\$				\$
Road Diets (pedestrian and bicycle portions)	\$	\$				\$			\$						\$
Road Safety Assessment for pedestrians and bicyclists						\$			\$			\$			\$
Safety education and awareness activities and programs to inform pedestrians, bicyclists, and motorists on ped/bike safety									\$SRTS		\$	\$*	\$*	\$*	
Safety education positions									\$SRTS		\$		\$*		
Safety enforcement (including police patrols)									\$SRTS		\$		\$*	\$*	
Safety program technical assessment (for peds/bicyclists)									\$SRTS		\$	\$*	\$		
Separated bicycle lanes	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Shared use paths / transportation trails	\$	\$	\$	\$	\$*	\$	\$	\$	\$		\$				\$
Sidewalks (new or retrofit)	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Signs / signals / signal improvements	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Signed pedestrian or bicycle routes	\$	\$	\$	\$	\$				\$		\$				\$
Spot improvement programs	\$	\$	\$	\$		\$	\$	\$	\$		\$				\$
Stormwater impacts related to pedestrian and bicycle projects	\$	\$	\$	\$	\$	\$	\$	\$	\$		\$				\$
Traffic calming	\$	\$	\$	\$		\$	\$	\$	\$		\$				\$
Trail bridges	\$	\$			\$*	\$	\$	\$	\$		\$				\$
Trail construction and maintenance equipment									\$RTP	\$RTP	\$				
Trail/highway intersections	\$	\$			\$*	\$	\$	\$	\$		\$				\$
Trailside and trailhead facilities (includes restrooms and water, but not general park amenities; see guidance)	~\$*	~\$*						\$*	\$*	\$*	\$				\$
Training					\$	\$		\$	\$		\$	\$*	\$*		
Training for law enforcement on ped/bicyclist safety laws								\$SRTS	\$SRTS		\$			\$*	
Tunnels / undercrossings for pedestrians and/or bicyclists	\$	\$	\$	\$	\$*	\$	\$	\$	\$		\$				\$

Abbreviations

ADA/504: Americans with Disabilities Act of 1990 / Section 504 of the Rehabilitation Act of 1973
TIGER: Transportation Investment Generating Economic Recovery Discretionary Grant program
TIFIA: Transportation Infrastructure Finance and Innovation Act (loans)
FTA: Federal Transit Administration Capital Funds
ATI: Associated Transit Improvement (1% set-aside of FTA)
CMAQ: Congestion Mitigation and Air Quality Improvement Program
HSIP: Highway Safety Improvement Program
NHPP: National Highway Performance Program
STBG: Surface Transportation Block Grant Program
TA: Transportation Alternatives Set-Aside (formerly Transportation Alternatives Program)
RTP: Recreational Trails Program
SRTS: Safe Routes to School Program / Activities
PLAN: Statewide Planning and Research (SPR) or Metropolitan Planning funds
NHTSA 402: State and Community Highway Safety Grant Program
NHTSA 405: National Priority Safety Programs (Nonmotorized safety)
FLTP: Federal Lands and Tribal Transportation Programs (Federal Lands Access Program, Federal Lands Transportation Program, Tribal Transportation Program, Nationally Significant Federal Lands and Tribal Projects)

Program-specific notes

Federal-aid funding programs have specific requirements that projects must meet, and eligibility must be determined on a case-by-case basis. For example:

- TIGER: Subject to annual appropriations.
- TIFIA: Program offers assistance only in the form of secured loans, loan guarantees, or standby lines of credit, but can be combined with other grant sources, subject to total Federal assistance limitations.
- FTA/ATI: Project funded with FTA transit funds must provide access to transit. See [Bikes and Transit](#) and the FTA Final Policy Statement on the [Eligibility of Pedestrian and Bicycle Improvements under Federal Transit Law](#).
 - Bicycle infrastructure plans and projects funded with FTA funds must be within a 3 mile radius of a transit stop or station, or if further than 3 miles, must be within the distance that people could be expected to safely and conveniently bike to use the particular stop or station.
 - Pedestrian infrastructure plans and projects funded with FTA funds must be within a ½ mile radius of a transit stop or station, or if further than ½ mile, must be within the distance that people could be expected to safely and conveniently walk to use the particular stop or station.
 - FTA funds cannot be used to purchase bicycles for bike share systems.
 - FTA encourages grantees to use FHWA funds as a primary source for public right-of-way projects.
- CMAQ projects must demonstrate emissions reduction and benefit air quality. See the CMAQ guidance at www.fhwa.dot.gov/environment/air_quality/cmaq/ for a list of projects that may be eligible for CMAQ funds. Several activities may be eligible for CMAQ funds as part of a bicycle and pedestrian-related project, but not as a highway project. CMAQ funds may be used for shared use paths, but may not be used for trails that are primarily for recreational use.
- HSIP projects must be consistent with a State’s [Strategic Highway Safety Plan](#) and either (1) correct or improve a hazardous road location or feature, or (2) address a highway safety problem.
- NHPP projects must benefit National Highway System (NHS) corridors.
- STBG and TA Set-Aside: Activities marked “\$SRTS” means eligible only as an SRTS project benefiting schools for kindergarten through 8th grade. Bicycle transportation nonconstruction projects related to safe bicycle use are eligible under STBG, but not under TA (23 U.S.C. 217(a)).
- RTP must benefit recreational trails, but for any recreational trail use. RTP projects are eligible under TA and STBG, but States may require a transportation purpose.
- SRTS: FY 2012 was the last year for SRTS funds, but SRTS funds are available until expended.
- Planning funds must be used for planning purposes, for example:
 - Maps: System maps and GIS;
 - Safety education and awareness: for transportation safety planning;
 - Safety program technical assessment: for transportation safety planning;
 - Training: bicycle and pedestrian system planning training.
- Federal Lands and Tribal Transportation Programs (FLTTP) projects must provide access to or within Federal or tribal lands:
 - Federal Lands Access Program (FLAP): Open to State and local entities for projects that provide access to or within Federal or tribal lands.
 - Federal Lands Transportation Program: For Federal agencies for projects that provide access within Federal lands.
 - Tribal Transportation Program: available for federally-recognized tribal governments for projects within tribal boundaries and public roads that access tribal lands.
- NHTSA 402 project activity must be included in the State’s Highway Safety Plan. Contact the State Highway Safety Office for details: <http://www.ghsa.org/html/about/shsos.html>
- NHTSA 405 funds are subject to State eligibility, application, and award. Project activity must be included in the State’s Highway Safety Plan. Contact the State Highway Safety Office for details: <http://www.ghsa.org/html/about/shsos.html>

Cross-cutting notes

- FHWA Bicycle and Pedestrian Guidance: http://www.fhwa.dot.gov/environment/bicycle_pedestrian/
- **Applicability of 23 U.S.C. 217(i) for Bicycle Projects:** 23 U.S.C. 217(i) requires that bicycle facilities “be principally for transportation, rather than recreation, purposes”. However, sections 133(b)(6) and 133(h) list “recreational trails projects” as eligible activities under STBG. Therefore, the requirement in 23 U.S.C. 217(i) does not apply to recreational trails projects (including for bicycle use) using STBG funds. Section 217(i) continues to apply to bicycle facilities other than trail-related projects, and section 217(i) continues to apply to bicycle facilities using other Federal-aid Highway Program funds (NHPP, HSIP, CMAQ). The transportation requirement under section 217(i) is applicable only to bicycle projects; it does not apply to any other trail use or transportation mode.
- There may be occasional DOT or agency incentive grants for specific research or technical assistance purposes.
- Aspects of many DOT initiatives may be eligible as individual projects. For example, activities above may benefit Ladders of Opportunity; safe, comfortable, interconnected networks; environmental justice; equity; etc.

MEMORANDUM TO COUNCIL

To: Mayor Branson and City Councilmembers
From: Aimée Kniazowski, City Manager
Debra Marlar, City Clerk

Date: April 27, 2017

Agenda Item: V. e. **Appointment to Port and Harbors Advisory Board**

SUMMARY: The recent appointments for the Port and Harbors Advisory Board (PHAB) in 2017 were on January 12, 2017, and March 23, 2017, during the Council's regular meetings. The one alternate seat that remains vacant has a term ending December 31, 2017; the seat is a one-year term. Two applications have been received for the Port and Harbors Advisory Board. The Mayor makes the Port and Harbor Advisory Board appointments and they are confirmed by the Council.

Port and Harbors Advisory Board The board is comprised of seven regular seats, two alternates, and one student seat. There is no residency requirement.

Two new applications have been received. The available seat for appointment is:

One alternate seats ending December 31, 2017 (one-year term)

Applicants:

Lloyd Shanley

Robert Lindsey

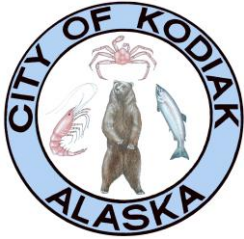
ATTACHMENTS:

Attachment A: Port and Harbors Current Member List and Applications

PROPOSED MOTION:

Move to confirm the Mayoral advisory board appointment to the Port and Harbors Advisory Board as stated.

APRIL 27, 2017
Agenda Item V. e. Memo Page 1 of 1



Office of the City Clerk

710 Mill Bay Road, Room 219, Kodiak, Alaska 99615

PORT AND HARBORS ADVISORY BOARD

Seven regular seats, two alternates, and one student seat

Effective March 23, 2017

TERM	BOARDMEMBER	HOME	WORK or CELL	FAX	MAILING ADDRESS
2017	Marty Owen kodiakowen@gmail.com	486-5079	654-8150		1223 Kouskov, St.
2017	Patrick O'Donnell gwfisheries@yahoo.com	486-2683	539-5296		P.O. Box 3075
2017	Nick Szabo herschel@gci.net	486-3853	486-3853	486-3853	P.O. Box 1633
2018	Tim Abena timabena@aol.com	486-3290	360 957-3200	486-3290	3103 Mill Bay Road
2018	Oliver Holm chicken@gci.net	486-6957	907-654-7005	N/A	P.O. Box 8749
2019	Stormy Stutes stutes@gci.net	486-8757	942-2121	486-8709	2230 Monashka Way
2019	Norm Lenon rymar@gci.net	512-0752	942-3593		522 Sut Larsen Way
2017 *Alternate 1	David Jentry dwjentry@gci.net	486-5205		486-5243	3622 Otmeloi Way
2017 *Alternate 2	VACANT				
Student (ex-officio)	VACANT				

Regular terms expire December 31 (three-year terms)
 Alternate terms expire December 31 (one-year terms)
 Student term expires May 31 (one-year term)

Legislation

Resolution Number 49–81
 Resolution Number 44–86
 Resolution Number 54–87
 Resolution Number 05–94
 Resolution Number 98–32

***[Clerk's Note: The alternates do not make motions or vote unless regular member(s) are absent.]**

Appointments

11/03/87	12/14/87	10/27/88
12/12/88	10/12/89	01/11/90
02/22/90	12/14/90	01/09/92
03/12/92	01/14/93	01/27/94
02/10/94	09/22/94	12/22/94
10/05/95	12/14/95	12/12/96
12/11/97	12/10/98	02/10/00
02/22/01	05/24/01	12/13/01
09/12/02	01/23/03	01/22/04
01/13/05	12/15/05	12/14/06
12/13/07	02/12/09	12/11/09
12/9/10	12/8/11	12/13/12
12/12/13	1/8/15	12/10/15
1/12/17	3/23/17	



City Clerk's Office
 710 Mill Bay Road, Rm 219
 Kodiak, AK 99615
 (907) 486-8636 * (907) 486-8633 (fax)



Advisory Board Application Form

Lloyd Shanley

NAME

907-654-7763

HOME TELEPHONE

Same

WORK TELEPHONE

None

FAX

lloydalaska@hotmail.com

EMAIL

523 Sut Larsen Way Kodiak AK 99615

RESIDENCE (STREET) ADDRESS

Same

MAILING ADDRESS

About 23 years

LENGTH OF RESIDENCE IN KODIAK

57 years

LENGTH OF RESIDENCE IN ALASKA

Are you a registered voter in the City of Kodiak?
 Do you own property in the City of Kodiak?

No
 No

On which boards are you interested in serving?
 (Please list in order of preference)

Please list your areas of expertise and education that
 would benefit the boards for which you are applying.

Port and Harbors

I currently work at Kodiak Electric as the Power

Generation manager. Building and maintaining critical

Infrastructure is a vital part of my job.

Community Activities: _____

I am an active member of the Kodiak Lions club where

I have held several board positions and will be the

Incoming president 7/1/17.

I also own my own sports boat and pay for a slip in

The harbor.

Professional Activities: _____

SIGNATURE

3/8/16
 DATE

Return application to City Clerk, 710 Mill Bay Road, Room 219, Kodiak, AK 99615
 Fax: 486-8633



Advisory Board Application Form

Robert B. Lindsey
NAME

cell 907 942 7769 907 486 7774
HOME TELEPHONE WORK TELEPHONE FAX

Kodiakwaves@gmail.com
EMAIL

3162 Spruce Cape Road
RESIDENCE (STREET) ADDRESS

SAME AS ABOVE KODIAK, AK 99615
MAILING ADDRESS

55 +/- years 55
LENGTH OF RESIDENCE IN KODIAK LENGTH OF RESIDENCE IN ALASKA

Are you a registered voter in Kodiak? YES NO Do you own property in Kodiak? YES NO

On which boards are you interested in serving?
(Please list in order of preference)

Ports + Harbors advisory (AET)

Please list your areas of expertise and education
that would benefit the boards for which you are
applying.

Fishing / CONSTRUCTION
ELECTRICAL SYSTEM
HARBOR

Community Activities:

City of Kodiak Rep -
CIRCAAC
Community Baptist Churd

Professional Activities:

Rob Lindsey 3/7/2017
SIGNATURE DATE

Return application to City Clerk, 710 Mill Bay Road, Room 220, Kodiak, AK 99615
Fax: 486-8633